

Proposed Code Change:

---

---

Supporting Statement (Reason for change should include an “authoritative source” and cost analysis where appropriate.):

---

Changes to the technical requirements in the standards must be based on recommended site improvement standards that are published by an academic or professional institution or organization, similar to those used in the original Rutgers Model Subdivision and Site Plan Ordinance.

---

New Rule, R.2001 d.352, effective October 1, 2001.  
 Sec: 33 N.J.R. 1237(a), 33 N.J.R. 3427(a).

SUBCHAPTER 4. STREETS AND PARKING

5:21-4.1 Street hierarchy

(a) Streets shall be classified in a hierarchy with design tailored to function. The street hierarchy definitions contained within this section are applicable only to local residential streets and are not to be considered related to the U.S. Department of Transportation, Federal Highway Administration's Functional Classification of Highways.

(b) The street hierarchy system shall be defined by road function and average daily traffic (ADT), calculated by trip generation rates from the current edition of "Trip Generation" by the Institute of Transportation Engineers, as indicated in Table 4.1 below. Trip generation rates from other sources may be used if the applicant demonstrates to the appropriate approving authority that these sources better reflect local conditions. In addition, the applicant shall investigate the opportunities for, and availability of, transit facilities and, if appropriate, consider their impact(s) on motor vehicle traffic trip generation rates per dwelling unit.

(c) Each residential street shall be classified and designed to meet the standards for one of the street types defined in Table 4.2 below. The entire length of the street need not be designed based on the highest ADT where the ADT varies along the street's length. However, each street segment between intersections shall be designed based on the highest ADT served in that segment.

(d) The municipality and the developer shall determine the highest order street required to be used in a given residential development, considering all of the following:

1. The size of the development (number and type of units). For example, using size to determine the highest order of street required, a development of up to 150 single-family detached units would not require any minor collectors or streets of a higher order;
2. The actual or potential development of adjacent sites (whether there is likely to be traffic passing through from neighboring developments). A "potential" development means a development having approvals granted, applications pending, or undergoing preliminary review; and
3. The streets proposed for that area, if any, as contained in the municipal master plan.

TABLE 4.1  
 AVERAGE DAILY MOTOR VEHICLE  
 TRAFFIC TRIP GENERATION  
 PER DWELLING UNIT<sup>1</sup>

| Land use <sup>2</sup>          | Peak rate |
|--------------------------------|-----------|
| Single-family detached housing | 10.1      |

| Land use <sup>2</sup>               | Peak rate |
|-------------------------------------|-----------|
| Townhouse                           | 5.9       |
| Low-rise apartment                  | 7.2       |
| Mid-rise apartment                  | 5.5       |
| High-rise apartment                 | 5.0       |
| Mobile home park                    | 5.0       |
| Retirement community                | 2.8       |
| Recreational homes (owner occupied) | 3.2       |

Notes:

<sup>1</sup> The trip generation rates listed are guidelines only. The actual use of trip generation rates is derived by the use of regression analysis and should be computed only by professionals proficient in the use of the ITE Trip Generation manual. The "Land Use" definitions are based on the ITE manual with slight modifications to address inconsistencies contained within the ITE manual.

<sup>2</sup> For two-family dwellings (duplexes), apply the values for single-family dwellings to each unit.

Source: Institute of Transportation Engineers, Trip Generation (Washington, D.C.: ITE, 1982), 3rd Edition. The table was updated with data from the 6th Edition of the manual published by ITE in 1997. The peak ADT rates take into consideration Saturday and Sunday rates, as well as weekday rates.

DEFINITIONS

| Land use                       | Definition  |
|--------------------------------|---|
| Single-family detached housing | Any single-family detached home on an individual lot.   |
| Townhouse                      | Attached multiple-family dwelling units where the only separation between units is vertical.  |
| Apartment                      | A dwelling unit located within the same building with at least three other dwelling units.  |
| Low-rise apartment             | Apartments in buildings that have one or two levels (floors).   |
| Mid-rise apartment             | Apartments in buildings that have more than two levels (floors) and less than ten levels.   |
| High-rise apartment            | Apartments in buildings with ten or more levels (floors).   |
| Mobile home park               | Generally trailers shipped, sited and installed on permanent foundations and in areas that typically have community facilities, such as recreation rooms, swimming pools, and laundry facilities.                                   |
| Retirement community           | Residential units similar to apartments and condominiums usually restricted to adults or senior citizens, and located in self-contained villages. Special services such as medical, dining, and retail facilities may be available. |

| <u>Land use</u>   | <u>Definition</u>   |
|-------------------|---|
| Recreational home | Dwellings usually located in a resort containing local services and complete recreational facilities. These are often second homes used by the owner or rented on a seasonal basis. |

TABLE 4.2  
RESIDENTIAL STREET HIERARCHY DEFINITIONS

| <u>Street type</u>                    | <u>Description</u>   | <u>Average daily traffic (maximum)</u> |
|---------------------------------------|--|--|
| Residential Access <sup>‡</sup>       | <p>Lowest order, other than rural street type, of residential streets Provides frontage for access to lots and carries traffic with destination or origin on the street itself. Designed to carry the least amount of traffic at the lowest speed. All, or the maximum number of housing units, shall front on this class of street.</p> <p><sup>†</sup> Residential access streets of "loop" configuration, that is, two ways out, should be designed so no section conveys an ADT greater than 1500. Each half of a loop street may be classified as a single residential access street, but the total traffic volume generated on the loop street should not exceed 1500 ADT, nor should it exceed 750 ADT at any point of traffic concentration.</p> | 1,500 <sup>†</sup>                     |
| Residential Neighborhood <sup>‡</sup> | <p>A type of residential access street conforming to traditional subdivision street design, and providing access to building lots fronting on a street and parking on both sides of street.</p> <p><sup>‡</sup> Applicant may choose either the RESIDENTIAL ACCESS or the RESIDENTIAL NEIGHBORHOOD street type for new streets. See section 4.8(b) for specific right-of-way and cartway width requirements for new streets that are a continuation of an existing street.</p>   |  |
| Minor Collector                       | <p>Middle order of residential street. Provides frontage for access to lots and carries traffic of adjoining residential access streets. Designed to carry somewhat higher traffic volumes than lower-order streets such as rural and residential access streets, with traffic limited to motorists having origin or destination within the immediate neighborhood. Is not intended to carry regional traffic.</p> <p>Each half of a loop-configured minor collector may be classified as a single minor collector street, but the total traffic volume conveyed on the loop should not exceed 3,500 ADT, nor should it exceed 1750 ADT at any point of traffic concentration.</p>   | 3,500                                  |
| Major Collector                       | <p>Highest order of residential streets. Conducts and distributes traffic between lower-order residential streets and higher-order streets—arterials and expressways. Carries the largest volume of traffic at higher speeds. Function is to promote free traffic flow; therefore, parking should be prohibited and direct access to homes from this level of street should be avoided. Collectors should be designed so they cannot be used as shortcuts by non-neighborhood traffic.</p>   | 7,500                                  |
| Special Purpose Streets               |  |  |
| Rural                                 | <p>A rural street is a street that serves dwellings on lots that are one acre or greater, AND primarily serves as access to abutting building lots, AND has no on-street parking, AND has lot-to-street access designed so vehicles do not back out of lots onto the street. Rural streets shall only connect to rural streets, rural residential lanes, or mixed-use collectors. However, a rural street shall not connect two mixed-use collectors.</p>  | 500                                    |
| Rural residential lane                | <p>A rural residential lane is a street that serves dwellings on lots that are two acres or greater, AND primarily serves as access to abutting building lots, AND has no on-street parking, AND has lot-to-street access designed so vehicles do not have to back out of lots onto the street. Rural residential lanes shall only connect to rural streets, rural residential lanes, or mixed-use collectors. However, a rural residential lane shall not connect two mixed-use collectors.</p>   | 200                                    |

| Street type                   | Description   | Average daily traffic (maximum)                                   |
|-------------------------------|---|---|
| Alley                         | A service road that provides a secondary means of access to lots. On same level as residential access street, but different standards apply. No parking shall be permitted; alleys should be designed to discourage through traffic. ADT level shall not exceed that of a residential access street.  | 500   |
| Cul-de-sac <sup>1</sup>       | A street with a single means of ingress and egress and having a turnaround, the design of which may vary. A divided-type entrance roadway to at least the first cross street, with median of sufficient width to ensure freedom of continued emergency access by lanes on one side, shall not be considered part of a cul-de-sac. Parking lots with a single means of ingress and egress shall not be included within the definition of cul-de-sac. | 250   |
| Marginal access street        | A service street that runs parallel to a higher-order street and provides access to abutting properties and separation from through traffic. May be designed as residential access street or minor collector, according to anticipated daily traffic.   | 1,500 (residential access total)<br>3,500 (minor collector total) |
| Divided street                | Municipalities may require streets to be divided to provide alternate emergency access, protect the environment, or avoid grade changes. Design standards should be applied to the combined dimensions of the two street segments, as required by the street class.   |   |
| Multifamily access cul-de-sac | A street with a single means of ingress and egress, which serves multifamily development, that provides a means for vehicles to turn around.  | 1,000   |
| Multifamily court             | A street with a single means of ingress and egress, which serves multifamily development, that does not provide a means for vehicles to turn around. The length of multifamily courts is limited to 300 feet.   | Note <sup>2</sup>   |

Notes:

<sup>1</sup> Streets serving multifamily developments with a single means of ingress and egress shall be classified as multifamily access cul-de-sacs.

<sup>2</sup> There is no ADT limit for multifamily courts specified because the length of the court will effectively limit the ADT to acceptable levels.

Administrative correction.  
See: 29 N.J.R. 1296(a).  
Amended by R.1999 d.374, effective November 1, 1999 (operative May 1, 2000).  
See: 31 N.J.R. 477(a), 31 N.J.R. 3259(a).  
In (c), added a second sentence; and in Table 4.2, added a third sentence in the Cul-de-sac Description, and substituted a reference to non-parallel parking for a reference to perpendicular parking in the Parking Loop Description.  
Administrative correction.  
See: 32 N.J.R. 684(b).  
Amended by R.2000 d.480, effective December 4, 2000 (operative June 3, 2001).  
See: 32 N.J.R. 2670(b), 32 N.J.R. 4277(a).  
Rewrote Table 4.1 and in Table 4.2, rewrote the description of "Rural residential lane".  
Amended by R.2002 d.399, effective December 16, 2002.  
See: 34 N.J.R. 2615(a), 34 N.J.R. 4412(a).  
In Table 4.1, inserted "Trip General" preceding "manual" in the second sentence of footnote 1, and added footnote 2; rewrote Table 4.2.  
Public Notice: Notice regarding the Publication of two Notices of Adoption in the December 16, 2002 New Jersey Register.  
See: 34 N.J.R. 4343(a), 4412(a), 35 N.J.R. 219(b).

5:21-4.2 Cartway width

(a) Cartway width for each street classification shall be determined by parking and curbing requirements that are based on intensity of development.

(b) Intensity of development shall be based on dwelling units per gross acre as follows:

| Intensity | Dwelling Units per Gross Acre <sup>†</sup> |
|-----------|--|
| Low       | Less than or equal to 4                    |
| Medium    | More than 4 and less than or equal to 8    |
| High      | More than 8                                |

Note: <sup>†</sup>In determining the intensity of development, the gross acreage shall not include dedicated common open space or other such areas restricted from future development.

(c) Cartway widths for each street classification are as shown in Table 4.3 below.

(d) Cartway width also shall consider possible limitations imposed by sight distances, climate, terrain, and maintenance needs.

(e) Municipalities may require additional cartway width for major or minor collectors which are part of a designated bike route as indicated in the bicycle circulation part of the municipal master plan to make them consistent with the AASHTO guidelines for bicycle-compatible streets.

(h) Where curbs and gutters are used and where the street is part of a designated bike route as indicated in the bicycle circulation part of the municipal master plan, the municipality may require that the cartway width be increased by one foot on each side of a street that uses a curb and gutter.

Amended by R.2000 d.480, effective December 4, 2000 (operative June 3, 2001).

See: 32 N.J.R. 2670(b), 32 N.J.R. 4277(a).

Added (h).

#### 5:21-4.4 Shoulders

(a) Shoulders should be used instead of curbs when:

1. Shoulders are required by CAFRA;
2. Soil and/or topography make the use of shoulders preferable; and/or
3. To preserve rural character.

(b) Shoulders shall be provided in accordance with the requirements in Table 4.3 in N.J.A.C. 5:21-4.2.

(c) Shoulders shall be four feet wide, except for minor collector streets of high intensity with off-street parking, which shall be six feet wide on each side for all streets, and major collector streets of medium and high intensity, which shall be eight feet wide on each side for all streets. Shoulders shall be located within the right-of-way as shown in the following street illustrations.

(d) Shoulders shall be constructed of materials such as stabilized earth, gravel, crushed stone, bituminous treatment, or other forms of pavement which provide for vehicle load support. Shoulders along major collectors and shoulders along streets that are part of a designated bike path as indicated in the bicycle circulation portion of the municipal master plan shall be paved with asphalt pavement.

Amended by R.1999 d.374, effective November 1, 1999 (operative May 1, 2000).

See: 31 N.J.R. 477(a), 31 N.J.R. 3259(a).

Rewrote (c).

Amended by R.2000 d.480, effective December 4, 2000 (operative June 3, 2001).

See: 32 N.J.R. 2670(b), 32 N.J.R. 4277(a).

In (a) and (a)2, deleted "and/or drainage swales" preceding "preferable"; and in (d), inserted the last sentence.

Public Notice: Notice regarding the Publication of two Notices of Adoption in the December 16, 2002 New Jersey Register.

See: 34 N.J.R. 4343(a), 4412(a), 35 N.J.R. 219(b).

#### 5:21-4.5 Sidewalks and graded areas

(a) Sidewalks and/or graded areas shall be required, depending on road classification and intensity of development, in accordance with the requirements set forth in Table 4.3 in N.J.A.C. 5:21-4.2.

(b) Sidewalks shall be provided where graded areas are specified in Table 4.3 when the conditions described in (b)1 or 2 below exist:

1. The minimum lot size in the development is smaller than one acre; and

i. The development or project is located within 2,500 feet of a train station, public or school bus route;

ii. The development or project is located within 2,500 feet of an existing recreational, business or retail use or a site where such use is permitted by existing zoning; or

iii. Where the proposed streets connect to or extend existing streets which have sidewalks on both sides; or

2. The minimum lot size in the development is smaller than two acres and the development is located within two miles of a school.

(c) Notwithstanding (b)1 and 2 above, sidewalks shall only be required on one side of rural streets or rural lanes and shall not be required in alleys.

(d) Sidewalks shall be placed parallel to the street, as shown in the street profile figures, unless an exception has been permitted to preserve topographical or natural features, or if required to provide visual interest, or unless the applicant shows that an alternative pedestrian system provides safe and convenient circulation (for example, in planned development).

(e) Sidewalks along streets with nonparallel parking shall be placed parallel to the street, and shall be placed so that sidewalks do not lead pedestrians between parked vehicles and the traveled way. This subsection shall not apply to driveways.

(f) Pedestrian-way easements at least 10-foot wide may be required by the municipal approving authority through the center of blocks more than 600-foot long. In providing circulation or access to schools, playgrounds, shopping, adjoining residential areas, or other community facilities, the municipality shall consider and may require pedestrian-way easements.

(g) Sidewalk width shall be four feet; wider widths may be necessary near pedestrian generators and employment centers. Where sidewalks abut the curb and cars overhang the sidewalk, widths shall be six feet. In high-density residential areas when sidewalks abut the curb, a sidewalk/graded area of at least six feet in width shall be required.

(h) Sidewalks and graded areas shall be constructed according to the specifications set forth in N.J.A.C. 5:21-4.18.

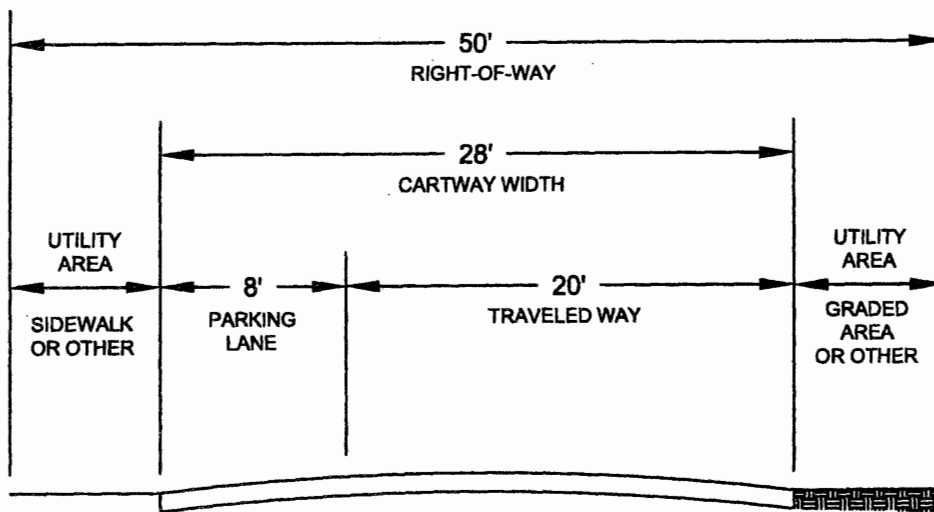
#### ILLUSTRATIONS OF STREET LAYOUTS FOLLOW:

Note: The individual components shown in the non-travel-way portion of the right-of-way such as utility areas, sidewalks, and graded areas are indicated for illustrative purposes only. Municipalities may vary the placement and

dimensions of these individual items, depending on utility company requirements and local practice and preferences. In addition, items such as shade trees may be accommodated within the total right-of-way widths indicated for each street type. Several street types are not illustrated because of the limited or various, as the case may be, design possibilities.

**RESIDENTIAL ACCESS**  
(low intensity)

Illustration 1 of 14

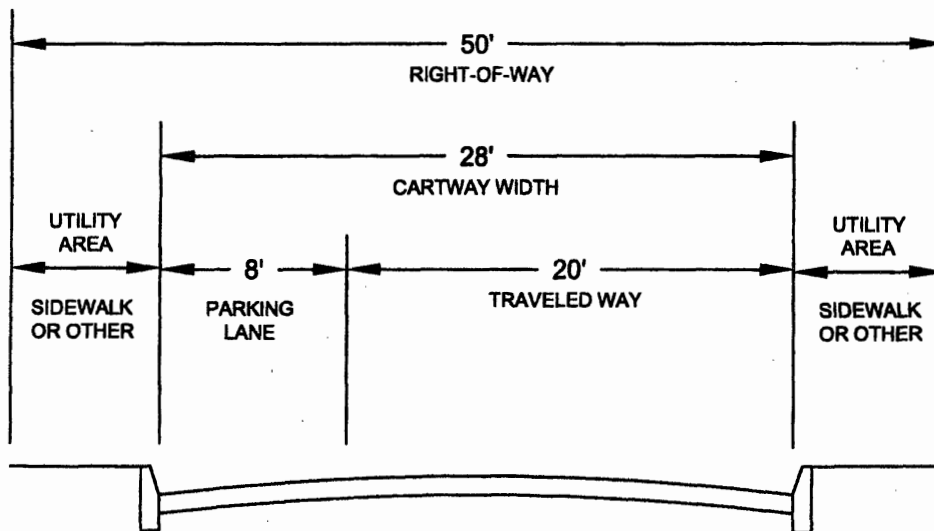


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |              |
|--------------------------|--------------|
| TRAVELED WAY:            | 20 FEET      |
| NUMBER OF PARKING LANES: | 1            |
| PARKING LANE WIDTH:      | 8 FEET       |
| CARTWAY WIDTH:           | 28 FEET      |
| CURB OR SHOULDER:        | NONE         |
| SIDEWALK OR GRADED AREA: | 1 SW<br>1 GA |
| RIGHT-OF-WAY:            | 50 FEET      |

**RESIDENTIAL ACCESS**  
 (high intensity with on-street parking  
 and medium intensity)

Illustration 2 of 14

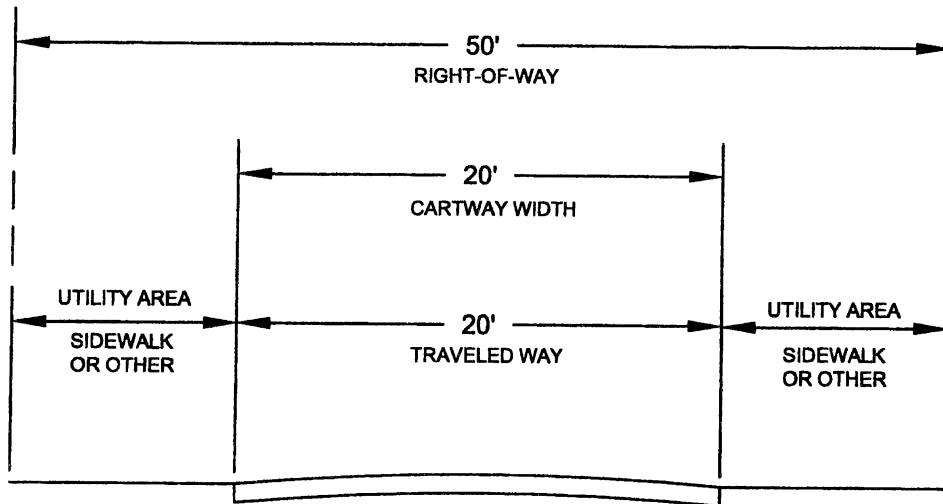


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |         |
|--------------------------|---------|
| TRAVELED WAY:            | 20 FEET |
| NUMBER OF PARKING LANES: | 1       |
| PARKING LANE WIDTH:      | 8 FEET  |
| CARTWAY WIDTH:           | 28 FEET |
| CURB OR SHOULDER:        | CURB    |
| SIDEWALK OR GRADED AREA: | 2 SW    |
| RIGHT-OF-WAY:            | 50 FEET |

**RESIDENTIAL ACCESS**  
 (high intensity with off-street parking)

Illustration 3 of 14

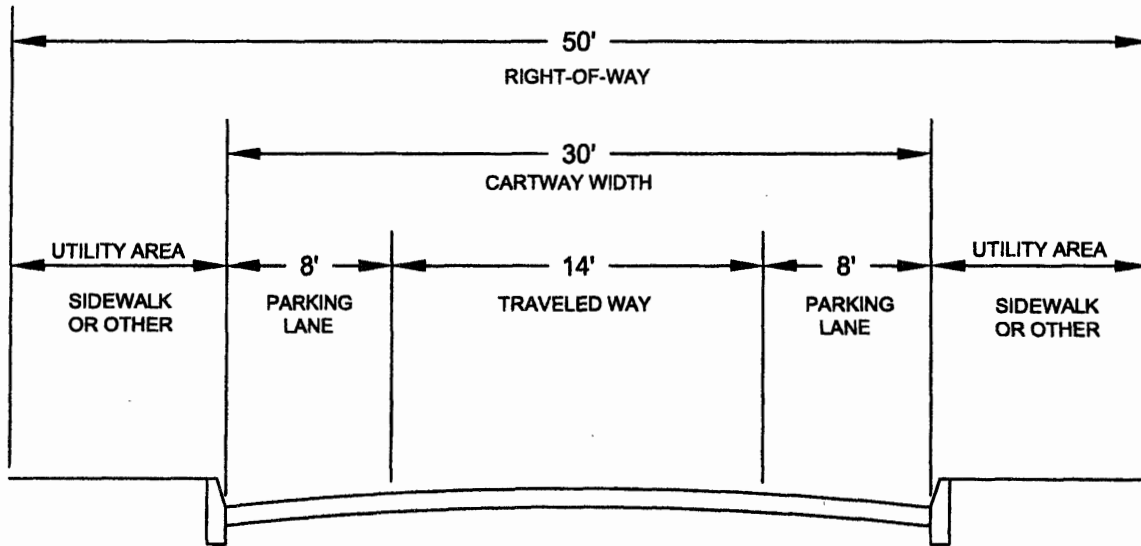


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |         |
|--------------------------|---------|
| TRAVELED WAY:            | 20 FEET |
| NUMBER OF PARKING LANES: | 0       |
| PARKING LANE WIDTH:      | 0 FEET  |
| CARTWAY WIDTH:           | 20 FEET |
| CURB OR SHOULDER:        | NONE    |
| SIDEWALK OR GRADED AREA: | 2 SW    |
| RIGHT-OF-WAY:            | 50 FEET |

NEIGHBORHOOD  
(all intensities)

Illustration 4 of 14

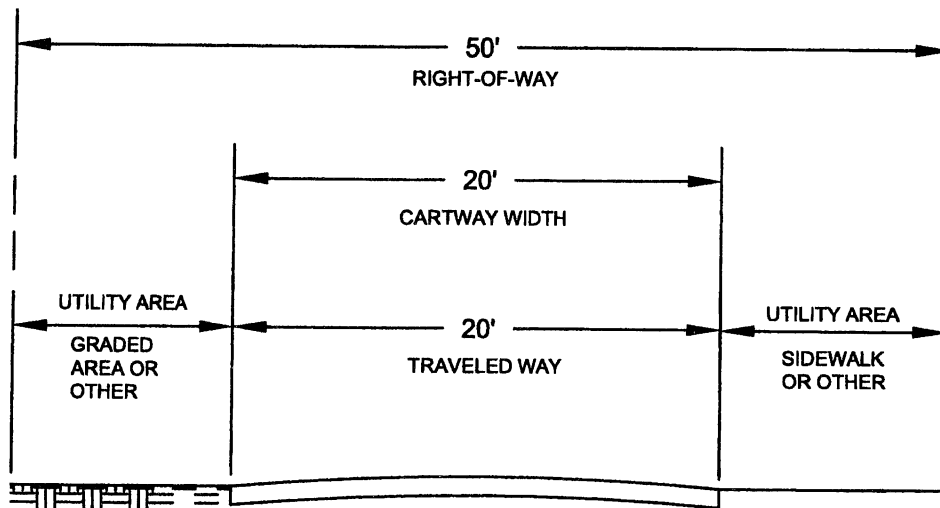


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |         |
|--------------------------|---------|
| TRAVELED WAY:            | 14 FEET |
| NUMBER OF PARKING LANES: | 2       |
| PARKING LANE WIDTH:      | 16 FEET |
| CARTWAY WIDTH:           | 30 FEET |
| CURB OR SHOULDER:        | CURB    |
| SIDEWALK OR GRADED AREA: | 2 SW    |
| RIGHT-OF-WAY:            | 50 FEET |

**MINOR COLLECTOR**  
 (low intensity with no parking)

Illustration 5 of 14

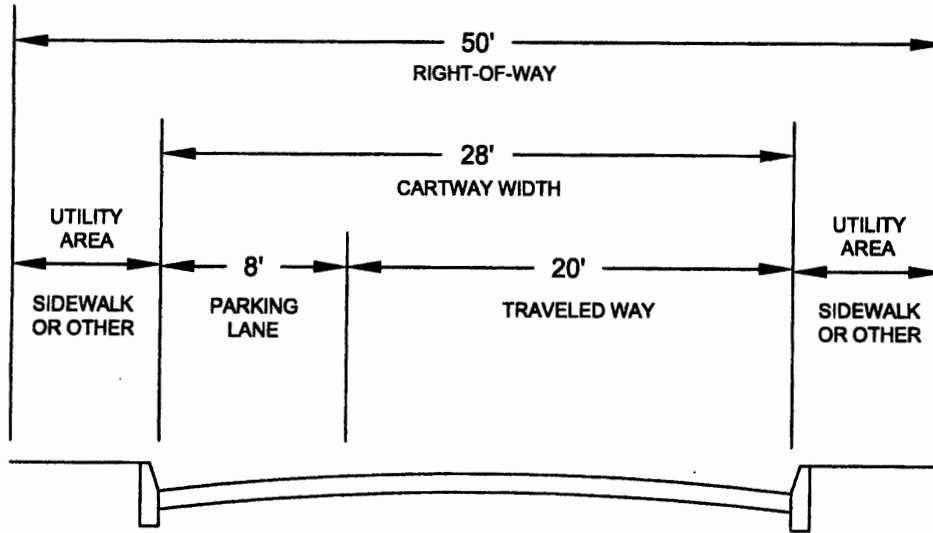


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |              |
|--------------------------|--------------|
| TRAVELED WAY:            | 20 FEET      |
| NUMBER OF PARKING LANES: | 0            |
| PARKING LANE WIDTH:      | 0 FEET       |
| CARTWAY WIDTH:           | 20 FEET      |
| CURB OR SHOULDER:        | NONE         |
| SIDEWALK OR GRADED AREA: | 1 SW<br>1 GA |
| RIGHT-OF-WAY:            | 50 FEET      |

**MINOR COLLECTOR**  
 (low, medium, and high intensity  
 with one parking lane)

Illustration 6 of 14

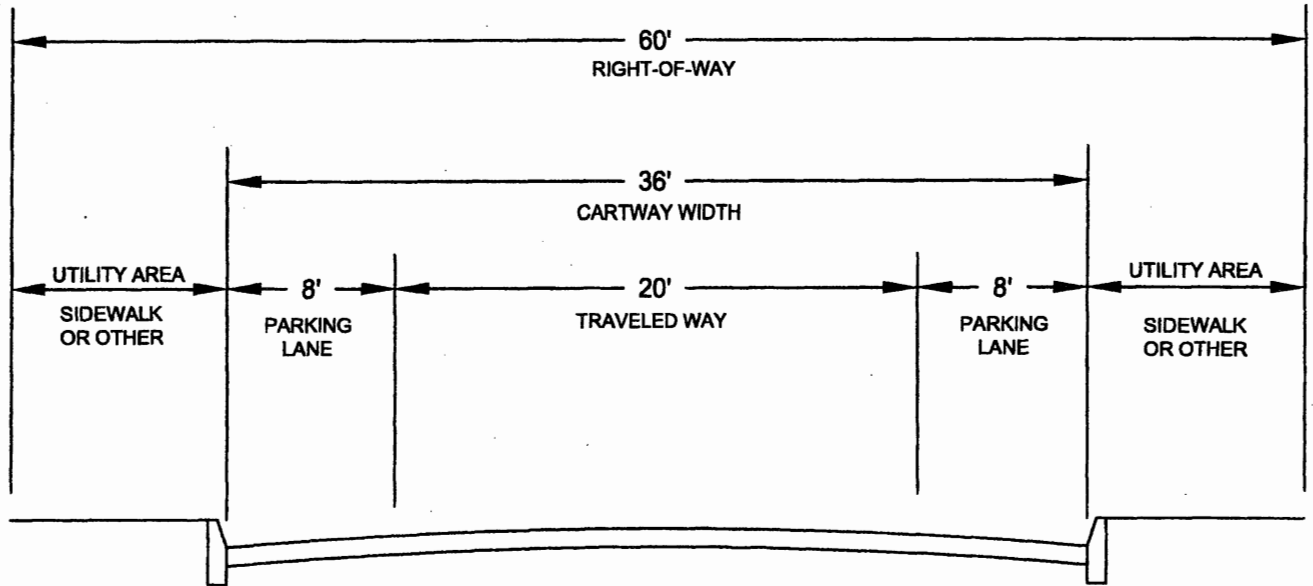


FOR ILLUSTRATIVE PURPOSES ONLY

|                           |          |
|---------------------------|----------|
| TRAVELED WAY:             | 20 FEET  |
| NUMBER OF PARKING LANES:  | 1        |
| PARKING LANE WIDTH:       | 8 FEET   |
| CARTWAY WIDTH:            | 28 FEET  |
| CURB OR SHOULDER:         | CURB     |
| SIDEWALK OR GRADED AREA:  |          |
| low, one parking lane:    | 1SW, 1GA |
| medium, one parking lane: | 2 SW     |
| high, one parking lane:   | 2 SW     |
| RIGHT-OF-WAY:             | 50 FEET  |

**MINOR COLLECTOR**  
 (medium and high intensities  
 with two parking lanes)

Illustration 7 of 14

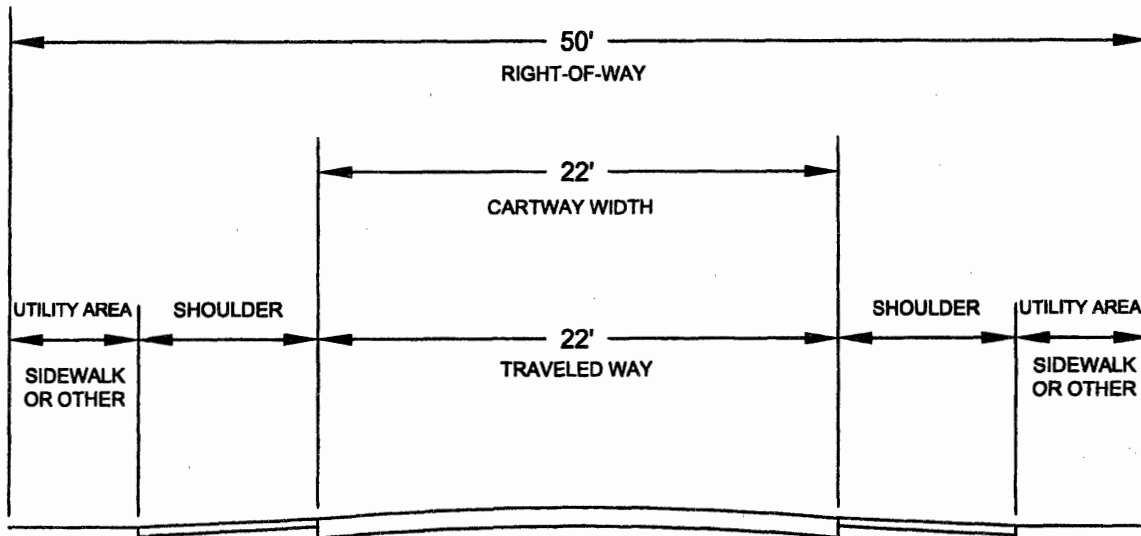


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |         |
|--------------------------|---------|
| TRAVELED WAY:            | 20 FEET |
| NUMBER OF PARKING LANES: | 2       |
| PARKING LANE WIDTH:      | 16 FEET |
| CARTWAY WIDTH:           | 36 FEET |
| CURB OR SHOULDER:        | CURB    |
| SIDEWALK OR GRADED AREA: | 2 SW    |
| RIGHT-OF-WAY:            | 60 FEET |

**MINOR COLLECTOR**  
 (medium and high intensity  
 with off-street parking  
 and shoulders)

Illustration 8 of 14

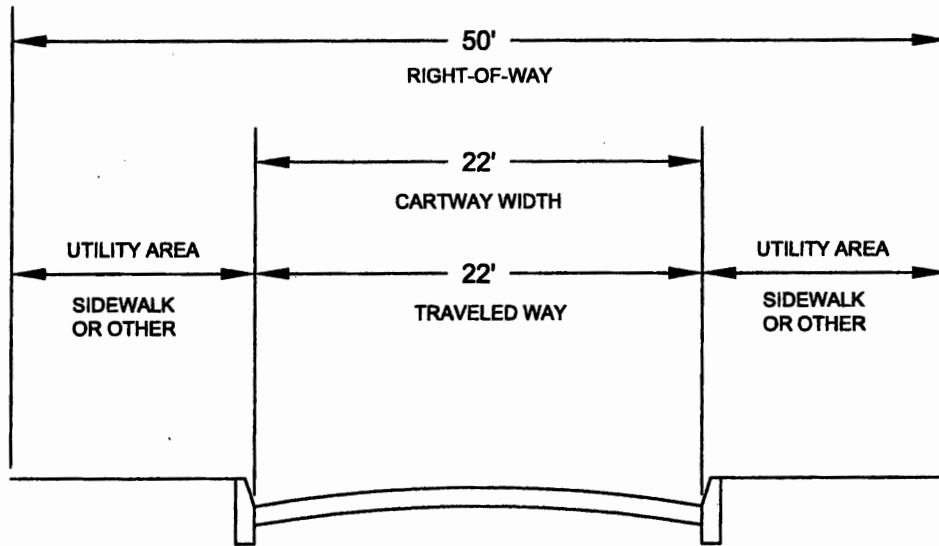


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |          |
|--------------------------|----------|
| TRAVELED WAY:            | 22 FEET  |
| NUMBER OF PARKING LANES: | 0        |
| PARKING LANE WIDTH:      | 0 FEET   |
| CARTWAY WIDTH:           | 22 FEET  |
| CURB OR SHOULDER:        | SHOULDER |
| medium intensity:        | 4 FEET   |
| high intensity:          | 6 FEET   |
| SIDEWALK OR GRADED AREA: | 2 SW     |
| RIGHT-OF-WAY:            | 50 FEET  |

**MINOR COLLECTOR**  
 (medium and high intensity  
 with off-street parking and curb)

Illustration 9 of 14

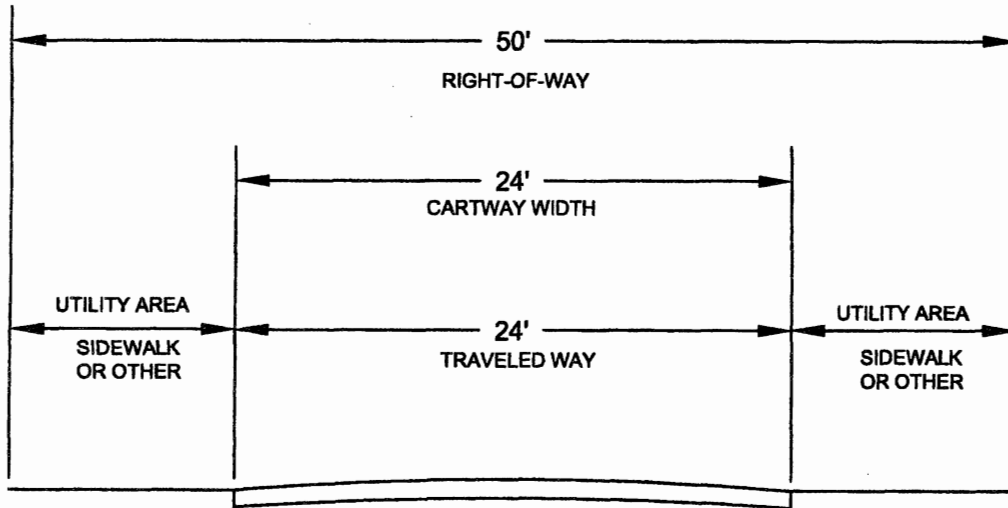


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |         |
|--------------------------|---------|
| TRAVELED WAY:            | 22 FEET |
| NUMBER OF PARKING LANES: | 1       |
| PARKING LANE WIDTH:      | 0 FEET  |
| CARTWAY WIDTH:           | 22 FEET |
| CURB OR SHOULDER:        | CURB    |
| SIDEWALK OR GRADED AREA: | 2 SW    |
| RIGHT-OF-WAY:            | 50 FEET |

**MAJOR COLLECTOR**  
(low intensity)

Illustration 10 of 14

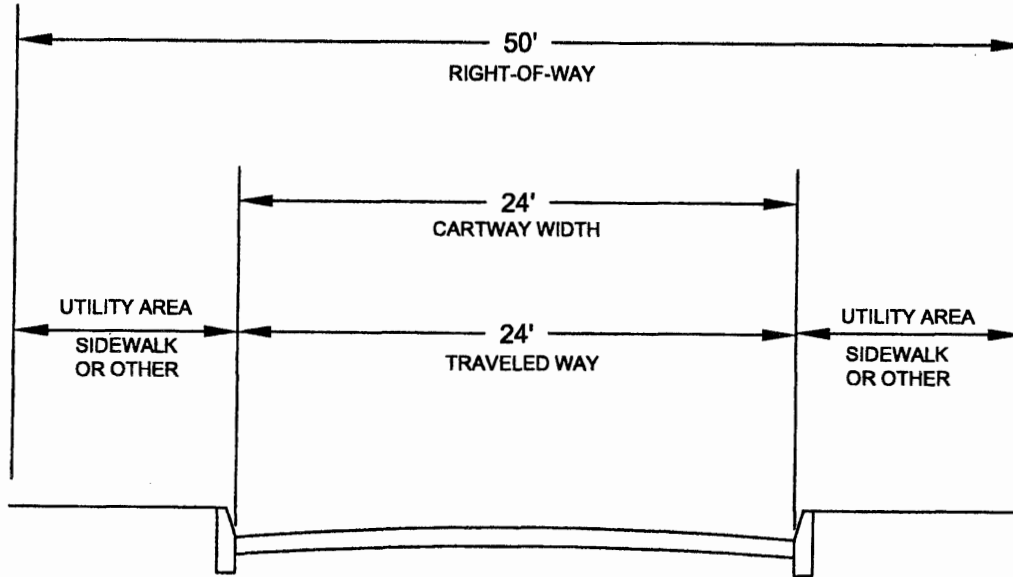


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |         |
|--------------------------|---------|
| TRAVELED WAY:            | 24 FEET |
| NUMBER OF PARKING LANES: | 0       |
| PARKING LANE WIDTH:      | 0 FEET  |
| CARTWAY WIDTH:           | 24 FEET |
| CURB OR SHOULDER:        | NONE    |
| SIDEWALK OR GRADED AREA: | 2 SW    |
| RIGHT-OF-WAY:            | 50 FEET |

**MAJOR COLLECTOR**  
 (medium and high intensity with curb)

Illustration 11 of 14

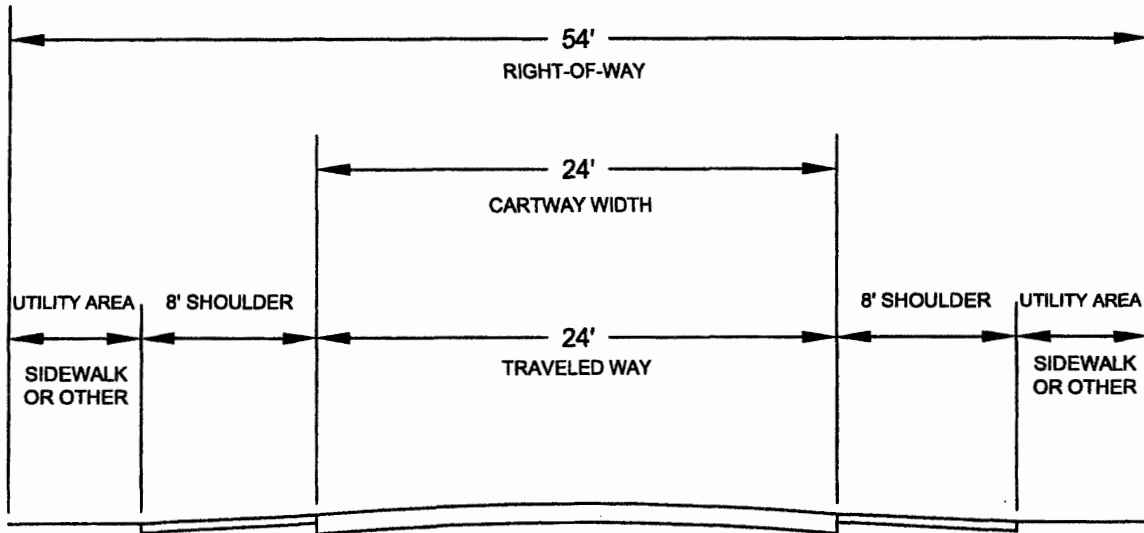


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |         |
|--------------------------|---------|
| TRAVELED WAY:            | 24 FEET |
| NUMBER OF PARKING LANES: | 0       |
| PARKING LANE WIDTH:      | 0 FEET  |
| CARTWAY WIDTH:           | 24 FEET |
| CURB OR SHOULDER:        | CURB    |
| SIDEWALK OR GRADED AREA: | 2 SW    |
| RIGHT-OF-WAY:            | 50 FEET |

**MAJOR COLLECTOR**  
 (medium and high intensity  
 with shoulders)

Illustration 12 of 14

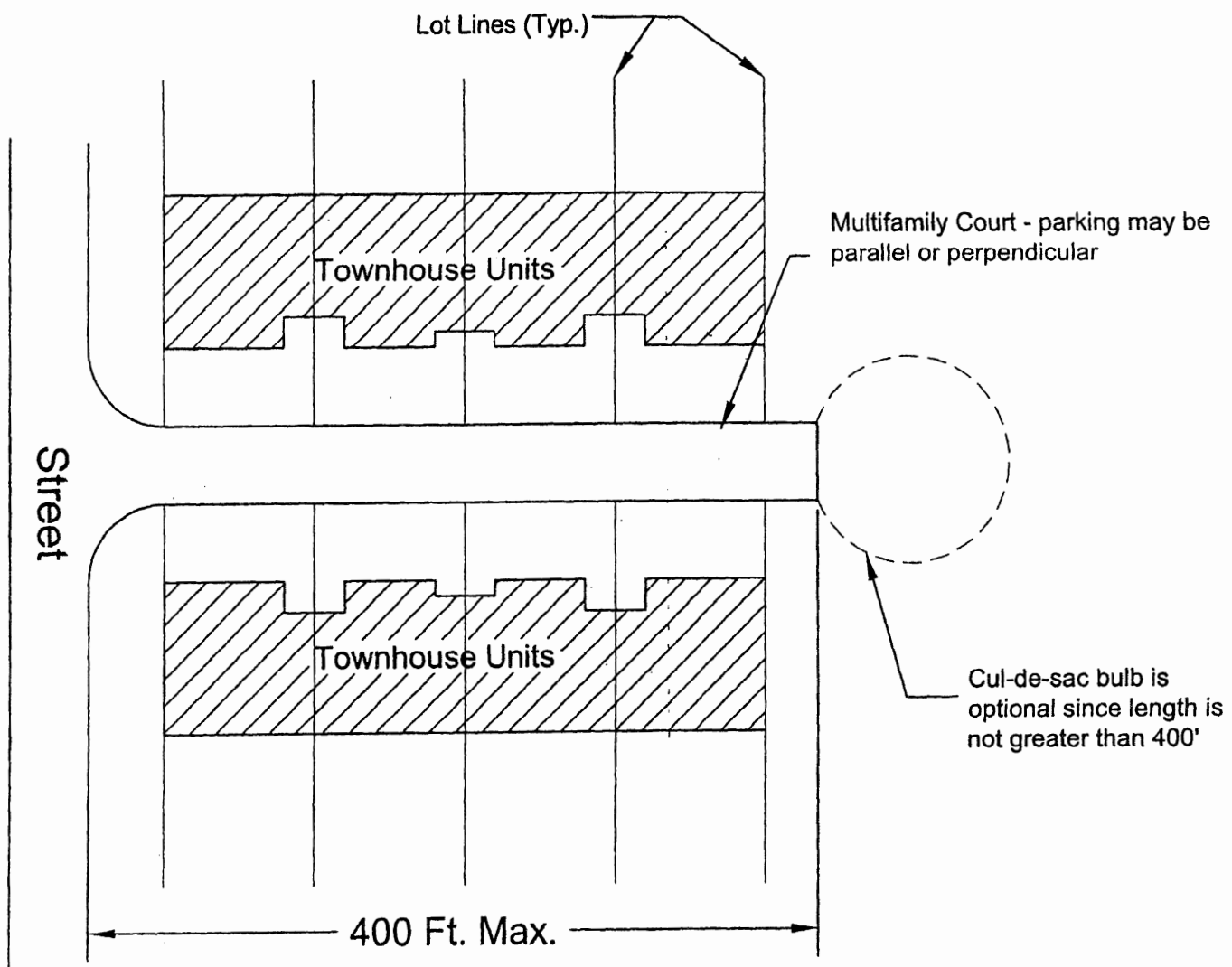


FOR ILLUSTRATIVE PURPOSES ONLY

|                          |          |
|--------------------------|----------|
| TRAVELED WAY:            | 24 FEET  |
| NUMBER OF PARKING LANES: | 0        |
| PARKING LANE WIDTH:      | 0 FEET   |
| CARTWAY WIDTH:           | 24 FEET  |
| CURB OR SHOULDER:        | SHOULDER |
| SIDEWALK OR GRADED AREA: | 2 SW     |
| RIGHT-OF-WAY:            | 54 FEET  |

MULTIFAMILY COURT

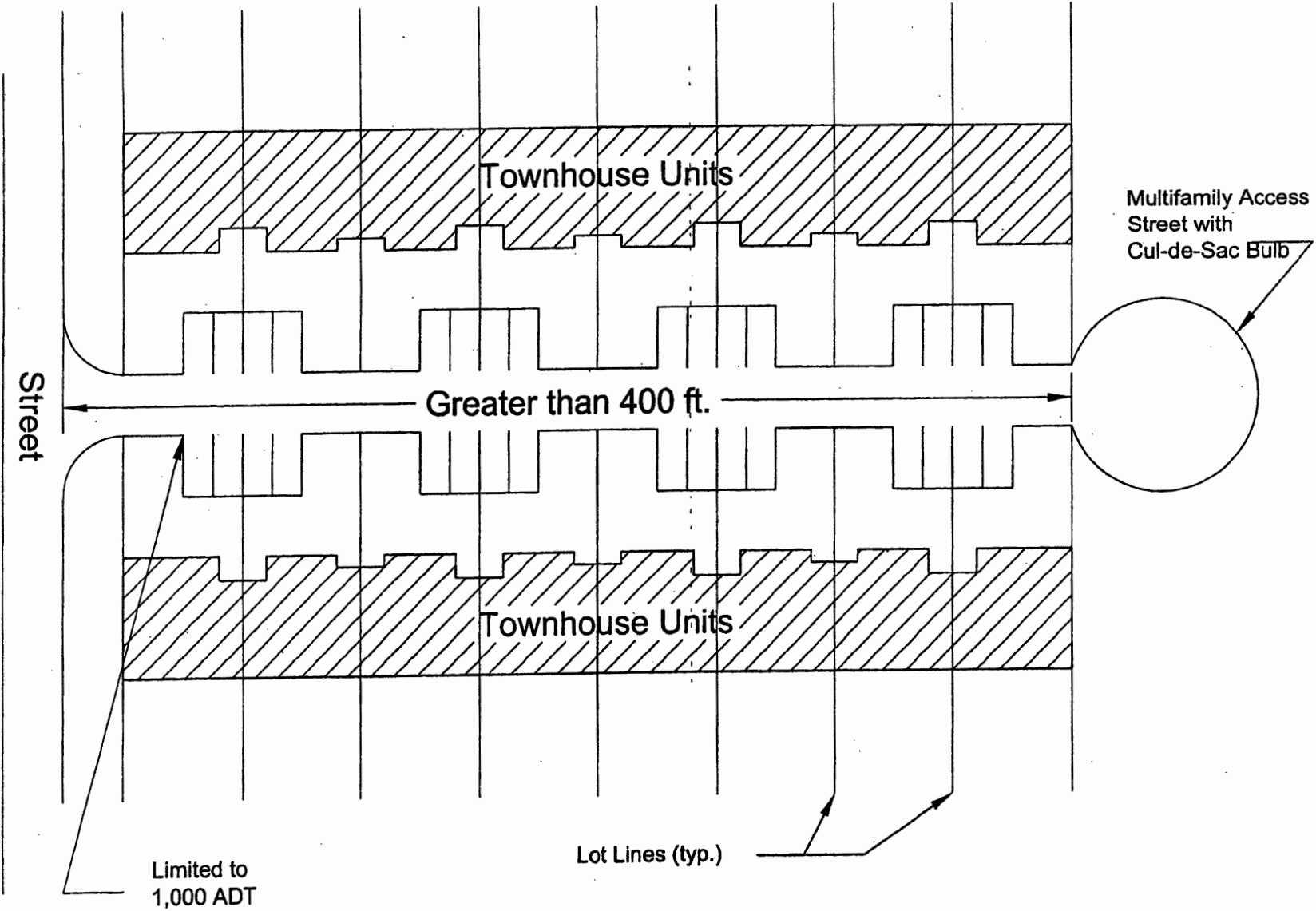
Illustration 13 of 14



FOR ILLUSTRATIVE PURPOSES ONLY

MULTIFAMILY  
CUL-DE-SAC

Illustration 14 of 14



FOR ILLUSTRATIVE PURPOSES ONLY

Administrative correction.

See: 29 N.J.R. 1296(a).

Amended by R.1999 d.374, effective November 1, 1999 (operative May 1, 2000).

See: 31 N.J.R. 477(a), 31 N.J.R. 3259(a).

Public Notice: Special area standards.

See: 33 N.J.R. 897(a).

Amended by R.2002 d.399, effective December 16, 2002.

See: 34 N.J.R. 2615(a), 34 N.J.R. 4412(a).

Rewrote (b)1; added new (e); recodified former (e) through (g) as new (f) through (h); added Illustrations 13 and 14.

Public Notice: Notice regarding the Publication of two Notices of Adoption in the December 16, 2002 New Jersey Register.

See: 34 N.J.R. 4343(a), 4412(a), 35 N.J.R. 219(b).

#### 5:21-4.6 Bikeways

(a) Separate bicycle paths and lanes shall be required only if such paths and lanes have been specified as part of a municipality's adopted master plan and/or official map.

(b) Bicycle lanes, where provided, shall be placed in the outside lane of a roadway, adjacent to the curb or shoulder. When on-street parking is permitted, the bicycle lane shall be between the parking lane and the outer lane of moving vehicles. Lanes shall be delineated with markings, preferably striping. Raised reflectors or curbs shall not be used.

(c) The construction of bikeways shall comply with the specifications set forth in N.J.A.C. 5:21-4.18.

#### 5:21-4.7 Utility areas

(a) Utility mains shall be located within the right-of-way or within utility easements outside the right-of-way.

(b) Utility areas shall be planted with grass, ground cover, or treated with other suitable cover material.

Amended by R.1999 d.374, effective November 1, 1999 (operative May 1, 2000).

See: 31 N.J.R. 477(a), 31 N.J.R. 3259(a).

Rewrote (a).

#### 5:21-4.8 Right-of-way and cartway

(a) The right-of-way shall be measured from lot line to lot line. Right-of-way requirements are shown in Table 4.3 in N.J.A.C. 5:21-4.2 and displayed graphically in the street illustrations in N.J.A.C. 5:21-4.5.

(b) The municipal approving authority may require the right-of-way and cartway widths of a new street that is a continuation of an existing street to be at least the same widths as the existing street.

(c) The right-of-way shall be of sufficient width to accommodate future development, as indicated by the municipal master plan.

(d) Where turning lanes are needed based on safety or capacity, additional right-of-way width, not to exceed the width and length of the turning lanes, may be required.

#### 5:21-4.9 Street grade and intersections

Street grade and intersection design shall be constructed according to the specifications set forth in N.J.A.C. 5:21-4.19.

#### 5:21-4.10 Pavement

(a) Street pavement thickness shall vary by street hierarchy, subgrade properties, and pavement type.

(b) Pavement design for rural, residential access, neighborhood, minor collector, and major collector streets shall conform to the specifications in N.J.A.C. 5:21-4.19.

#### 5:21-4.11 Street and site lighting (Reserved)

#### 5:21-4.12 Underground wiring

(a) All electric, telephone, television, and other communication facilities, both main and service lines servicing new developments, shall be provided by underground wiring within easements or dedicated public rights-of-way, installed in accordance with the prevailing standards and practices of the utility or other companies providing such services.

(b) Lots that abut existing easements or public rights-of-way, where overhead electric or telephone distribution supply lines and service connections have heretofore been installed, may be supplied with electric and telephone service from those overhead lines, but the service connections from the utilities' overhead lines shall be installed underground.

(c) Overhead lines may be permitted as an exception by the municipal approving authority in areas of severe geological conditions. The placement and alignment of the poles shall be designed to lessen the visual impact of overhead lines.

#### 5:21-4.13 Street and traffic signs

(a) Design and placement of traffic signs included in "Manual on Uniform Traffic Control Devices for Streets and Highways" shall follow the requirements specified in "Manual on Uniform Traffic Control Devices for Streets and Highways," published by the U.S. Department of Transportation and adopted by the N.J. Department of Transportation.

(b) At least two street name signs shall be placed at each four-way street intersection and one at each "T" intersection. Signs shall be placed so as not to obstruct sight distances and under light standards, if present, so that they are clearly visible. The design of street name signs should be: consistent, of a style appropriate to the community, of a uniform size and color, and erected in accordance with local standards.

(c) At signalized intersections, street signs shall be located on the overhead arm supporting the traffic signal, or otherwise suitably suspended over the intersection. Road-

way clearance shall be a minimum of 15 feet from the bottom of any sign or supporting equipment and the top of the paved surface.

**5:21-4.14 Parking: number of spaces**

(a) An adequate number of on-street and off-street parking spaces shall be required in all developments to accommodate residents and visitors. For projects containing dwelling units required by the New Jersey Uniform Construction Code's Barrier Free Subcode (N.J.A.C. 5:23-7) to be accessible, accessible parking spaces for people with disabilities shall be provided in accordance with the requirements of the Barrier Free Subcode and shall be considered part of the total number of required spaces.

(b) For residential developments, parking shall be provided, as set forth in Table 4.4 below. If applicant does not specify the number of bedrooms per unit, note "c" for each category in Table 4.4 shall apply for the parking requirement.

(c) Alternative parking standards to those shown in Table 4.4 shall be accepted if the applicant demonstrates these standards better reflect local conditions. Factors affecting minimum number of parking spaces include household characteristics, availability of mass transit, urban versus suburban location, and available off-site parking resources .

(d) Garage and driveway combinations shall be counted as follows:

1. Each garage car space shall be counted as 1.0 off-street parking space regardless of the dimensions of the driveway.
2. A one-car garage and driveway combination shall count as 2.0 off-street parking spaces, provided the driveway measures a minimum of 18 feet in length between the face of the garage door and the right-of-way.
3. A two-car garage and driveway combination shall count as 3.5 off-street parking spaces, provided a minimum parking width of 20 feet is provided for a minimum length of 18 feet as specified for a one-car garage and driveway combination.

(e) When housing is included in mixed-use development, a shared parking approach to the provision of parking shall be permitted.

(f) When, in the judgment of the local approving authority, on-street parking is available, then only that proportion of the parking requirement which is not available on the street shall be provided in off-street parking facilities. A length of 23 feet per on-street parking space shall be used in calculating the number of available on-street parking spaces.

**TABLE 4.4  
PARKING REQUIREMENTS FOR  
RESIDENTIAL LAND USES<sup>a</sup>**

| Housing unit type/size <sup>b</sup> | Parking requirement per dwelling unit  |
|-------------------------------------|--|
| Single-Family Detached              |  |
| 2 Bedroom                           | 1.5  |
| 3 Bedroom                           | 2.0  |
| 4 Bedroom                           | 2.5 <sup>c</sup>   |
| 5 Bedroom                           | 3.0  |
| Two Family (Duplex)                 | "Single-Family Detached" values shall apply to each unit.  |
| Garden Apartment                    |  |
| 1 Bedroom                           | 1.8  |
| 2 Bedroom                           | 2.0 <sup>c</sup>   |
| 3 Bedroom                           | 2.1  |
| Townhouse                           |  |
| 1 Bedroom                           | 1.8  |
| 2 Bedroom                           | 2.3 <sup>c</sup>   |
| 3 Bedroom                           | 2.4  |
| High Rise                           |  |
| 1 Bedroom                           | 0.8  |
| 2 Bedroom                           | 1.3 <sup>c</sup>   |
| 3 Bedroom                           | 1.9  |
| Mobile Home                         |  |
| 1 Bedroom                           | 1.8  |
| 2 Bedroom                           | 2.0 <sup>c</sup>   |
| Retirement Community                | Values shall be commensurate with the most appropriate housing unit type and size noted above that the retirement community resembles.               |
| Recreational Homes (owner occupied) | Values shall be commensurate with the most appropriate housing unit type and size noted above that the recreational homes (owner occupied) resemble. |
| Mid-Rise Apartment                  | "Garden Apartment" values shall apply  |
| Assisted living                     | 0.50   |

Notes: <sup>a</sup> When determination of the required number of parking spaces results in a fractional space for the entire development, any fraction of one-half or less may be disregarded, while a fraction in excess of one-half shall be counted as one parking space. <sup>b</sup> Requirements for attached units (apartment/condominium/ townhouse) include provisions for guest parking. <sup>c</sup> If applicant does not specify the number of bedrooms per unit, this off-street parking requirement shall apply.

Source: Modified and adapted from U.S. Department of Commerce, Bureau of the Census, Public Use File—New Jersey (cross-tabulation of vehicles by housing unit for units constructed 1975 to 1980).

Notes: