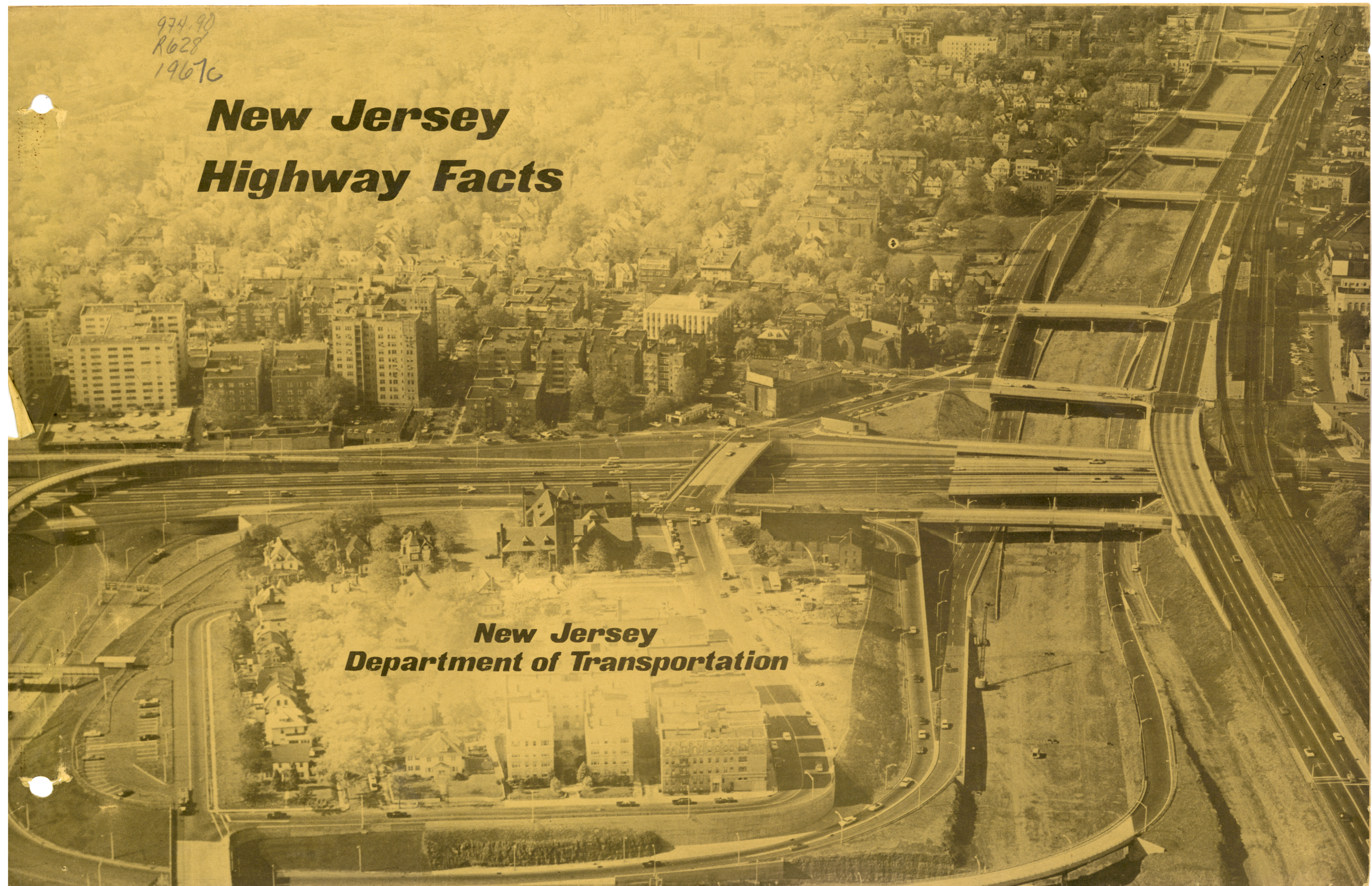


974.90  
R628  
1967C

974.90  
R628  
1967C

# ***New Jersey Highway Facts***

***New Jersey  
Department of Transportation***



.90  
R628  
1967c

In past years, it has not been uncommon for many persons to complain that they could not understand what the Highway Department was doing with "all the money" it had to solve New Jersey's highway problems. At the same time, the Department has felt that few persons had any real appreciation of the magnitude of the task it had.

In the hope of achieving a better mutual understanding, the Department of Transportation has been bringing the facts to the attention of the public. We believe that this latest compilation of facts may help put the highway situation in a clearer perspective so that the public can better decide what kind of program it wishes to support in the coming years, what it will require in terms of finances and how long it can take to execute.

In order to understand why there is a continuing demand for more highways, despite a massive interstate construction program, it is necessary to consider the growth factors which, while not directly related to the funding and operation of the Department of Transportation, have a profound effect on its effort to provide the highway facilities which are required by the public. These factors are:

Increasing population and land consumption

Increasing motor vehicle registration

Increasing motor vehicle travel

These growth factors are presented in the first section.

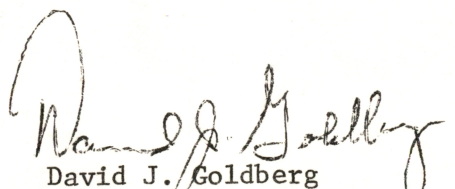
A direct factor bearing on the Department's ability to fulfill its mission is the money appropriated to it. The sources and disposition of these funds, and their effect on construction of primary-urban highways is presented in the second section.

The third section presents the status and costs for construction of the New Jersey portion of the National Interstate and Defense Freeway system.

Section four includes charts and information which delineate the present status and costs of state freeways.

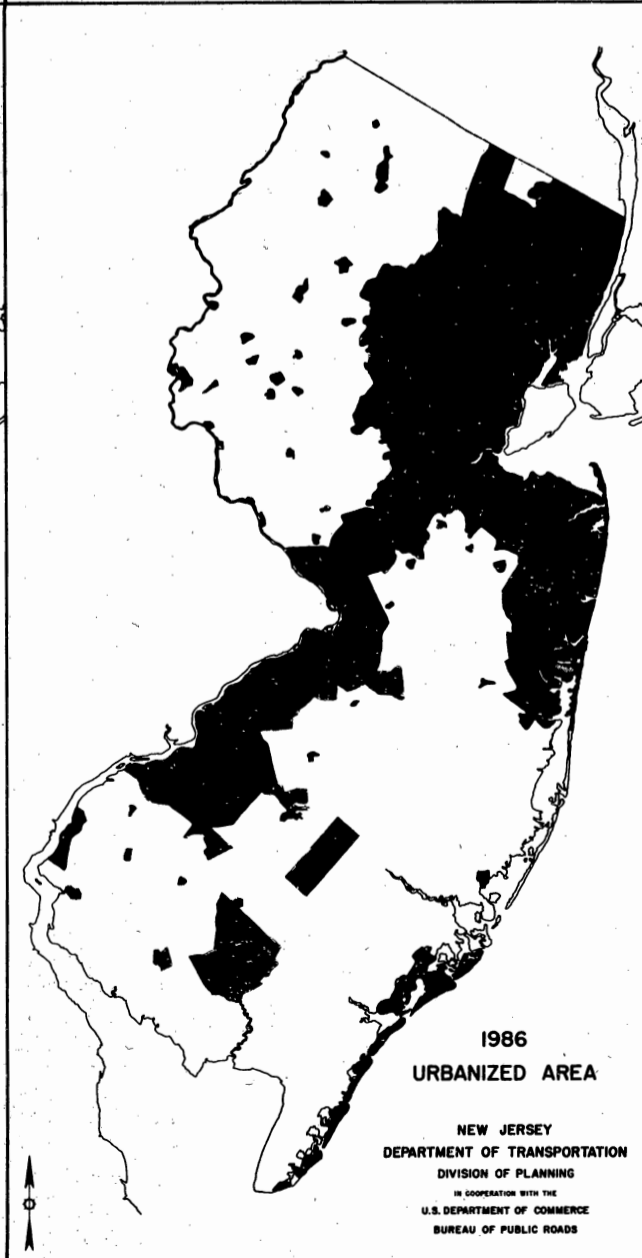
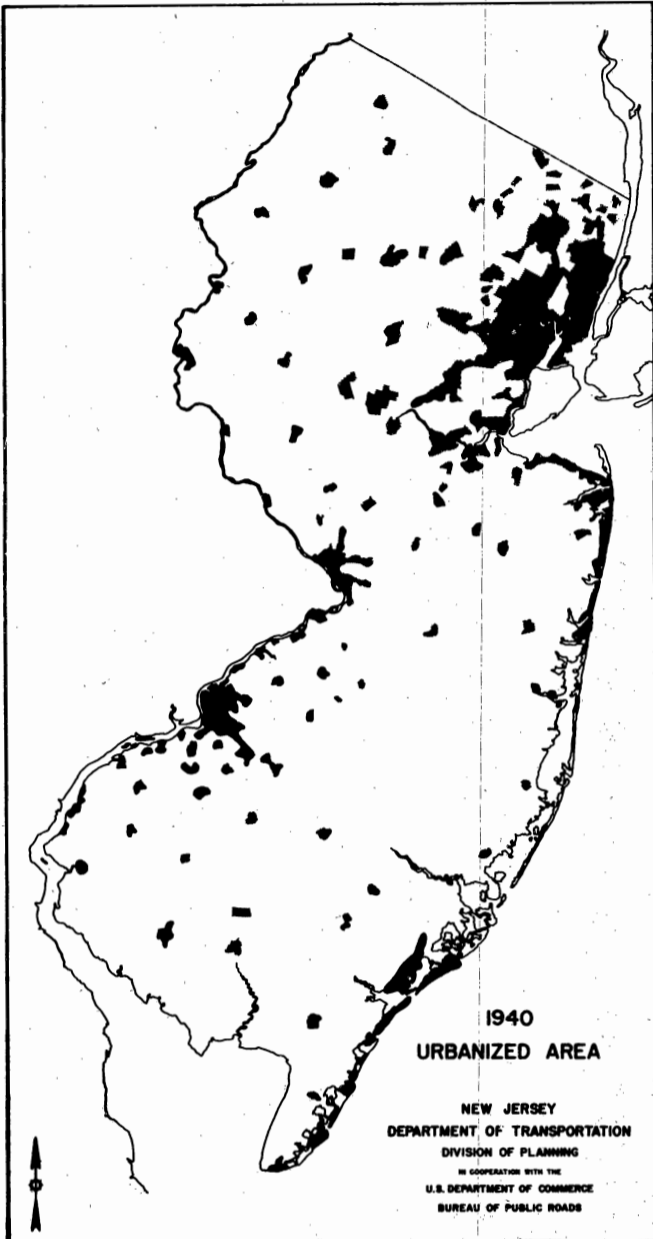
Section five includes highways which have been legislated in recent years and their status. Also included is information on an important method of increasing the capacity and safety of existing highways -- dualization. The information includes mileage completed, under way and in the planning stage.

I hope the information contained herein will be of assistance to all persons genuinely interested in resolving our highway problems. It is intended to help the public focus on the problem so that the necessary common agreements can be reached and a meaningful highway program for the future can be charted.

  
David J. Goldberg  
Commissioner of Transportation



# ***Growth of New Jersey***



# COMPARATIVE INCREASE

1950-1965

VEHICLE MILEAGE

107%

M.V. REGISTRATION

88%

POPULATION

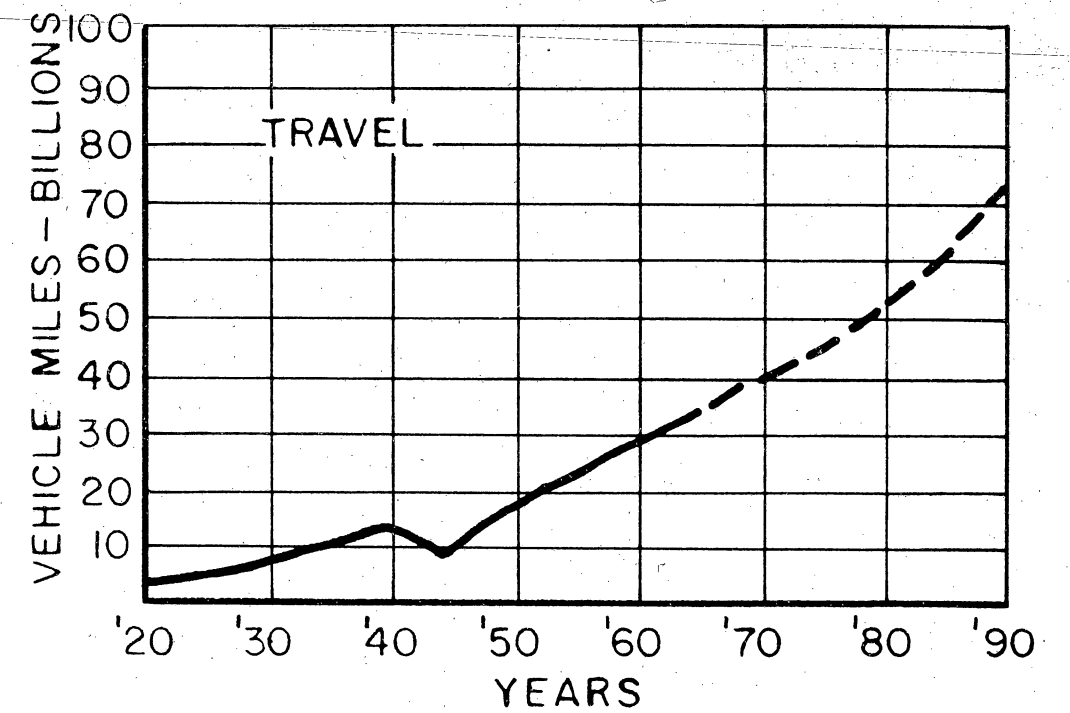
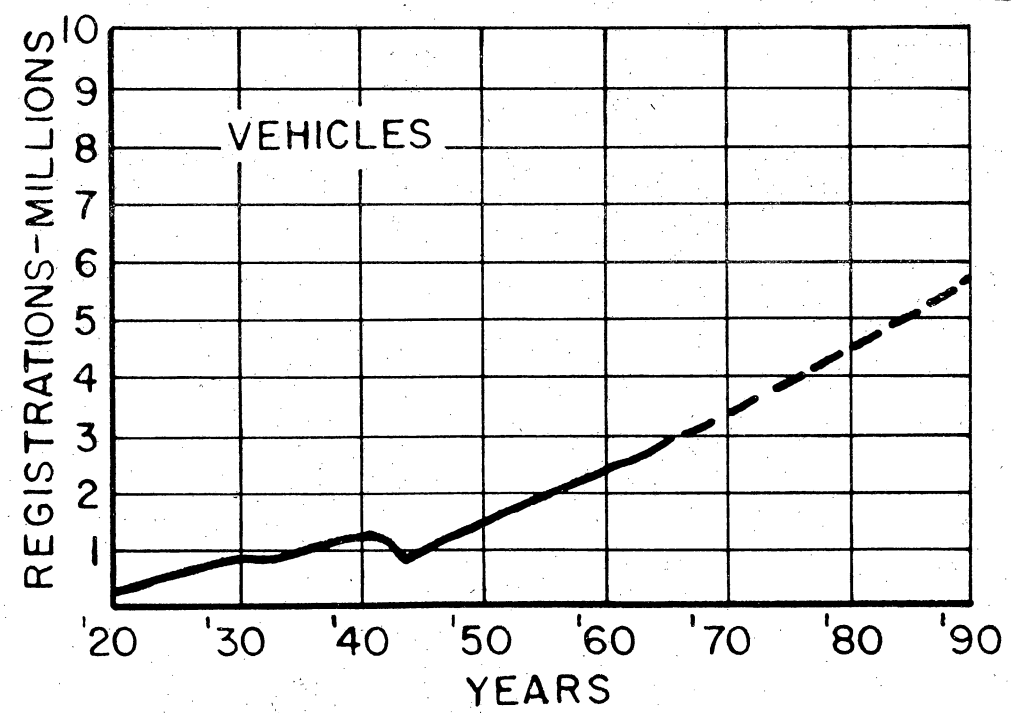
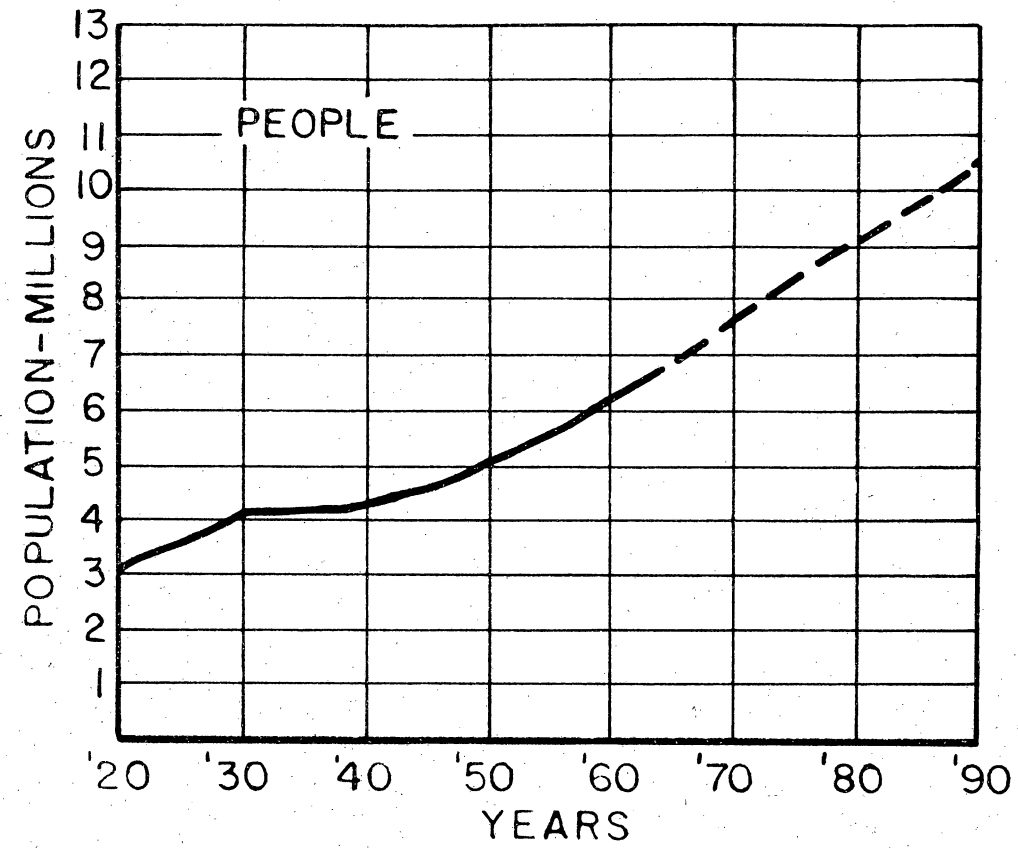
42%

HIGHWAY CAPACITY

28%

	<u>LANE MILES</u>	<u>TOTAL SYSTEM</u>
1950	4,590	1,722
1965	5,884	1,943
(1966)	(5,998)	(1,957)

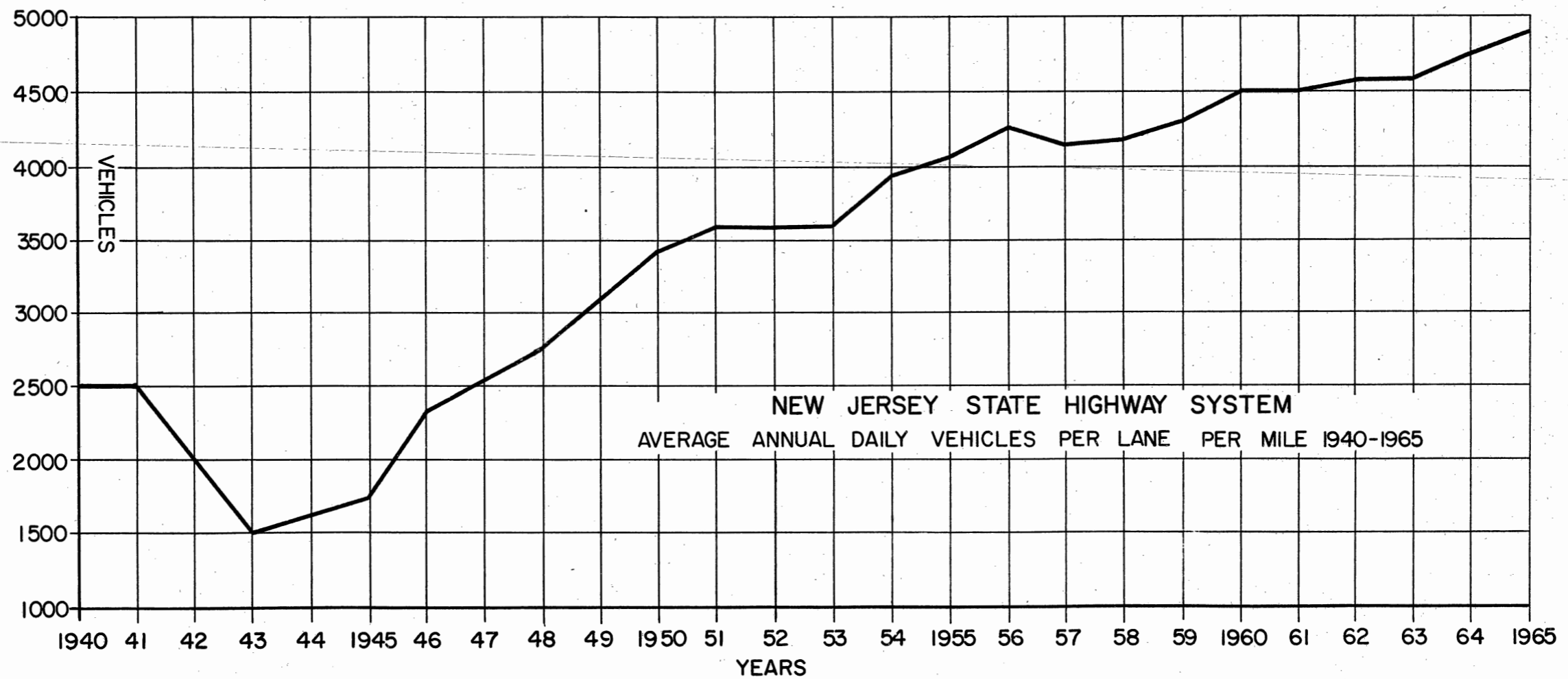
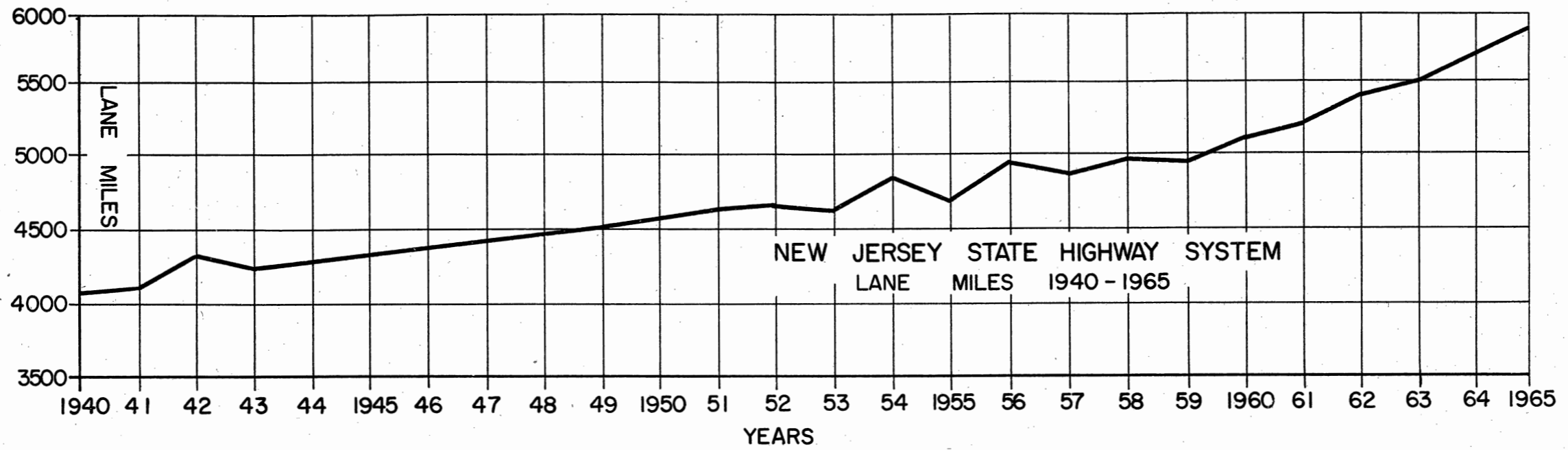
# GROWTH TRENDS

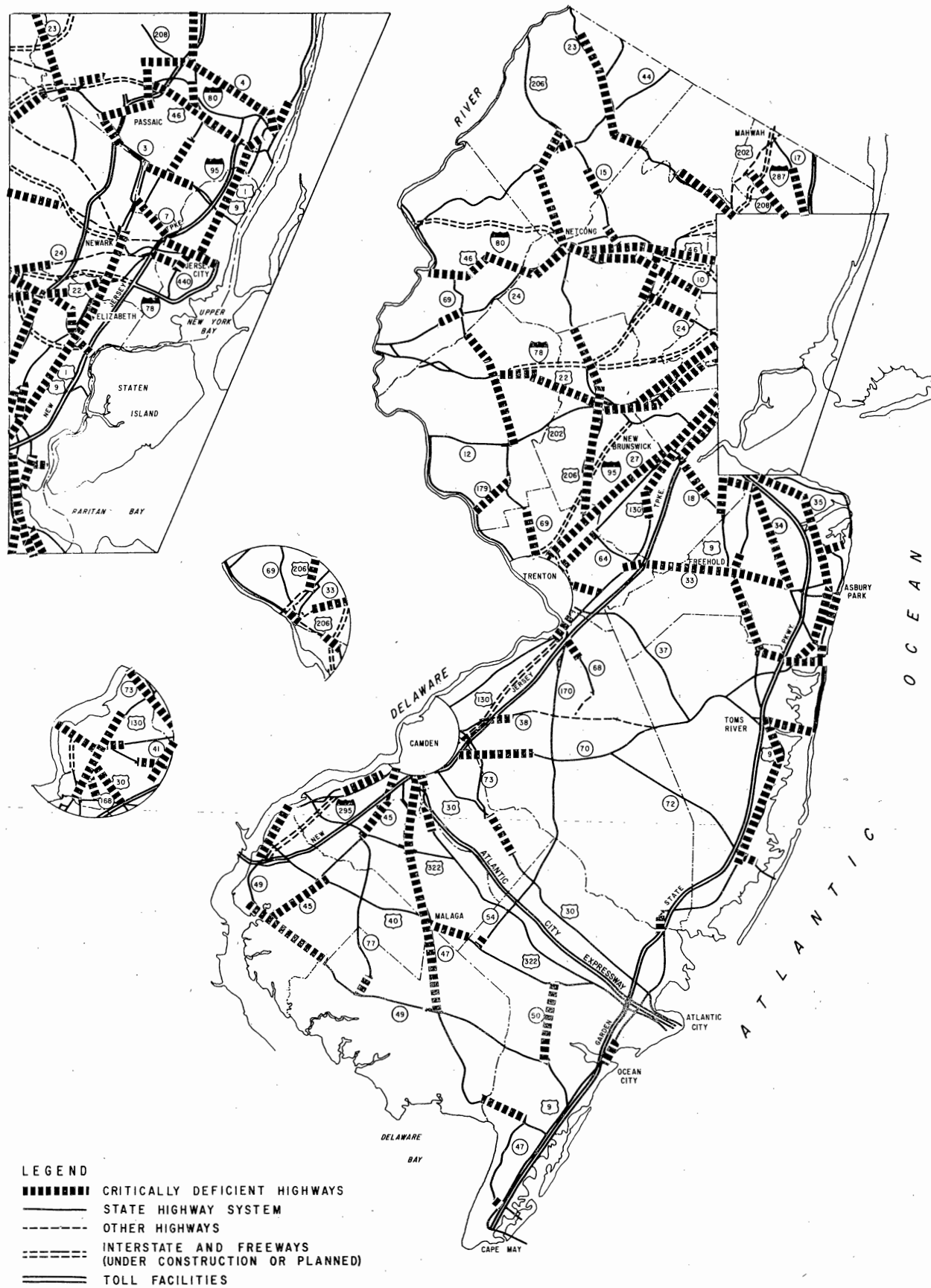


NUMBER OF VEHICLES PER MILE OF ROAD<sup>(1)</sup>  
 New Jersey, Study Area, and the United States  
 1955-1964

<u>YEAR</u>	<u>NEW JERSEY</u>	<u>CONNECTICUT</u>	<u>DELAWARE</u>	<u>MARYLAND</u>	<u>NEW YORK</u>	<u>PENNSYLVANIA</u>	<u>RHODE ISLAND</u>	<u>STUDY AREA</u>	<u>UNITED STATES</u>
1955	70.9	59.0	34.8	46.5	44.5	35.2	74.3	45.0	18.4
1956	72.6	61.3	36.5	48.5	45.7	36.5	73.5	46.4	19.0
1957	73.8	63.5	37.7	49.1	45.3	37.0	76.9	46.7	19.4
1958	76.9	62.7	38.4	48.2	46.2	37.4	77.6	47.4	19.6
1959	74.4	63.5	39.8	50.0	47.3	38.4	79.3	48.3	20.3
1960	77.1	66.2	41.5	51.8	47.6	39.2	81.1	47.8	20.8
1961	78.4	67.6	42.8	52.5	48.6	40.0	79.7	50.5	21.2
1962	80.7	70.2	44.6	55.2	50.1	40.8	82.0	51.9	21.9
1963	84.7	73.8	46.8	57.6	54.7	42.0	82.9	54.9	22.8
1964	88.3	77.0	49.2	60.4	56.3	43.4	86.4	56.9	23.6

(1) Excludes motorcycles.

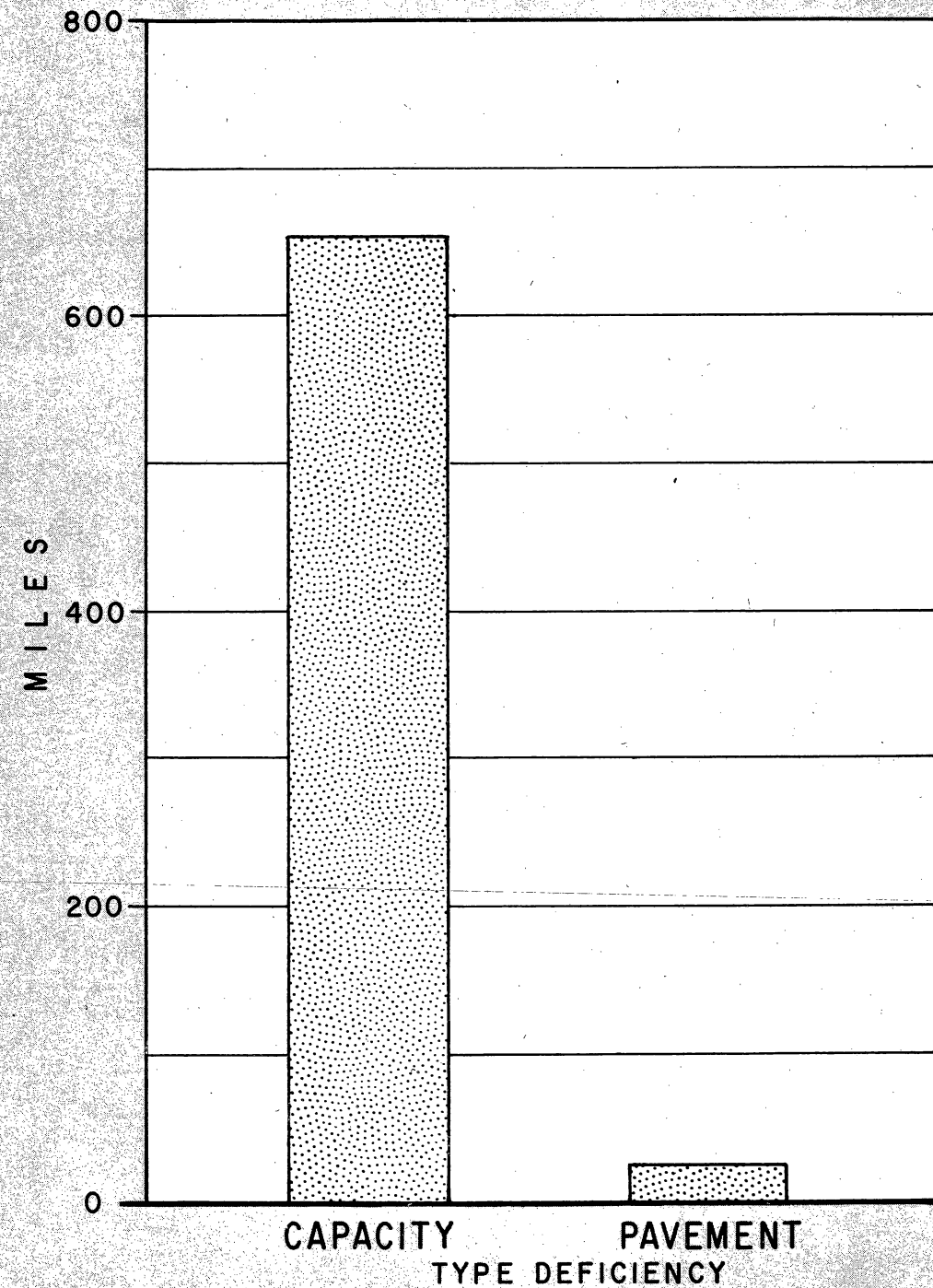




- LEGEND**
- CRITICALLY DEFICIENT HIGHWAYS
  - STATE HIGHWAY SYSTEM
  - ..... OTHER HIGHWAYS
  - ==== INTERSTATE AND FREEWAYS (UNDER CONSTRUCTION OR PLANNED)
  - TOLL FACILITIES

**EXISTING CRITICAL CONSTRUCTION NEEDS  
NEW JERSEY**

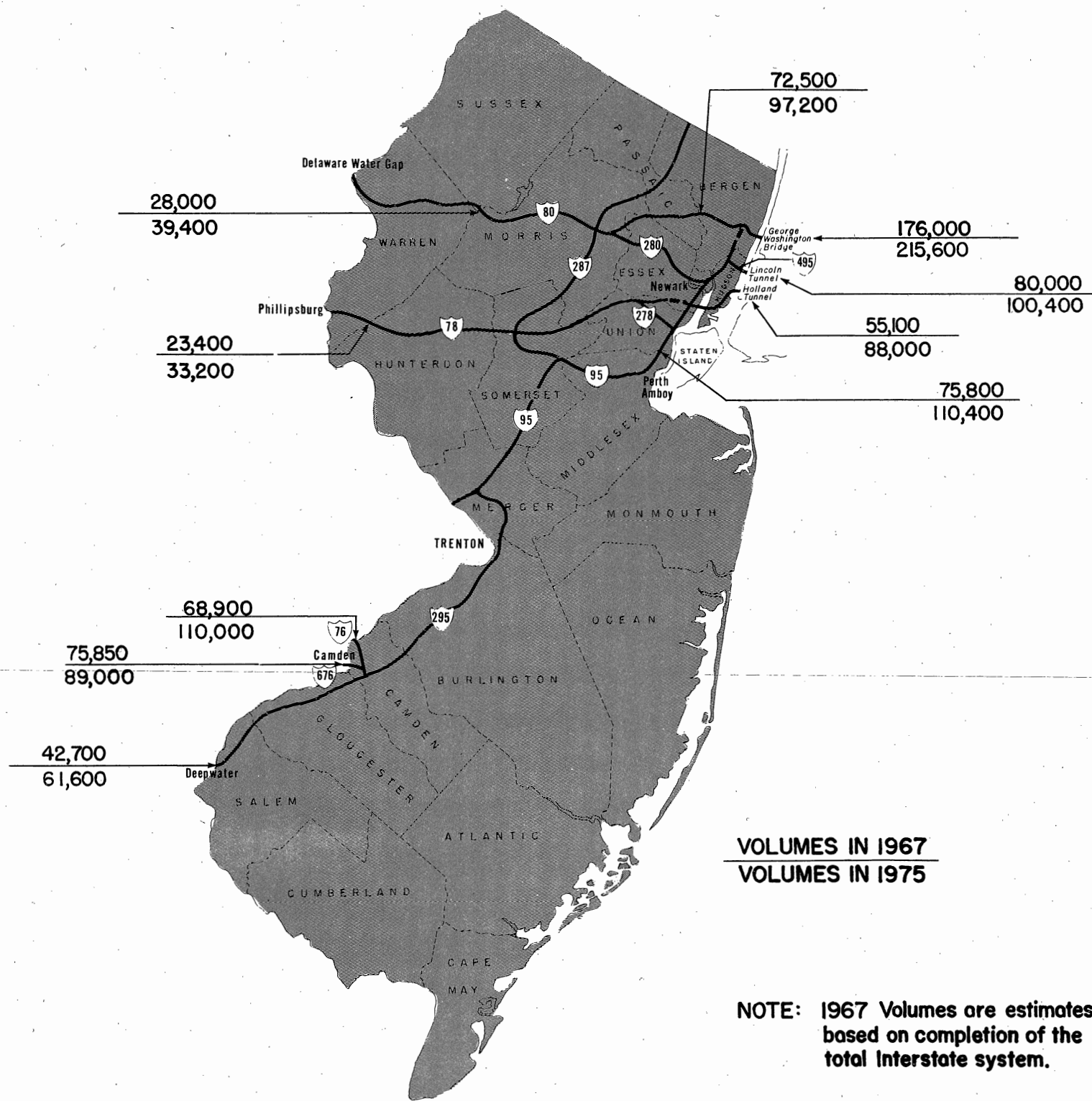
A recent report of the Department of Transportation made by Wilbur Smith and Associates cataloged the sections of the state highway system which are now inadequate to handle existing traffic demands. This map sets forth the location of these critically deficient highways.



**BACKLOG MILES BY  
TYPE DEFICIENCY  
STATE HIGHWAYS  
NEW JERSEY**

The estimated cost of upgrading the critically deficient highways to accommodate existing traffic is set forth on this chart.

This estimate does not take into consideration the fact that traffic is increasing on New Jersey highways at the rate of 5% per year or the cost of providing entirely new roads in areas, such as Tocks Island, which do not have a suitable existing road network.



**VOLUMES IN 1967**  
**VOLUMES IN 1975**

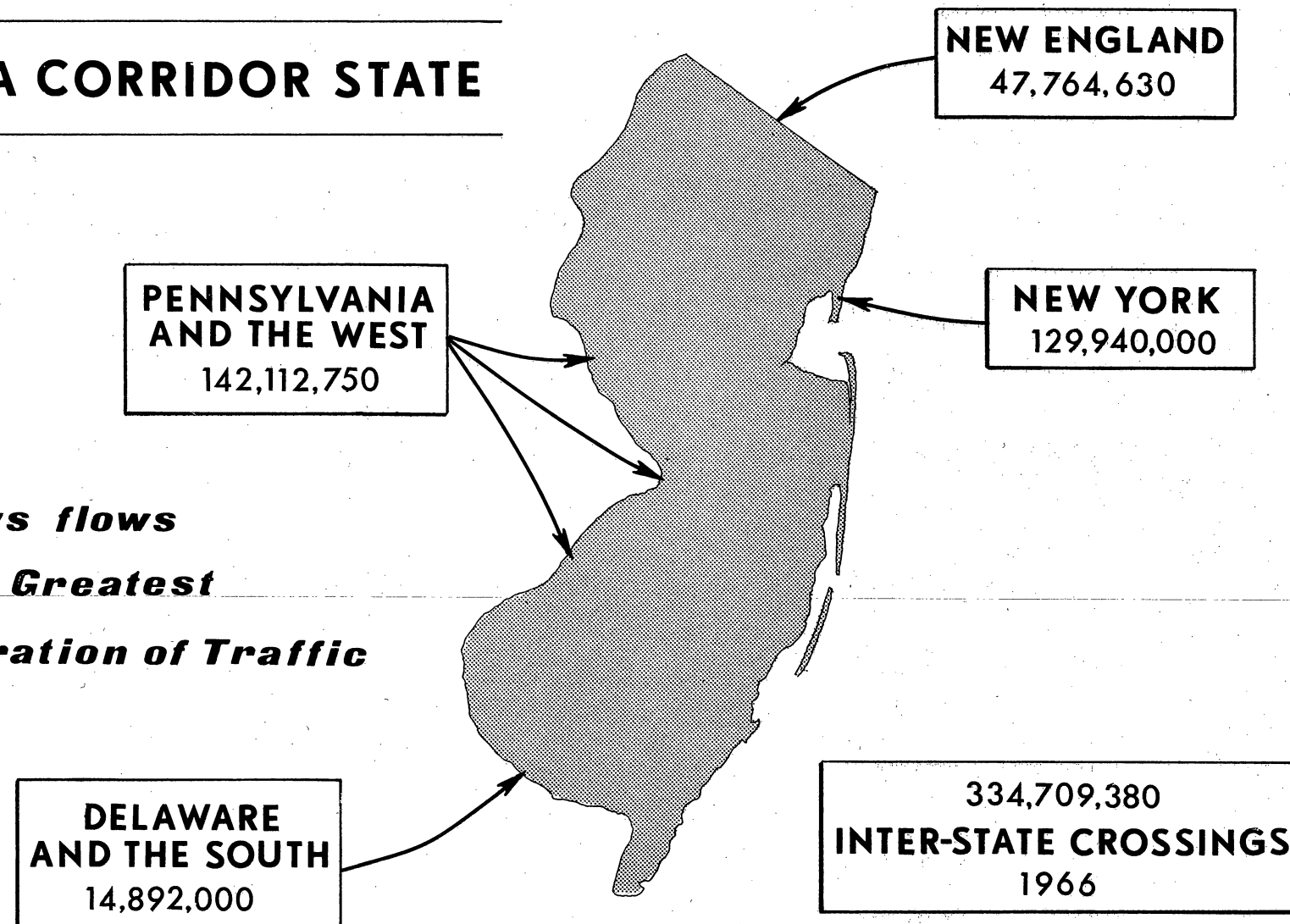
**NOTE: 1967 Volumes are estimates based on completion of the total Interstate system.**

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**NEW JERSEY IS A CORRIDOR STATE**

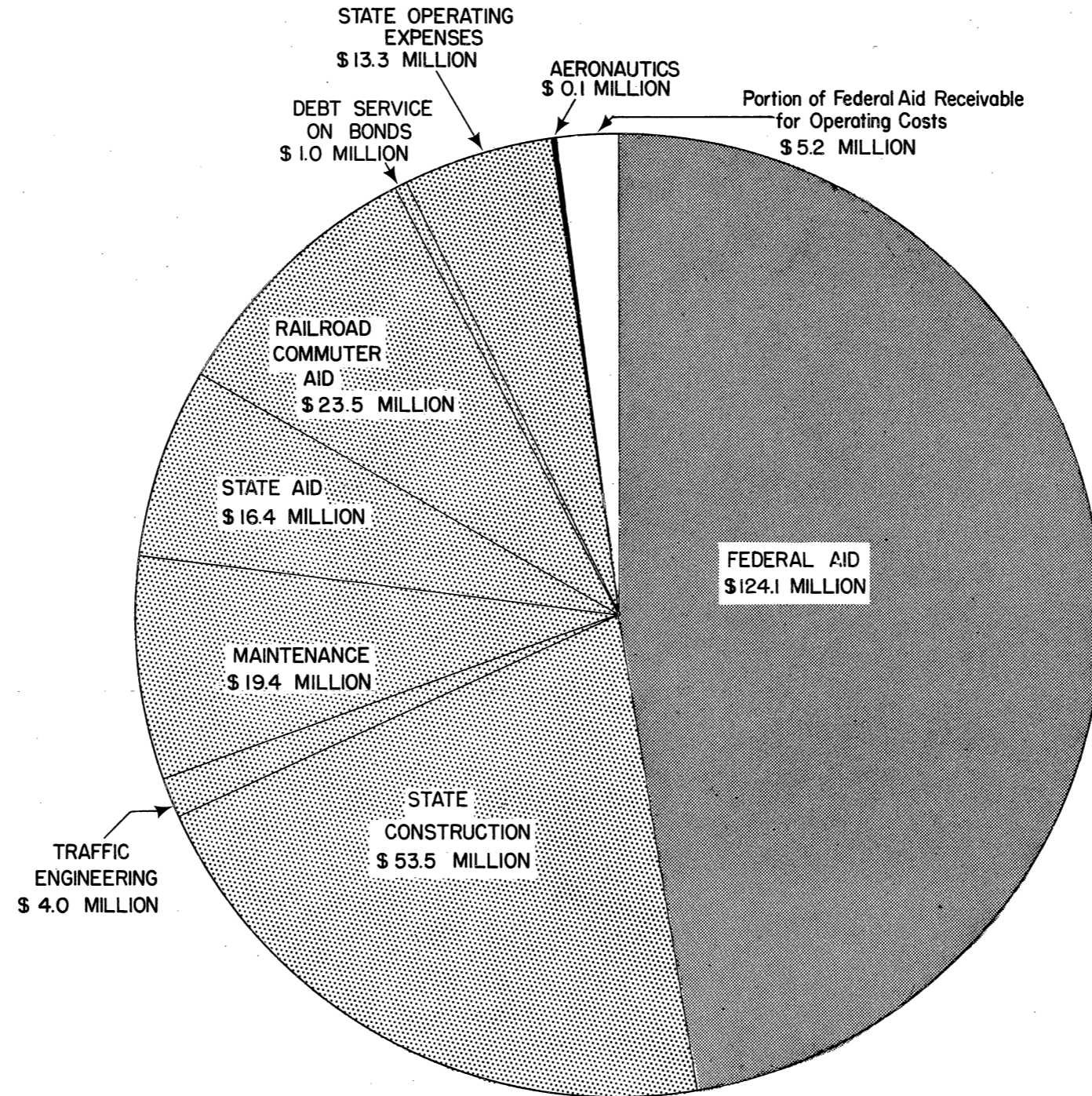
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*Over its Highways flows  
the World's Greatest  
Concentration of Traffic*



# ***Financial Data***

**NEW JERSEY DEPARTMENT OF TRANSPORTATION  
BUDGET FOR 1967-68**



BALANCES

Federal-State (50-50) Appropriations

One Hundred per cent State Appropriations

PROJECTED TO JUNE 30, 1968

	<u>Federal</u>	<u>State</u>	<u>Total</u>
Federal-State Balance	\$37,623,033.17	\$36,946,268.20	\$74,569,301.37
100% State Balance	<u>\$-----</u>	<u>\$13,883,481.57</u>	<u>\$13,883,481.57</u>
Combined Balances (Oct. 31, 1967)	\$37,623,033.17	\$50,829,749.77	\$88,452,782.94

PROJECTED DEDUCTIONS FROM COMBINED BALANCE

Federal-state and 100% State

From November 14 to June 30, 1968

Construction Program Obligations	\$69,360,000.00	
Right of Way Obligations	\$17,675,000.00	<u>\$87,035,000.00</u>
PROJECTED BALANCE, JUNE 30, 1968		\$ 1,417,782.94

TOTAL OF STATE HIGHWAY CONSTRUCTION BY COUNTY

EXPENDITURES AND COMMITMENTS

Construction Totals

	<u>1917-1954</u>	<u>1954-1962</u>	<u>1962-1967</u>
Atlantic	\$ 18,258,060.51	\$ 7,933,462.57	\$ 1,804,813.87
Bergen	76,163,783.36	143,868,778.49	59,265,586.37
Burlington	19,514,348.84	17,766,338.15	24,962,149.65
Camden	39,110,222.50	60,557,474.28	36,339,660.08
Cape May	10,777,888.99	1,842,237.32	86,270.87
Cumberland	5,560,736.93	755,164.53	11,623,328.20
Essex	66,186,936.36	34,225,952.22	180,210,738.59
Gloucester	26,071,200.45	14,470,978.32	13,469,239.83
Hudson	62,171,518.38	9,105,357.92	21,222,586.42
Hunterdon	18,281,042.35	27,071,536.43	34,931,371.71
Mercer	29,585,981.12	17,981,592.24	13,200,150.35
Middlesex	68,076,499.21	58,718,789.01	23,705,479.21
Monmouth	25,507,974.37	10,719,601.40	44,254,092.86
Morris	21,727,829.44	59,060,206.78	55,465,155.64
Ocean	22,647,009.89	18,934,304.81	12,803,799.73
Passaic	25,801,127.29	42,694,912.41	65,462,525.24
Salem	5,865,324.56	4,214,420.12	9,076,519.98
Somerset	18,270,896.70	35,038,517.22	60,020,475.82
Sussex	7,152,521.30	2,907,754.96	1,536,438.95
Union	48,954,654.66	15,918,913.67	70,423,166.74
Warren	15,069,816.01	7,096,911.39	12,830,375.38
	<u>\$ 630,755,373.22</u>	<u>\$ 590,883,204.24*</u>	<u>\$ 752,693,925.49*</u>

\* Includes Interstate Funds

TOTAL OF STATE HIGHWAY CONSTRUCTION BY COUNTY

EXPENDITURES AND COMMITMENTS

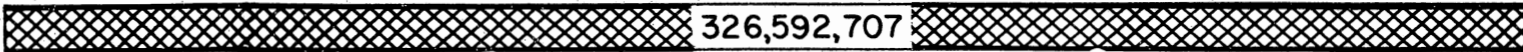
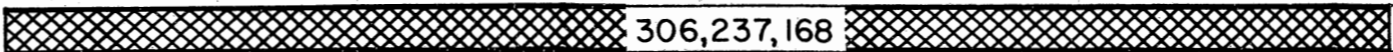


Construction Totals

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Essex	66,186,936.36	34,225,952.22	180,210,738.59
Gloucester	26,071,200.45	14,470,978.32	13,469,239.83
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Hunterdon	18,281,042.35	27,071,536.43	34,931,371.71
Mercer	29,585,981.12	17,981,592.24	13,200,150.35
Middlesex	68,076,499.21	58,718,789.01	23,705,479.21
Monmouth	25,507,974.37	10,719,601.40	44,254,092.86
Morris	21,727,829.44	59,060,206.78	55,465,155.64
Ocean	22,647,009.89	18,934,304.81	12,803,799.73
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Sussex	7,152,521.30	2,907,754.96	1,536,438.95
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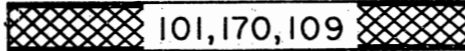
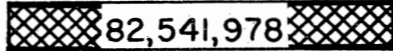


\* Includes State And Interstate Funds

NEW JERSEY DEPARTMENT OF TRANSPORTATION  
HIGHWAY CONSTRUCTION PROGRAM FUNDS  
FISCAL 1957-1966

PRIMARY & URBAN

FUNDS AVAILABLE		326,592,707
EXPENDED OR COMMITTED		306,237,168
DIVERTED, OTHER PURPOSES		1,916,986
BALANCE		18,438,553

NON-FEDERAL

FUNDS AVAILABLE		101,170,109
EXPENDED OR COMMITTED		82,541,978
DIVERTED, OTHER PURPOSES		16,446,876
BALANCE		2,181,255

TOTAL AVAILABLE	427,762,816
TOTAL EXPENDED OR COMMITTED	388,779,146
TOTAL DIVERTED, OTHER PURPOSES	18,363,862
BALANCE	20,619,808

ANALYSIS OF CONSTRUCTION PROGRAM

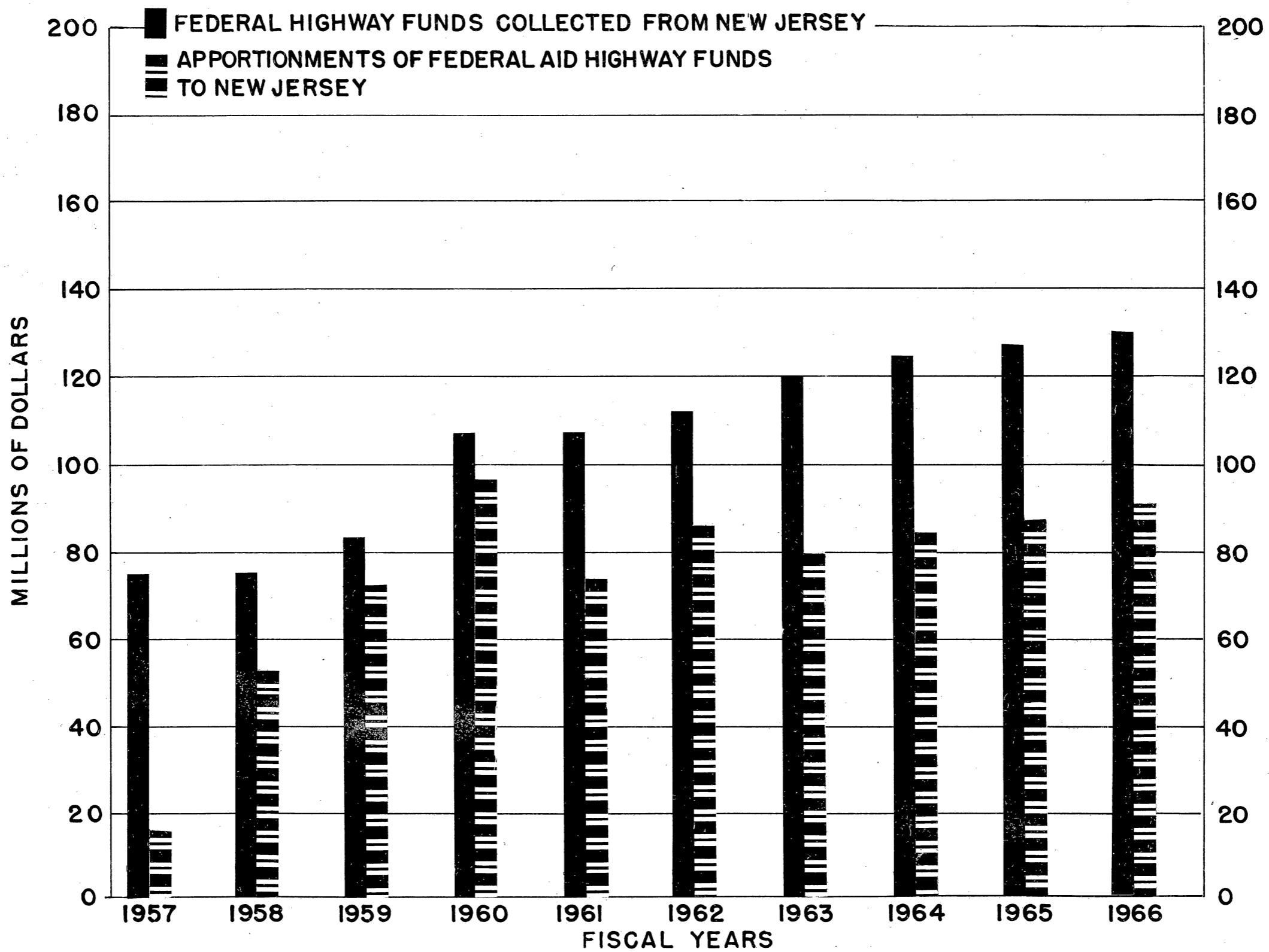
FUNDS

AS OF JUNE 30, 1967

Year	Appropriation	Transfers Within Program	Other Revenue	Available	EXPENDITURES			BALANCE OF COMMITMENTS			TRANSFERS FROM PROGRAM						Transfer to Interstate	To State Treasurer	Balance of Approp.
					Construction	Right of Way	Preliminary Engineering	Construction	Right of Way	Preliminary Engineering	Admins. & Operating 610	Construction Institut. Rds 611	Construction Operating 612	Federal Aid Secondary 613	Railroad Transport. 630	Flood Damage			
PRIMARY AND URBAN																			
1956-57	21,821,101	255,623	0	22,076,724	18,748,537	1,980,862	948,531	0	88,309	0	238,943	0	35,442	36,100	0	0	0	0	0
1957-58	30,074,594	2,333,019	4,833(a)	32,412,446	27,618,994	3,900,521	757,011	9,060	126,860	0	0	0	0	0	0	0	0	0	0
1958-59	31,290,423	( 2,994,954)	61,386(b)	28,356,855	23,534,703	2,529,531	1,985,592	0	252,811	0	0	0	54,218	0	0	0	0	0	0
1959-60	28,574,136	( 6,962,665)	61,500(a)	21,672,971	15,860,095	3,592,053	2,053,236	13,640	77,847	0	0	0	3,000	0	0	73,100	0	0	0
1960-61	27,819,914	3,434,635	3,219,700(c)	34,474,249	26,602,332	6,668,028	( 791,768)	303,408	612,281	951,768	0	0	128,200	0	0	0	0	0	0
1961-62	29,614,778	13,377,055	1,478(a)	42,993,311	17,315,003	12,306,969	1,368,508	1,237,737	9,757,559	0	0	0	200,000	0	0	0	0	0	807,535
1962-63	33,516,656	17,676,481	0	51,193,137	25,649,741	6,080,716	2,902,649	6,570,031	7,998,072	1,013,076	0	0	359,816	0	0	0	0	0	619,036
1963-64	33,125,764	(10,543,233)	0	22,582,531	4,077,239	5,648,070	1,255,969	9,313,273	1,496,105	656,748	0	0	0	0	0	0	0	0	135,127
1964-65	54,430,850	(22,215,198)	2,000,000(d)	34,215,652	9,151,691	9,051,922	754,344	3,122,378	2,460,678	1,081,425	0	0	379,226	0	0	0	0	0	8,213,988
1965-66	31,549,120	5,065,711	0	36,614,831	4,266,142	4,209,545	1,148,211	11,815,632	4,809,705	1,293,788	196,133	62,640	150,168	0	0	0	0	0	8,662,867
	321,817,336	( 573,526)	5,348,897	326,592,707	172,824,477	55,968,217	12,382,283	32,385,159	27,680,227	4,996,805	435,076	62,640	1,310,070	36,100	0	73,100	0	0	18,438,553

NON FEDERAL																			
1956-57	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1957-58	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1958-59	12,687,134	( 1,878,183)	2,244(a)	10,811,195	6,208,650	2,364,873	1,301,884	129,761	336,027	0	0	0	470,000	0	0	0	0	0	0
1959-60	20,384,537	289,614	351,441(e)	21,025,592	12,010,932	3,470,372	1,134,260	750	1,677,073	0	0	0	227,900	0	0	0	2,504,305	0	0
1960-61	13,119,810	( 2,464,351)	24,299(a)	10,679,758	7,986,136	1,649,485	783,023	54,468	56,646	0	0	0	150,000	0	0	0	0	0	0
1961-62	10,700,272	( 3,021,804)	939,364(f)	8,617,832	5,291,406	2,149,618	374,118	28,779	736,711	0	0	0	0	37,200	0	0	0	0	0
1962-63	102,642	1,426,007	513,114(g)	10,141,763	6,162,134	1,189,700	705,135	419,137	1,139,668	20,045	130,883	0	375,061	0	0	0	0	0	0
1963-64	9,500	8,291,088	0	17,880,588	5,722,418	887,829	1,918,005	300,011	182,621	120,901	0	0	1,192,246	9,325	3,000,000	0	0	4,547,232	0
1964-65	6,638,059	508,278	0	7,146,337	3,948,709	843,579	491,545	313,948	78,971	410,161	0	0	953,466	5,803	0	0	0	0	100,155
1965-66	17,033,666	( 2,166,622)	0	14,867,044	3,430,762	435,673	1,387,944	781,471	2,968,218	938,421	0	0	2,843,455	0	0	0	0	0	2,081,100
	98,355,620	984,027	1,830,462	101,170,109	50,761,147	12,991,129	8,095,914	2,028,325	7,175,935	1,489,528	130,883	0	6,212,128	52,328	3,000,000	0	2,504,305	4,547,232	2,181,255

(a) Receipts from Accounts Receivable going into the Program  
 (b) \$57,440 from Scudder Falls Bridge and \$3,946 Receipts, see note (a)  
 (c) \$3,200,000 from 612 Construction Operating and \$19,700 Receipts, see note (a)  
 (d) \$2,000,000 from 612 Construction Operating  
 (e) \$348,365 from City of Bayonne and Jersey City and \$3,076 Receipts, see note (a)  
 (f) \$935,865 from 612 Construction Operating and \$3,499 Receipts, see note (a)  
 (g) \$410,500 from Atlantic City Expressway and \$102,614 from Secondary and Feeder Roads



### DISPOSITION OF STATE HIGHWAY USER TAXES

■ HIGHWAY PURPOSES  
▨ NON-HIGHWAY PURPOSES

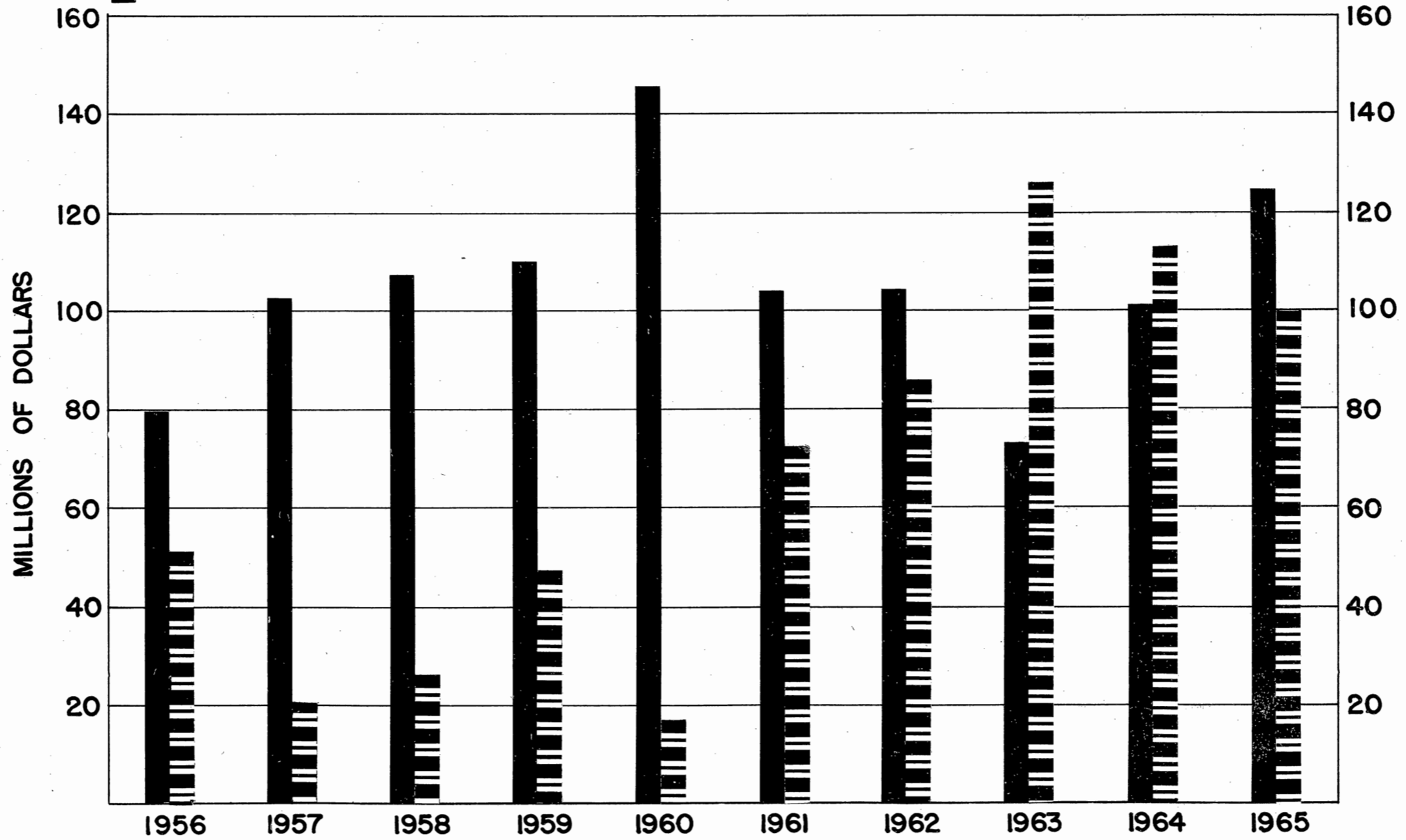


TABLE  
SHOWING DISPOSITION OF RECEIPTS\* FROM  
STATE IMPOSTS ON HIGHWAY USERS

<u>YEAR</u>	<u>NET FUNDS DISTRIBUTED</u>	<u>NEW JERSEY HIGHWAY PURPOSES</u>	<u>NEW JERSEY NON-HIGHWAY PURPOSES</u>	<u>50 STATE AND DISTRICT OF COLUMBIA TOTAL NON-HIGHWAY PURPOSES</u>
1965	224,090,000	124,599,000	99,491,000	654,991,000
1964	214,934,000	101,369,000	113,565,000	621,534,000
1963	199,190,000	73,206,000	125,984,000	502,213,000
1962	189,858,000	104,167,000	85,691,000	441,168,000
1961	177,125,000	104,017,000	73,108,000	424,431,000
1960	162,334,000	145,494,000	16,840,000	376,852,000
1959	157,532,000	110,011,000	47,521,000	451,331,000
1958	132,804,000	106,694,000	26,110,000	303,326,000
1957	122,722,000	102,415,000	20,307,000	304,966,000
1956	<u>129,939,000</u>	<u>79,021,000</u>	<u>50,918,000</u>	<u>324,888,000</u>
TOTAL	<u>1,710,528,000</u>	<u>1,050,993,000</u>	<u>659,535,000</u>	<u>4,405,700,000</u>

This chart indicates that nationwide nearly \$4½ billion in highway user receipts were diverted to non-highway uses during the decade from 1956 to 1965. Of this total, New Jersey alone was responsible for more than 15% of this amount – nearly \$650 million.

\*Receipts include Motor Fuel Taxes, Vehicle Registration and Miscellaneous Items.

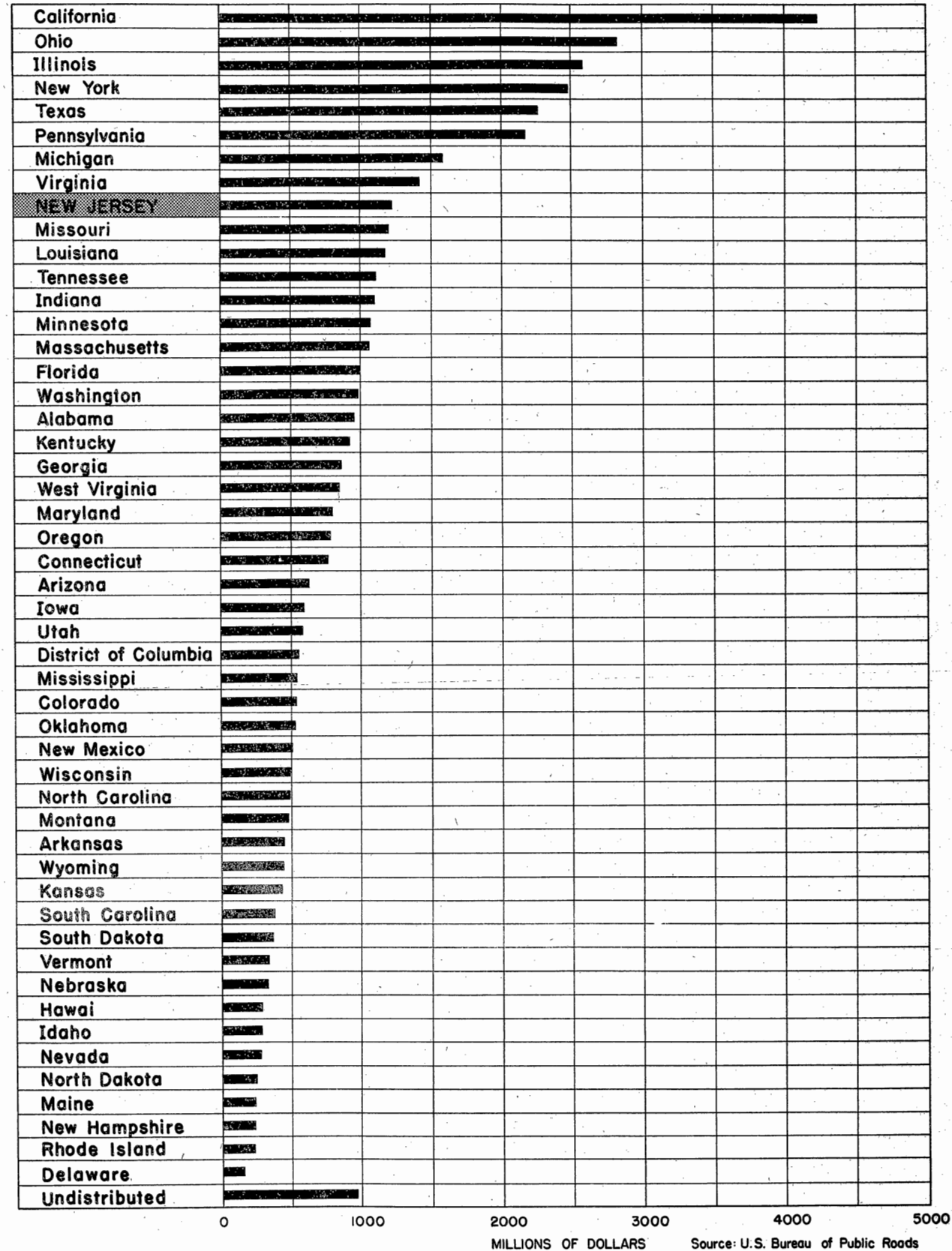
COMBINED STATE AND FEDERAL FUNDS FOR  
STATE HIGHWAYS IN NEW JERSEY

FOR THE FISCAL YEAR ENDING <u>JUNE 30</u>	STATE <u>APPROPRIATIONS</u> (thousands of dollars)	FEDERAL-AID <sup>(1)</sup> <u>APPROPRIATIONS</u>	<u>TOTAL</u>
1957	\$42,254	\$39,459	\$ 81,713
1958	39,641	51,669	91,310
1959	58,949	62,671	121,620
1960	61,633	95,841	157,474
1961	58,871	72,118	130,989
1962	60,281	84,653	144,934
1963	59,852	78,489	138,341
1964	63,307	83,755	147,062
1965	67,894	86,419	154,313
1966	76,603	89,588	166,191
1967	90,555	94,866	185,421

(1) Excludes Federal-aid Secondary Funds.

# INTERSTATE SYSTEM

## TOTAL COSTS (1965 ESTIMATE)



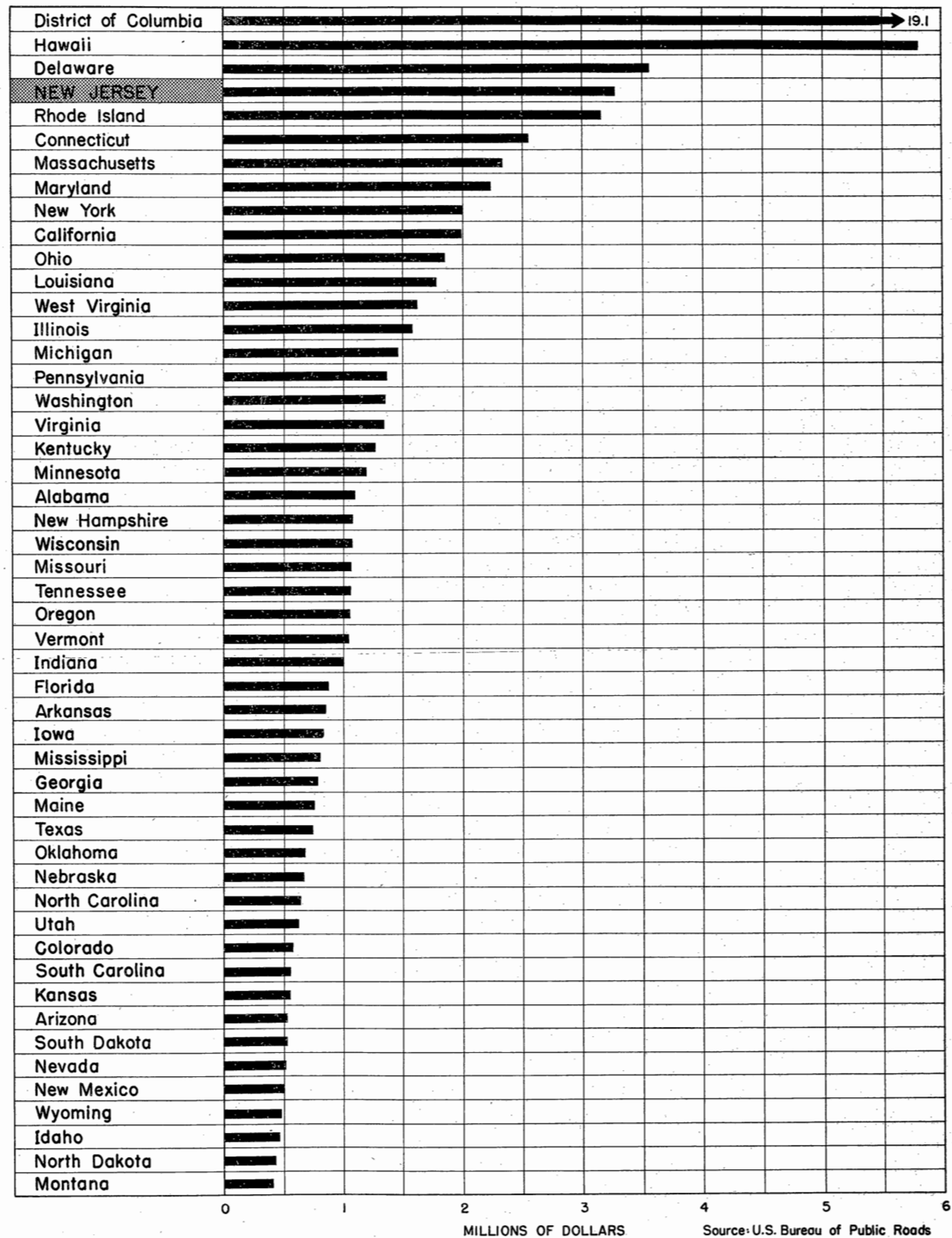
As indicated, these cost estimates were made in 1965. Updated estimates are now being completed and will be available in 1968. The new estimates will show substantial increases in the cost of building the Interstate System.

Source: U.S. Bureau of Public Roads  
Quarterly Report on Progress No. 34-42

# INTERSTATE SYSTEM

## CONSTRUCTION COSTS PER MILE

(EXCLUSIVE OF TOLL FACILITIES)  
1965 ESTIMATE



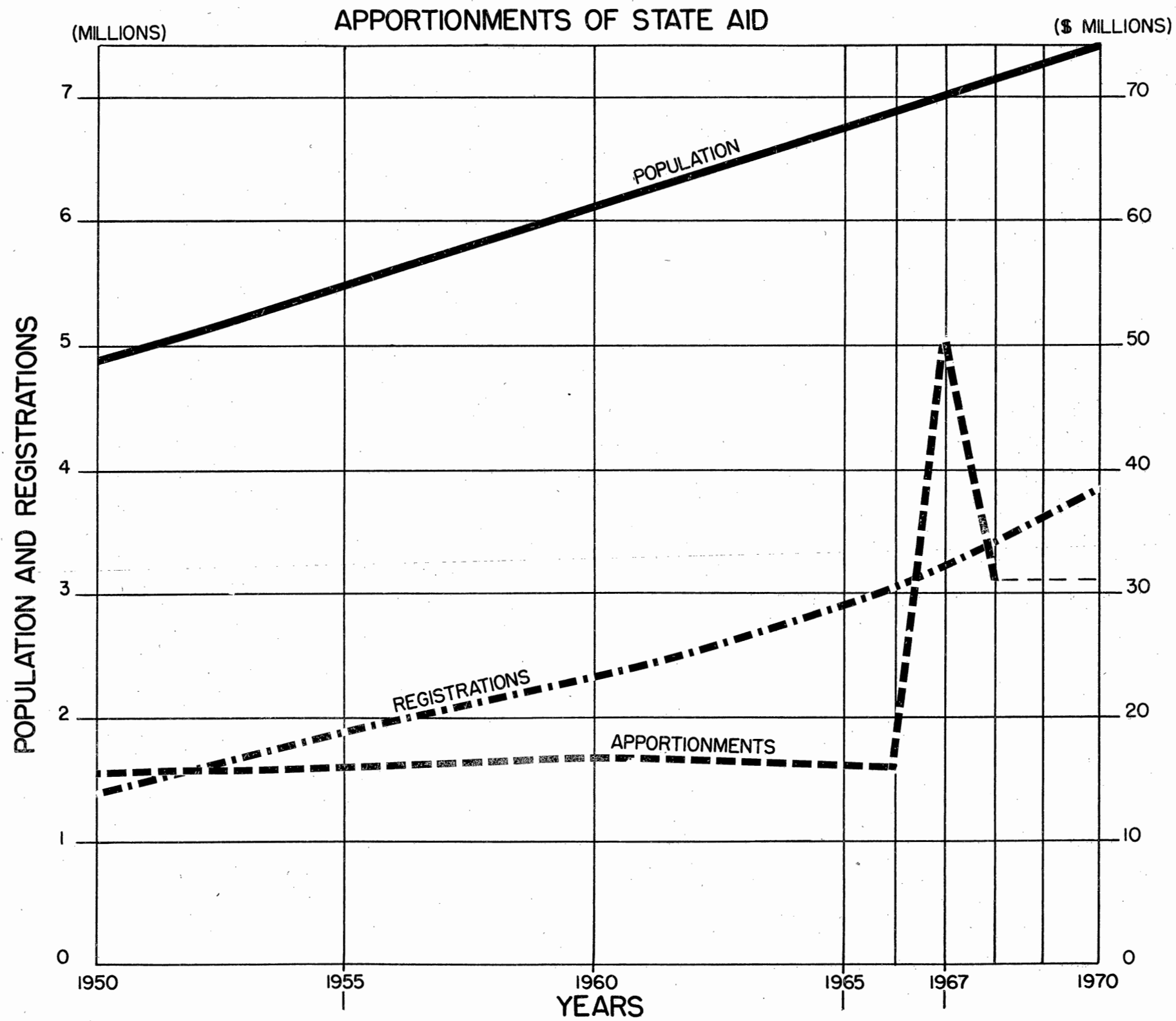
Similar increases in the cost per mile estimates are expected as a result of the updated 1968 estimates. New Jersey's cost per mile have increased from \$3.3 million to \$3.6 million.

Source: U.S. Bureau of Public Roads  
Quarterly Report on Progress No. 34-42

#### DIVISION OF LOCAL GOVERNMENT AID

One of the major operating branches of the Department of Transportation is the Division of Local Government Aid which discharges the Commissioner's responsibility of administering the State and Federal Aid Programs for improvement of the secondary road network. This network augments the primary system of state highways, and its roads come under the jurisdiction of the various counties and municipalities throughout the State. The aid referred to is in the form of financial and professional assistance to these local governments for maintenance, repair, construction, reconstruction, lighting and policing of roads and associated bridges.

Two separate and distinct aid programs are administered, and these are determined by the source of funds. State funds make up the great majority of available aid money and these are administered by the State Aid Bureau. Federal funds are administered by the Federal Aid Secondary Bureau. Both Bureaus are supervised in their operations by the Division Director, who is responsible to the Assistant Commissioner for Highways and the Commissioner of Transportation.



COMPARISON OF  
POPULATION,  
REGISTRATIONS &  
APPORTIONMENTS  
TO LOCAL AGENCIES  
IN NEW JERSEY  
BETWEEN 1950-1967

NOTE:  
THE INCREASE IN  
APPORTIONMENTS FOR THE  
YEAR 1967 WAS DUE TO  
EXCESS FUNDS MADE  
AVAILABLE BY LEGISLATURE  
AS EXTRAORDINARY STATE  
AID.

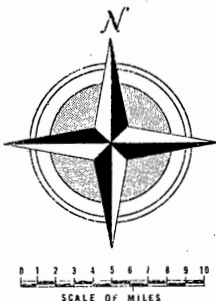
***Interstate Freeways***

***Status and Costs***

# STATUS OF INTERSTATE SYSTEM

November 1967

- LEGEND**
- ▬▬▬▬ UNDER STUDY
  - ▬▬▬▬ ALIGNMENT RECOMMENDATIONS TO THE BUREAU OF PUBLIC ROADS
  - ▬▬▬▬ UNDER DESIGN
  - ▬▬▬▬ UNDER CONSTRUCTION
  - ▬▬▬▬ OPEN TO TRAFFIC

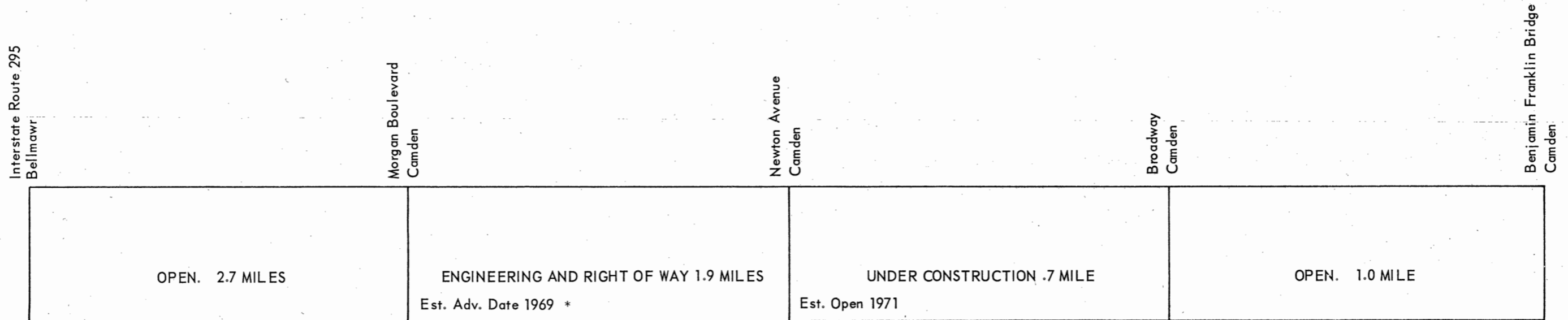


ROUTE	INTERSTATE MILEAGE				TOTAL
	▬▬▬▬	▬▬▬▬	▬▬▬▬	▬▬▬▬	
76	0.0	0.0	1.9	0.7	6.3
78	2.4	0.0	10.8	19.5	66.2
80	0.0	0.0	22.1	19.7	68.1
95	0.0	30.9	1.5	1.0	74.2
278	7.2*	0.0	0.0	1.2	9.0
280	0.0	0.0	8.1	8.9	18.1
287	2.9	16.0	0.0	13.8	62.9
295	0.0	0.0	29.1	11.6	71.9
495	0.0	0.0	0.0	0.0	3.6
676	0.0	0.0	0.0	0.0	1.1
<b>TOTAL</b>	<b>12.5</b>	<b>46.9</b>	<b>73.5</b>	<b>76.4</b>	<b>381.4</b>

\*DUE TO UNANIMOUS LOCAL OPPOSITION TO ROUTE 278 WEST OF ROUTE 1 THE DEPARTMENT HAS REQUESTED THAT THE FEDERAL GOVERNMENT AUTHORIZE THE REASSIGNMENT OF THESE FUNDS.

# Interstate Route 76

OPEN. 3.7 MILES	UNDER CONSTRUCTION .7 MILE	ENGINEERING AND RIGHT OF WAY 1.9 MILES
-----------------	-------------------------------	--



**CAMDEN COUNTY**

\* Depends on Availability of Federal Funds

State-Federal Cost to Date: \$14,593,035  
 Estimated Cost to Complete: 24,049,000 \*\*  
 Total Cost: \$38,642,035

\*\* Cost to complete includes the cost of upgrading and expanding certain sections now open to traffic as a result of design changes and safety improvements mandated by Congress.

Construction period – two years minimum

INTERSTATE ROUTE 76 – Length: 6.3 Miles

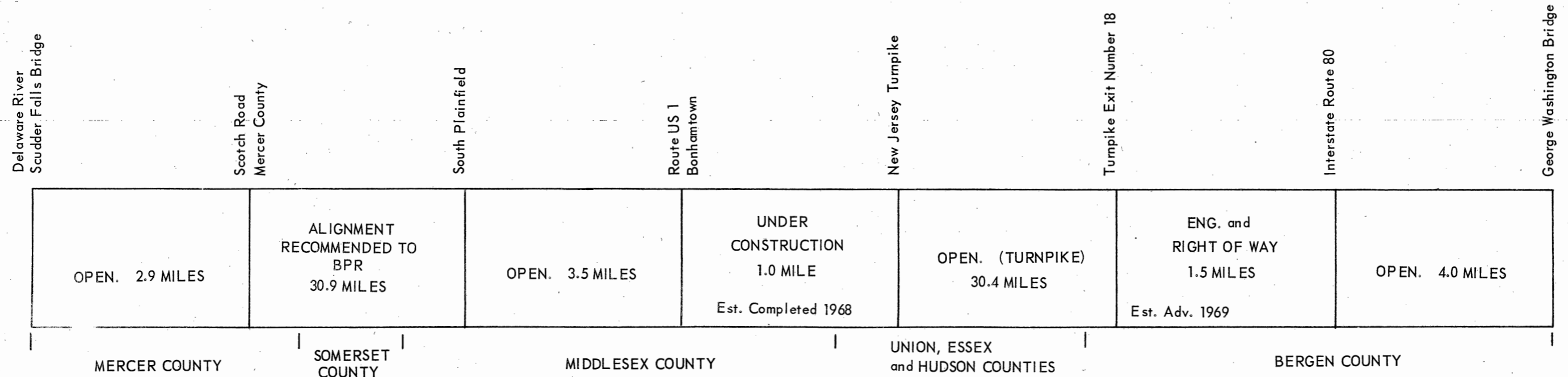
Will provide a direct express link north from Route 295 in Camden County to the Walt Whitman Bridge and the City of Camden. The Freeway then crosses the Benjamin Franklin Bridge into Philadelphia.





# Interstate Route 95

OPEN. 40.8 MILES (INCLUDING 30.4 MILES TURNPIKE )	UNDER CONST. 1.0 MILES	ENG. AND R.O.W. 1.5 MILES	ALIGNMENT RECOMMENDED TO BPR 30.9 MILES
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\* Depends on Availability of Federal Funds

State-Federal Costs to Date: \$ 61,510,812  
 Estimated Cost to Complete : \$117,976,000 \*\*  
 Total Cost: \$179,486,812

\*\* Cost to complete includes the cost of upgrading and expanding certain sections now open to traffic as a result of design changes and safety improvements mandated by Congress.

Construction period - two years minimum

INTERSTATE ROUTE 95 - Length: 74.2 Miles

This Maine-to-Florida freeway crosses the George Washington Bridge into New Jersey, follows the New Jersey Turnpike south to Metuchen, then turns east to the South Plainfield area. The alignment then turns south through Somerset and Mercer Counties and crosses the Delaware River into Pennsylvania west of Trenton. The New Jersey portion will provide a direct link between the Philadelphia Metropolitan area and the Newark-New York area, and relieve traffic pressure on Routes 206, 27, 1, and 130.

# Interstate Route 278

OPEN. .6 MILE	UNDER CONSTRUCTION 1.2 MILES	PRELIMINARY STUDY 7.2 MILES * * *
---------------	------------------------------	-----------------------------------

Interstate Route 78  
Springfield

Route 1 and 9  
Linden

Bridge Approach  
Newark

Goethals Bridge  
Newark

PRELIMINARY STUDY 7.2 MILES * * *  No Target	UNDER CONSTRUCTION 1.2 MILES  Est. Open 1968	OPEN. .6 MILE
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\* Depends on Availability of Federal Funds

UNION COUNTY

INTERSTATE ROUTE 278 – Length 9.0 Miles

State–Federal Cost to Date: \$ 11,611,455  
 Estimated Cost to Complete: \$103,342,000 \*\*  
 Total Cost: \$114,953,455

\*\* Cost to complete includes the cost of upgrading and expanding certain sections now open to traffic as a result of design changes and safety improvements mandated by Congress.

Construction period – two years minimum

A belt highway designed to relieve Route 78 of traffic bound for Union County communities, Linden, Elizabeth, Staten Island and New York City. It branches southward from Route 78 in Springfield Township, Union County, crosses the Goethals Bridge into Staten Island, the Verrazano Bridge into Brooklyn, then extends northward to Interstate Route 95 in the Bronx.

\* \* \* NOTE: Due to substantial opposition in Union County, a request has been submitted to the federal government to reallocate funds assigned to the construction of Route 278 between Springfield and Route 1 & 9 to a central New Jersey Freeway.

# Interstate Route 280

OPEN. 1.1 MILE	UNDER CONSTRUCTION 8.9 MILES	ENGINEERING AND RIGHT OF WAY 8.1 MILES
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Interstate Route 80 Par-Troy Hills	Passaic River East Hanover Township	Tulip Avenue West Orange	Northfield Avenue West Orange	Clifton Avenue Newark	Grant Avenue Harrison	Interstate Route 95 Kearny
ENGINEERING AND RIGHT OF WAY 3.2 MILES Est. Adv. 1968 *	UNDER CONSTRUCTION 5.3 MILES Est. Open 1972	ENGINEERING AND RIGHT OF WAY 1.5 MILES Est. Adv. 1968 *	UNDER CONSTRUCTION 3.6 MILES Est. Open 1970	OPEN. 1.1 MILES	ENGINEERING AND RIGHT OF WAY 3.4 MILES Est. Adv. 1969 *	
MORRIS COUNTY	ESSEX COUNTY		HUDSON COUNTY			

\* Depends on Availability of Federal Funds  
 State-Federal Costs to Date: \$ 66,969,979  
 Estimated Cost to Complete : \$ 89,930,000 \*\*  
 Total Cost: \$156,899,979

\*\* Cost to complete includes the cost of upgrading and expanding certain sections now open to traffic as a result of design changes and safety improvements mandated by Congress.

Construction period - two years minimum

INTERSTATE ROUTE 280 - Length: 18.1 Miles

A branch of Interstate Route 80 which will provide an expressway from rapidly developing eastern Morris County through the densely populated areas of the Oranges and Newark in Essex County and heavily industrialized Harrison and Kearny in Hudson County. The freeway will extend from parent Route 80 in Parsippany-Troy Hills Township, Morris County, to Interstate Route 95 (New Jersey Turnpike) in Kearny.

# Interstate Route 287

OPEN. 30.2 MILES	UNDER CONSTRUCTION 13.8 MILES	ENGINEERING AND RIGHT OF WAY 18.9 MILES
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Interstate Route 95  
South Plainfield
Mount Airy Road  
Bernards Township
Route 10  
Hanover Township
Route 202 (Main Road)  
Montville Township
Route 17  
Mahwah Township
New York State Line  
Suffern

OPEN. 22.1 MILES	UNDER CONSTRUCTION 13.8 MILES  Est. Open 1971	OPEN. 7.4 MILES	ENGINEERING AND RIGHT OF WAY 18.9 MILES  Est. Adv. 1969 *	OPEN. .7 MILE
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MIDDLESEX COUNTY

SOMERSET COUNTY

MORRIS COUNTY

PASSAIC COUNTY

BERGEN COUNTY

\* Depends on Availability of Federal Funds

State-Federal Costs to Date: \$103,276,490  
 Estimated Cost to Complete : \$131,703,000 \*\*  
 Total Cost: \$234,979,490

\*\* Cost to complete includes the cost of upgrading and expanding certain sections now open to traffic as a result of design changes and safety improvements mandated by Congress.

Construction period - two years minimum

INTERSTATE ROUTE 287 -- Length: 62.9 Miles

A belt highway which encircles metropolitan northeast New Jersey. It stems from Interstate Route 287 (the New York Thruway) near Suffern, New York, extends through Morristown and the heavily industrialized and populated Somerville-Bound Brook areas, and meets Interstate 95, South Plainfield. Route 95 then extends east to the New Jersey Turnpike near Metuchen, which becomes Route 95 north to Route 46. At Metuchen, Route 95 also joins Route 440 Freeway which extends east through Perth Amboy to the Outerbridge Crossing Bridge and south Staten Island.

# Interstate Route 295

OPEN. 31.2 MILES (1.0 MILE TOLL)	UNDER CONSTRUCTION 11.6 MILES	ENGINEERING AND RIGHT OF WAY 29.1 MILES
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Delaware Memorial Bridge

Hollywood Avenue  
Upper Penns Neck Township

Route US 130  
Repaupo

Route 38  
Mount Laurel Township

Interstate Route 95  
Mercer County

OPEN. 2.9 MILES (1.0 MILE TOLL)	UNDER CONSTRUCTION 11.6 MILES  Est. Open 1969	OPEN. 28.3 MILES	ENGINEERING AND RIGHT OF WAY 29.1 MILES  Est. Adv. Date, First Contract, 1968 *
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SALEM COUNTY

GLOUCESTER COUNTY

CAMDEN COUNTY

BURLINGTON COUNTY

MERCER  
COUNTY

\* Depends on Availability of Federal Funds

State-Federal Costs to Date: \$ 72,971,576

Estimated Cost to Complete : \$120,161,000 \*\*

Total Cost : \$193,132,576

\*\* Cost to complete includes the cost of upgrading and expanding certain sections now open to traffic as a result of design changes and safety improvements mandated by Congress.

Construction period - two years minimum

INTERSTATE ROUTE 295 - Length: 71.9 Miles

Will serve the New Jersey side of the Delaware River as its parent highway, Interstate Route 95, serves the Pennsylvania side. Route 295 crosses the Delaware Memorial Bridge into Salem County and follows a course west of the New Jersey Turnpike through the Camden metropolitan area, curves around Trenton and rejoins Route 95 in Hopewell Township, Mercer County.

# Interstate Route 495

Interstate Route 95  
Secaucus

Lincoln Tunnel

OPEN. 3.6 MILES (TOLL)

HUDSON COUNTY

INTERSTATE ROUTE 495 – Length: 3.6 Miles

State–Federal Cost to Date: -----  
(Paid with non–Interstate funds)

Estimated Cost to Complete: \$16, 924, 999 \*\*

\*\* Cost to complete includes the cost of upgrading and expanding certain sections now open to traffic as a result of design changes and safety improvements mandated by Congress.

Construction period – two years minimum

The New Jersey approach to the Lincoln Tunnel extends from Interchange 16 of the New Jersey Turnpike in Secaucus through the Lincoln Tunnel, across New York City until it joins and follows the Long Island Expressway.

# Interstate Route 676

Walt Whitman Bridge  
Gloucester City

Interstate Route 76  
Camden

OPEN. 1.1 MILES (TOLL)

CAMDEN COUNTY

INTERSTATE ROUTE 676 – Length: 1.1 Miles

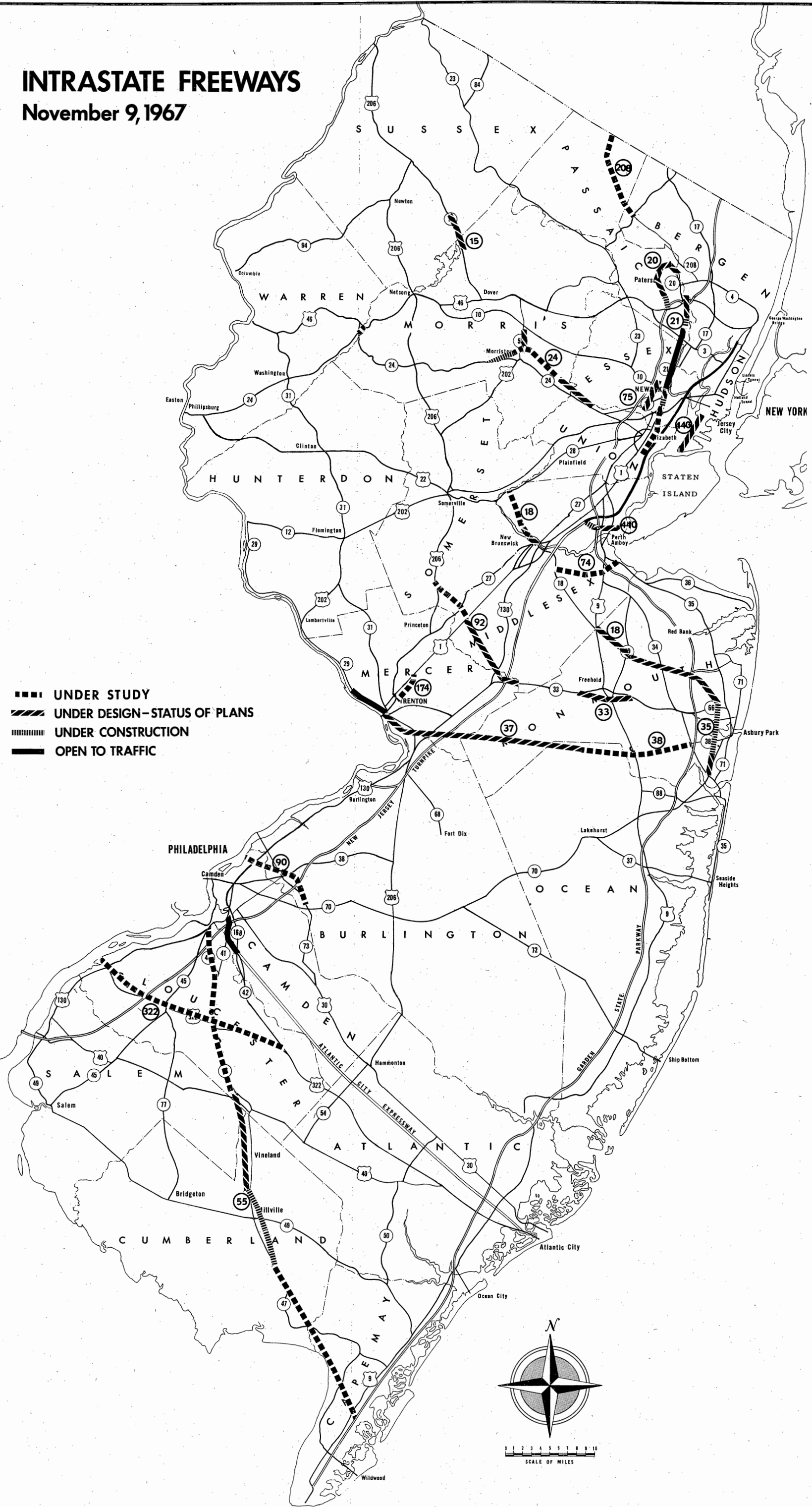
Constructed with non–Interstate funds

Is the mile–long link from Route 76 west to the Walt Whitman Bridge which gives access to south Philadelphia, the Philadelphia International Airport and Navy Yard.

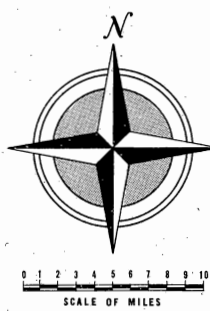
***State Freeways***  
***Status and Costs***

# INTRASTATE FREEWAYS

November 9, 1967

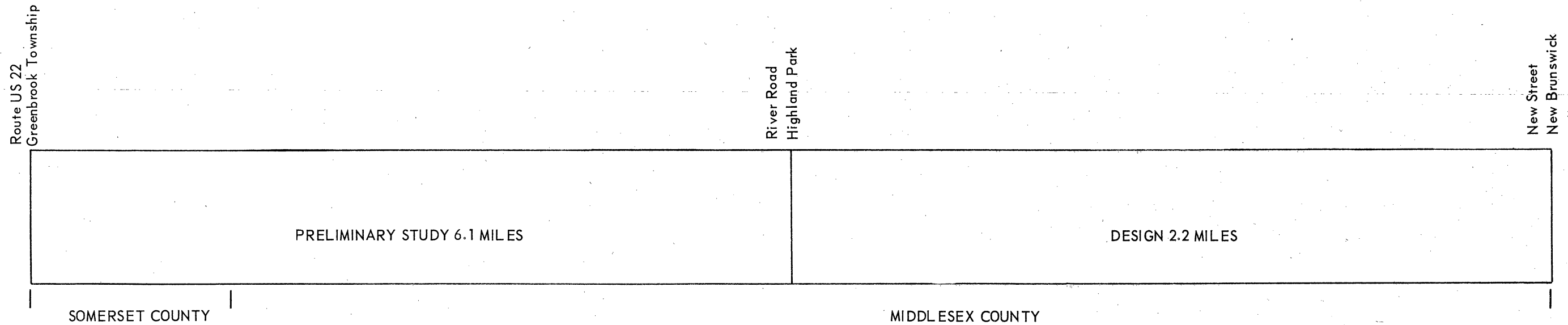
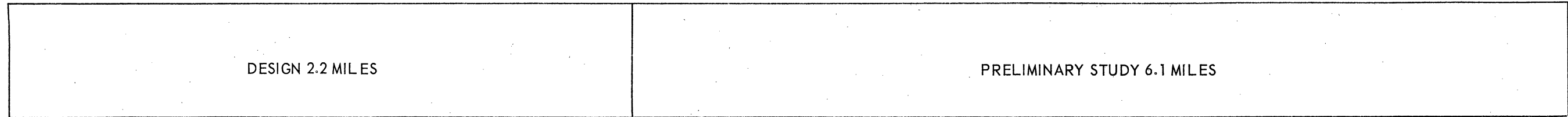


- UNDER STUDY
- /// UNDER DESIGN - STATUS OF PLANS
- |||| UNDER CONSTRUCTION
- OPEN TO TRAFFIC





# Route 18 Freeway



Cost to Date: \$ 1,000,000  
 Estimated Cost to Complete: \$43,000,000 \*  
 Estimated Total Cost: \$44,000,000

\* Depends on Availability of Federal Funds

ROUTE 18 FREEWAY – Length: 8.3 Miles

This north-south artery is needed to provide improved access to New Brunswick, a new crossing of the Raritan River, connections to east-west Route I-95, I-287 and New Jersey 28 and to ultimately serve interregional traffic in the corridor from north central New Jersey to the north Jersey shore.

# Route 18 Freeway

DESIGN 17.1 MILES

Route US 9  
Madison Township

Garden State Parkway  
Eatontown

DESIGN 17.1 MILES

MIDDLESEX COUNTY

MONMOUTH COUNTY

Cost to Date: \$ 4,100,000  
Estimated Cost to Complete: \$22,500,000 \*  
Estimated Total Cost: \$26,600,000

\* Depends on Availability of Federal Funds

ROUTE 18 FREEWAY – Length: 17.1 Miles

This artery will connect existing land-service Route 18 with Route 35 Freeway in the vicinity of the Garden State Parkway spur in Eatontown. It will ultimately serve interregional traffic in the corridor from north central Jersey to the north Jersey shore and provide connections to Route 78 and 34.

# Route 20 Freeway

UNDER CONSTRUCTION 1.8 MILES	ENGINEERING AND RIGHT OF WAY 3.2 MILES
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Route US 46  
Clifton

Valley Road  
Paterson

First Avenue  
Paterson

UNDER CONSTRUCTION 1.8 MILES	ENGINEERING AND RIGHT OF WAY 3.2 MILES
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PASSAIC COUNTY

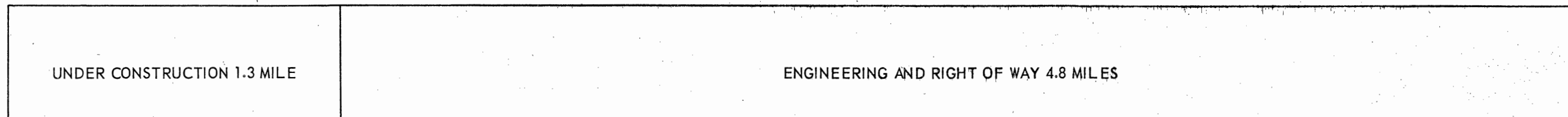
Cost to Date: \$ 8,000,000  
 Estimated Cost to Complete: \$50,000,000 \*  
 Estimated Total Cost: \$58,000,000

\* Depends on Availability of Federal Funds

ROUTE 20 FREEWAY – Length: 5.0 Miles

This Route will connect Route 46 in Clifton and Route 20 in Paterson, Passaic County. It will be a peripheral highway around Paterson, a much needed highway to relieve traffic in this area and will interchange with Route 80 in Paterson. This route is legislated.

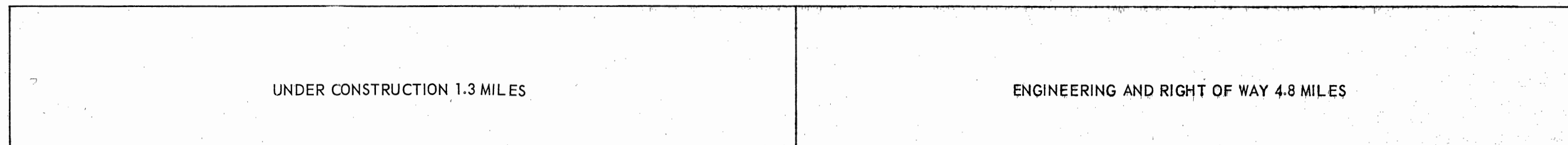
# Route 21 Freeway



South Parkway  
Clifton

Lafayette Street  
Passaic

Interstate Route 80  
Paterson



PASSAIC COUNTY

Cost to Date: \$16,000,000  
 Estimated Cost to Complete: \$34,200,000 \*  
 Estimated Total Cost: \$50,200,000

**ROUTE 21 FREEWAY** – Length: 6.1 Miles

This Route will connect Route 21 Freeway at Clifton, Passaic County, and I-80 in East Paterson, Bergen County. It will relieve north-south traffic in this congested area. The route has been legislated.

\* Depends on Availability of Federal Funds

# Route 24 Freeway

UNDER CONST. 1.0 MILE	ENGINEERING AND RIGHT OF WAY 10.0 MILES	DESIGN 5.0 MILES
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West of Morristown

Brooklake Road  
Chartham

Erie-Lackawanna Railroad  
Summit

Interstate Route 78  
Springfield

ENGINEERING AND RIGHT OF WAY 10.0 MILES	DESIGN 5.0 MILES	UNDER CONSTRUCTION 1.0 MILE
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MORRIS COUNTY

UNION COUNTY

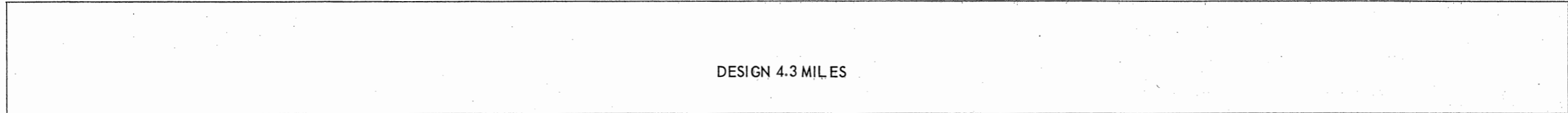
Cost to Date: \$18,200,000  
 Estimated Cost to Complete: \$62,300,000 \*  
 Estimated Total Cost: \$80,500,000

\* Depends on Availability of Federal Funds

ROUTE 24 FREEWAY – Length: 16.0 Miles

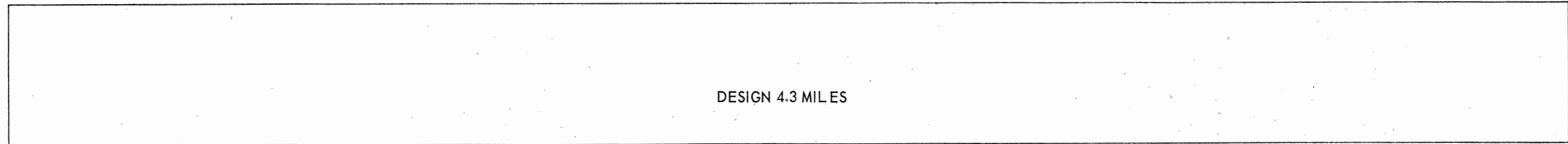
This artery will connect Route I-78 in Union County with Route 24 west of Morristown, Morris County. Legislation has been passed for this freeway. It will relieve existing Route 24 which is totally inadequate for the traffic in this corridor.

# Route 29 Freeway



Willow Street  
Trenton

Route 37  
White Horse



MERCER COUNTY

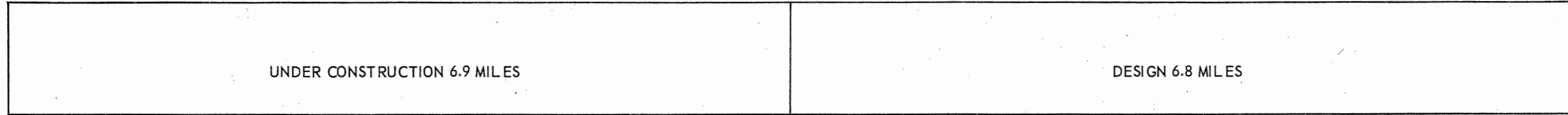
Cost to Date: \$ 6,200,000  
Estimated Cost to Complete: \$29,200,000 \*  
Estimated Total Cost: \$35,400,000

\* Depends on Availability of Federal Funds

ROUTE 29 FREEWAY – Length 4.3 Miles

This Route will ultimately provide a badly needed artery for the free flow of north-south traffic to the State Capital. At its south-eastern terminus it will connect with Route 37 Freeway and hence become a part of the Central Jersey Expressway.

# Route 35 Freeway

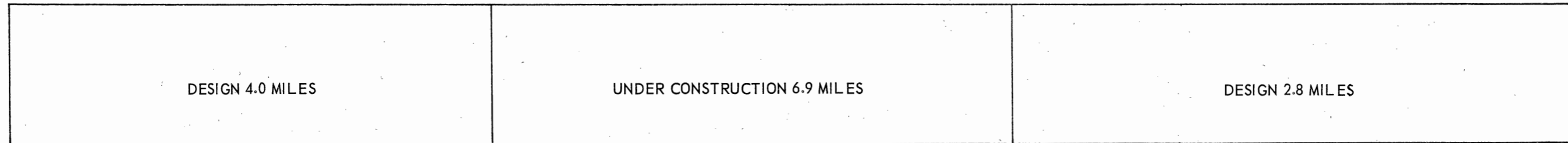


Route 34  
Brielle Circle

18th Avenue  
Wall Township

Deal Road  
Ocean Township

Route 35  
Shrewsbury Borough



MONMOUTH COUNTY

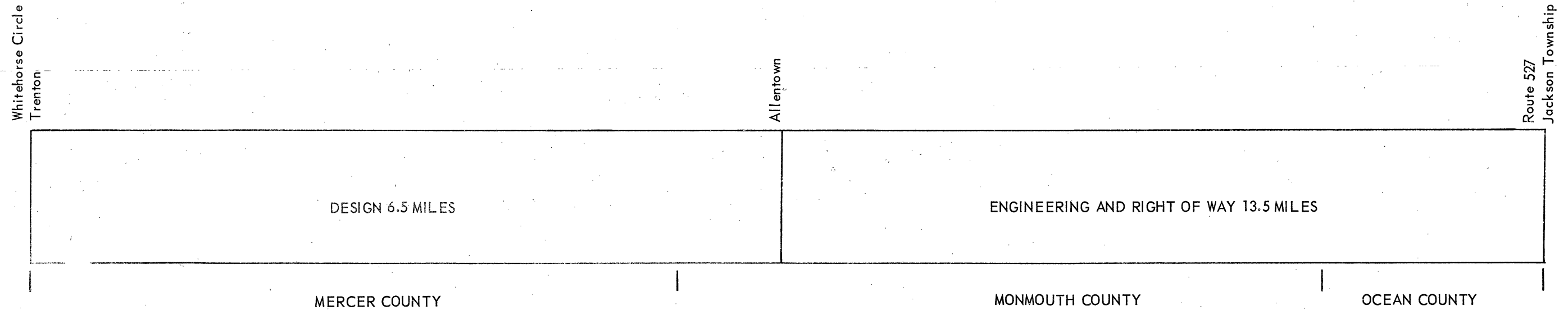
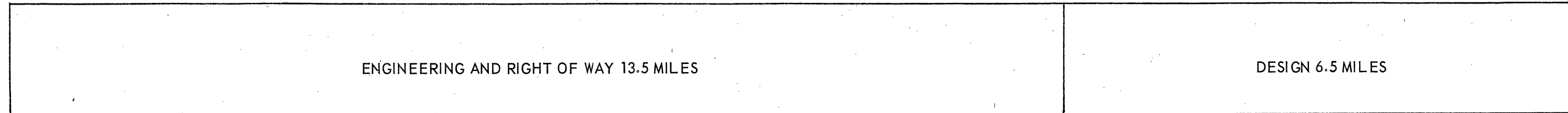
ROUTE 35 FREEWAY – Length: 13.7 Miles

Cost to Date: \$12,700,000  
 Estimated Cost to Complete: \$11,000,000 \*  
 Estimated Total Cost: \$23,700,000

\* Depends on Availability of Federal Funds

This Freeway is on new alignment and is needed to provide a route with freeway characteristics which will relieve existing Route 35. The present land-service road, most of which is three-lane, is congested and hazardous due to traffic generated by army installations, shopping facilities and coastal resorts. In addition to by-passing heavily developed areas, the freeway will provide connections to east-west Routes 38, 33, 66, 18 and the Garden State Parkway spur at Eatontown. The northern terminus will be in the vicinity of Shrewsbury Borough.

# Route 37 Freeway



Cost to Date: \$ 5,000,000  
 Estimated Cost to Complete: \$34,200,000 \*  
 Estimated Total Cost: \$39,200,000

\* Depends on Availability of Federal Funds

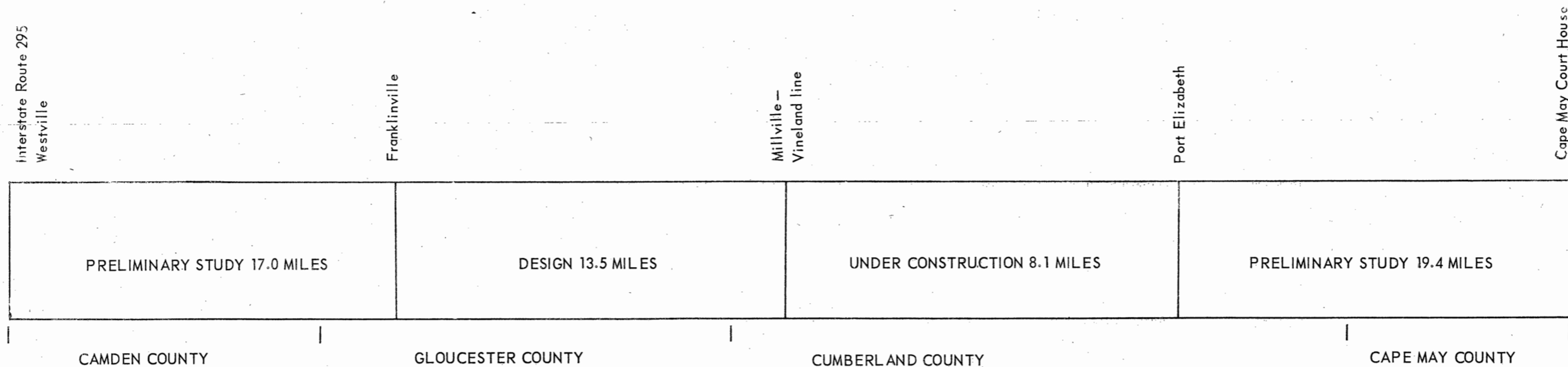
## ROUTE 37 FREEWAY – Length: 20.0 Miles \*\*

This freeway, on new alignment, will become a part of the Central Jersey Freeway System as announced by Governor Hughes in 1965. Its termini will be in the vicinity of South Broad Street, Trenton on the west and at Bennetts Mills in Jackson Township on the east. It will become a vital link in providing easy access from the Trenton metropolitan area to the shore resorts. Design of the freeway will preserve and enhance the natural beauty of the corridor

\*\* Freeway connection between Bennetts Mills and shore points has been planned as a responsibility of the New Jersey Highway Authority. Legislation now pending in Congress would provide financing so that the Department of Transportation could build this connection more rapidly.

# Route 55 Freeway

UNDER CONSTRUCTION 8.1 MILES	DESIGN 13.5 MILES	PRELIMINARY STUDY 36.4 MILES
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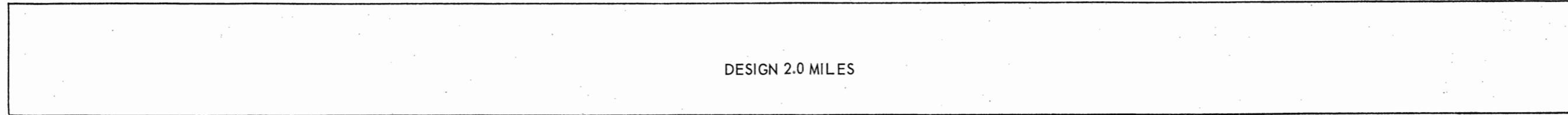
Cost to Date: \$ 9,500,000  
 Estimated Cost to Complete: \$23,500,000 \*  
 Estimated Total Cost: \$33,000,000

\* Depends on Availability of Federal Funds

ROUTE 55 FREEWAY -- Length: 58.0 Miles

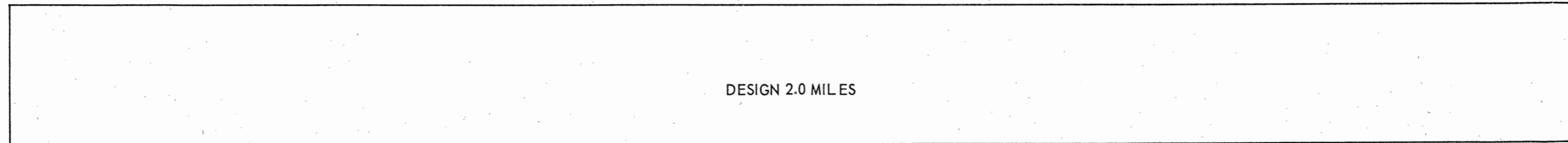
This Route will extend from Route U. S. 9 in the vicinity of Cape May Court House, Cape May County to Route I-295 at Westville, Gloucester County, on new alignment. The Freeway is legislated.

# Route 75 Freeway



Interstate Route 78  
Newark

Interstate Route 280  
Newark



ESSEX COUNTY

Cost to Date: \$ 1,500,000  
Estimated Cost to Complete: \$67,700,000 \*  
Estimated Total Cost: \$69,200,000

\* Depends on Availability of Federal Funds

ROUTE 75 FREEWAY – Length: 2.0 Miles

This Freeway, a new, approved alignment, is a vital link in a north-south artery which will ultimately provide a through route from Route I-80 in Passaic County to the Goethals Bridge and Staten Island. Completion of the project will prove costly because of dense urban development within the area it traverses.

# Route 81 Freeway

PRELIMINARY STUDY 3.0 MILES

Interstate Route 278  
Goethals Bridge

Route U. S. 1 & 9  
Elizabeth

Route U. S. 22  
Newark

PRELIMINARY STUDY 3.0 MILES

UNION COUNTY

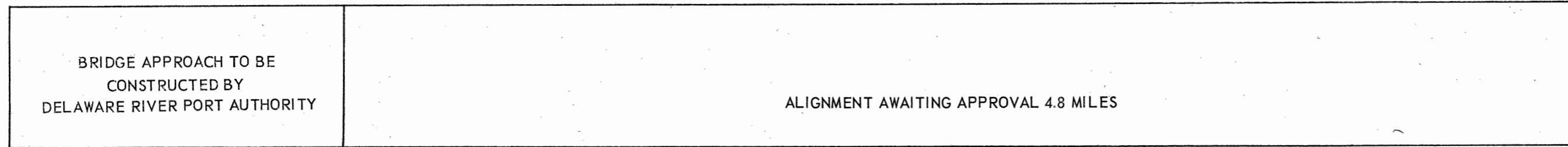
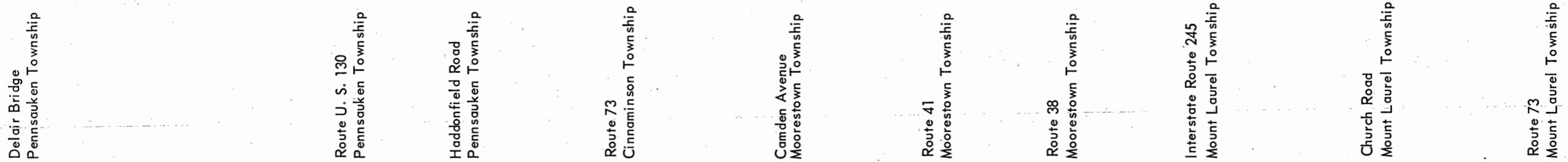
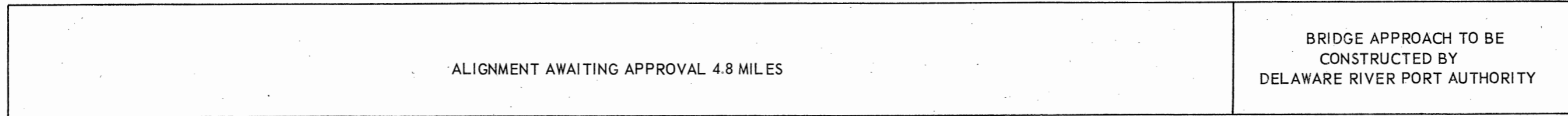
Cost to Date: \$ 1,000,000  
Estimated Cost to Complete: \$35,000,000 \*  
Estimated Total Cost: \$36,000,000

\* Depends on Availability of Federal Funds

ROUTE 81 FREEWAY - Length: 3.0 Miles

This route, on new alignment, will bypass the congested area of Elizabeth and will provide relief for Route U. S. 1 and 9 which is over capacity. With the scheduled expansion of the Port Elizabeth area, this arterial route will also serve the port facilities now in operation.

# Route 90 Freeway



CAMDEN COUNTY

BURLINGTON COUNTY

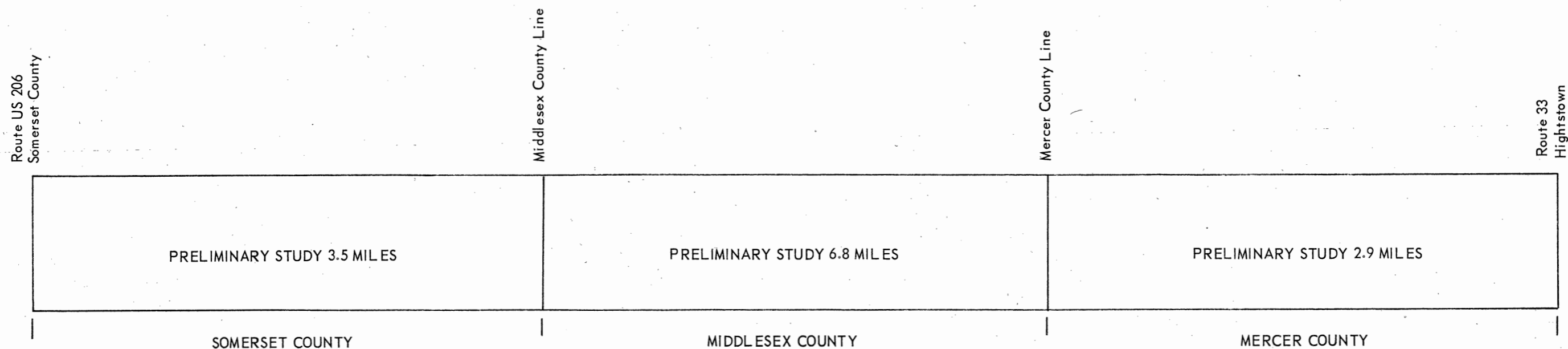
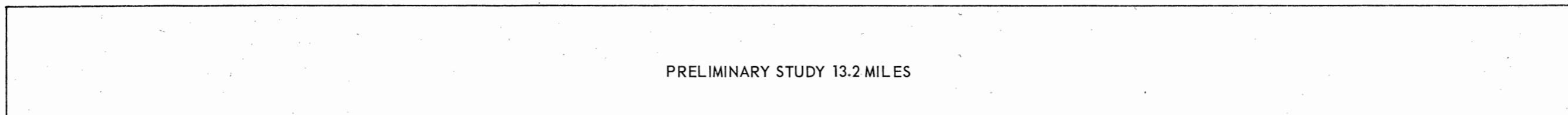
Estimated Total Cost: \$42,000,000 \*

\* Depends on Availability of Federal Funds

ROUTE 90 FREEWAY – Length: 6.3 Miles

This freeway, on new alignment, will connect with the proposed Delair Bridge over the Delaware River in Pennsauken, and Route 73 in the vicinity of Church Road, Mount Laurel Township, Burlington County. It will provide a corridor for traffic north and east of the Camden-Philadelphia area destined for New Jersey shore resorts, bypassing built up areas. This route will also relieve existing Route 70, 73 and U. S. 130.

# Route 92 Freeway



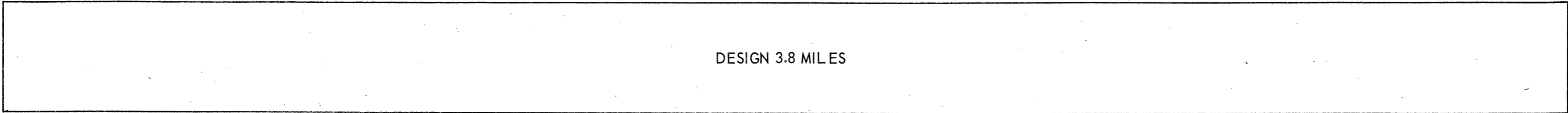
Cost to Date: \$ 500,000  
 Estimated Cost to Complete: \$33,600,000 \*  
 Estimated Total Cost: \$34,100,000

\* Depends on Availability of Federal Funds

ROUTE 92 FREEWAY – Length: 13.2 Miles

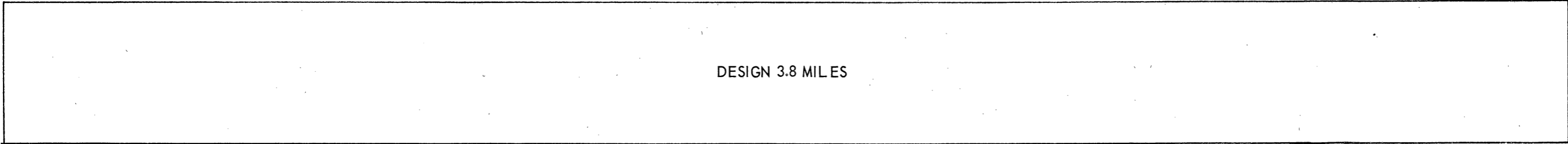
This Route is on new alignment and connects Route 206 north of Princeton with Route 33 just east of Hightstown near the New Jersey Turnpike. It is needed to provide a limited access by-pass of Hightstown, Kingston, Princeton and Trenton for through traffic. This facility will provide more efficient distribution of traffic throughout this rapidly expanding tri-county suburban area.

# Route 440 Freeway



Interstate Route 95  
Woodbridge

Outerbridge  
Crossing



MIDDLESEX COUNTY

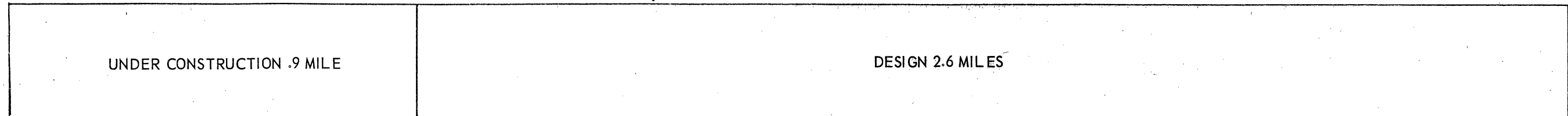
Cost to Date: \$ 9,750,000  
Estimated Cost to Complete: \$39,250,000 \*  
Estimated Total Cost: \$49,000,000

\* Depends on Availability of Federal Funds

### ROUTE 440 FREEWAY – Length 3.8 Miles

This link, an extension of Route I-95, will complete a circumferential route around the New York metropolitan area via Staten Island and the Verrazano-Narrows Bridge. Current industrial development between Route U.S. 1 and Perth Amboy including the former Raritan Arsenal property adds to the need for this facility.

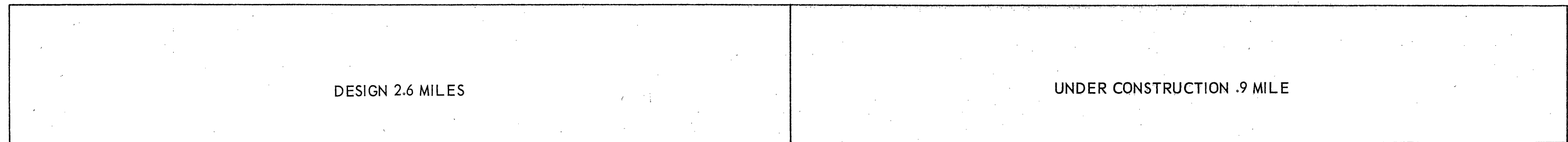
# Route 440 Freeway



Bayonne Bridge

54th Street  
Jersey City

63rd Street  
Jersey City



HUDSON COUNTY

Cost to Date: \$ 6,900,000  
 Estimated Cost to Complete: \$39,000,000 \*  
 Estimated Total Cost: \$45,900,000

\* Depends on Availability of Federal Funds

ROUTE 440 FREEWAY – Length: 3.5 Miles

This Route will be a four-lane divided highway over new alignment most of which will be built on fill in Newark Bay. It will complete an artery connecting the tunnel approach roads with the Bayonne Bridge to Staten Island and will provide relief for local roads through a densely developed urban area.

STATE FREEWAYS IN PRELIMINARY AND DESIGN STAGES

ROUTE 15 FREEWAY

MORRIS AND SUSSEX COUNTIES

This Freeway will be on new alignment connecting existing Route 15 near Lake Shawnee, Morris County to Route U.S. 206 and County Route 565 near Ross Corner Sussex County. This Freeway will provide additional access to Tocks Island National Recreation Area due to be opened in 1975, and relieve traffic on existing Route 15. The route is in final design, and will cost an estimated \$20,000,000. It is approximately 12 miles long.

ROUTE 24 - 46 CONNECTOR

HACKETTSTOWN BYPASS

The present connection between Route 24 and Route 46 at Hackettstown is Route 57. Route 57 and Route 46 intersect at a very sharp angle. This proposed bypass will eliminate this angle and provide improved interchange between these two routes.

The length of this bypass is 1.2 miles and the estimated cost is \$2,500,000.

Preliminary studies are being conducted. No public hearing has been scheduled.

ROUTE 74

MIDDLESEX COUNTY

This proposed route on new alignment connects Route 18 and Route 35 through the heavily industrialized section of Middlesex County. The primary purpose is to serve the area east of Old Bridge to the vicinity of Morgan. The length of this route is 9.5 miles and the estimated cost is \$23,000,000. No public hearing has been held. Preliminary studies are being conducted.

ROUTE 208

BERGEN AND PASSAIC COUNTIES

The extension of Route 208 north and west of Route U.S. 202 at Oakland will allow the resort traffic destined for the Greenwood Lake region a much needed free flowing facility. Present traffic must now travel on inadequate roads in this corridor. An adequate facility will develop the potential recreational areas in this region of the state and provide a connection with the major routes in Sussex County and to Orange County area of New York State.

Preliminary studies are being made to determine a feasible location. No public hearing has been held. This route is estimated to cost \$30,000,000 and will be 10.5 miles long.

ROUTE U.S. 322

GLOUCESTER COUNTY

Route U.S. 322 will be a freeway on new alignment. It will connect the proposed Chester-Bridgeport Bridge at Bridgeport with Route 42 in the vicinity of Williamstown, a distance of 21.2 miles. The present alignment of Route U.S. 322 is a land service two-lane road which will be inadequate to serve the future traffic when the new bridge is opened.

A location study is underway. No public hearing has been held. The estimated cost is \$59,600,000.

***Legislated Highways  
Dualizations  
Widenings and Improvements***

ROUTE \_\_\_\_\_  
DORSET AVENUE CONNECTOR

Atlantic County  
Length - 1.0 Mile

(No route number assigned yet.)  
Branches from Route 40-322 midway between Atlantic City and Pleasantville, extends southeast to a connection with Dorset Avenue and West End Avenue in Ventnor. Not programmed for study as yet.

ROUTE \_\_\_\_\_  
ROUTE 24 FREEWAY, HANOVER TOWNSHIP  
TO ROUTE U.S. 46, DENVILLE

Morris County  
Length - 8.0 Miles

This provides a route from a point on Route 24 Freeway, Hanover Township, Morris County north to a point on Route U.S. 46, Denville. It is awaiting preliminary study.

ROUTE 15 FREEWAY  
LAKE SHAWNEE TO ROSS CORNER

Morris-Sussex  
Counties  
Length - 12 Miles

Beginning on State Highway Route 15 in the vicinity of Lake Shawnee, Morris County, and then north on new alignment through Jefferson Township, Morris County and Frankford Township, Sussex County, to State Highway Route 15 in the vicinity of Route U.S. 206 and County Route 565 near Ross Corner, Sussex County. The route is in final design.

ROUTE 18 FREEWAY WEST  
NEW BRUNSWICK TO ROUTE 22

Middlesex County  
Length - 8.3 Miles

Begins at Route 18 in New Brunswick and extends northwest parallel to the Raritan River, crossing the River near Metlars Lane and continuing to Route 22 near Greenbrook Road in Somerset County. Legislation includes a spur to Interstate Route 287 in the vicinity of Metlars Lane, Piscataway Township. The portion from New Brunswick across the River to River Road is under design. The portion from River Road to Route 22 is under study. (Note: East and west Freeway portions will connect to the existing Route 18 land-service road now open between Route 18 in New Brunswick and Route 9 in Madison Township, Middlesex County.)

ROUTE 18 FREEWAY EAST  
ROUTE 9 TO ROUTE 35 FREEWAY

Monmouth County  
Length - 16.4 Miles

Beginning at its intersection with Route 9 in Middlesex County just west of the Monmouth County line, extending east to a future junction with the Route 35 Freeway near the Garden State Parkway spur in Eatontown. Alignment has been approved and is under detailed engineering design.

ROUTE 24 AND S-24 FREEWAY  
NEWARK TO PHILLIPSBURG

Union, Essex, Morris  
Warren Counties  
Length - 50 Miles

Beginning in Newark by connections with Route U.S. 1 and the New Jersey Turnpike, and then generally along the direction of and in the vicinity of Springfield Avenue by way of Irvington, Maplewood, Springfield, Morristown, Chester, Long Valley, Washington and Phillipsburg, with a spur to Route U.S. 46, Hackettstown, and a spur to Route U.S. 1 Elizabeth. Freeway designation: 1952. Construction is underway from Interstate Route 78 west to the Erie-Lackawanna Railroad, Summit. The route is in design from there west to Morristown, and in preliminary study from Morristown west to Phillipsburg.

ROUTE 33 FREEWAY  
FREEHOLD BYPASS

Monmouth County  
Length - 7.0 Miles

Branches from existing Route 33 west of Freehold, extends around the south side of Freehold, rejoins Route 33 east of Freehold. Freeway is in planning stage.

ROUTE 35 FREEWAY  
EATONTOWN TO BELMAR

Monmouth County  
Length - 13.7 Miles

Beginning in the vicinity of the Borough of Eatontown, Monmouth County, at State Highway Route 35 on the north, then in a southerly direction east of the Garden State Parkway, through Ocean, Neptune and Wall Townships to State Highway Route 35 in the vicinity of the Brielle traffic circle, Monmouth County on the south. Construction is underway from Deal Road, Ocean Township south to 18th Avenue, Wall Township. The other portions are in final design.

\* Route 24 Freeway legislated 1952

Route 35 Freeway legislated 1958

STATE HIGHWAYS LEGISLATED SINCE 1960

ROUTE 37 FREEWAY  
ROUTE 29 FREEWAY TO COUNTY  
ROUTE 527, JACKSON TOWNSHIP

Mercer, Monmouth and  
Ocean Counties  
Length - 24 Miles

Begins on the east side of a future major interchange which also will include Route 29 Freeway and Route 206. Extends east to the vicinity of Bennetts Mills in Jackson Township, Ocean County. Engineering plans are underway. (Note: Freeway connections between Bennetts Mills and New Jersey shore points have been planned as a responsibility of the New Jersey Highway Authority. Legislation now pending in Congress would provide financing so the Department of Transportation could build this connection more rapidly.)

ROUTE 55 FREEWAY  
CAPE MAY COURT HOUSE TO ROUTE 295

Cape May, Cumberland,  
Gloucester Counties  
Length - 58 Miles

Begins at Cape May Court House, extends northwest between Millville and Vineland to Interstate Route 295 in Westville, Gloucester County. The Millville-Vineland bypass is under construction; middle portion, from Port Elizabeth to Franklinville in Cumberland County is being designed for construction and remaining portions are in the planning stage.

ROUTE 74 FREEWAY  
EAST BRUNSWICK TO ROUTE 35

Middlesex County  
Length - 9.5 Miles

Begins at Route 18 in East Brunswick, extends east to Route 35 south of Cheesequake Creek in Middlesex County. The route is in the planning stage.

ROUTE 75 FREEWAY  
ROUTE I 78 TO ROUTE I 280- NEWARK

Essex County  
Length - 2.0 Miles

This freeway on new alignment is a vital link in a north-south artery which will ultimately provide a through route from Interstate Route 80 in Passaic County to the Goethals Bridge and Staten Island. It is in design stage.

ROUTE 81 FREEWAY  
ELIZABETH TO NEWARK

Union, Essex Counties  
Length - 3.0 Miles

Begins at Interstate Route 278 interchange with the New Jersey Turnpike in Elizabeth, extends north parallel to the Turnpike, to the vicinity of McClellan Street, Newark. Design has commenced pending public hearing.

ROUTE 85 FREEWAY  
HOLLAND TUNNEL TO LINCOLN TUNNEL

Hudson County  
Length - 2.2 Miles

Beginning at the Holland Tunnel and extending north through Jersey City, Hoboken and Weehawkin to the Lincoln Tunnel. Alignment is in planning stage.

ROUTE 90 FREEWAY  
DELAIR BRIDGE TO ROUTE 73

Camden, Burlington  
Counties  
Length - 6.3 Miles

Extends from the approaches to the bridge to be constructed near the Tacony-Palmyra bridge in Pennsauken Township, Camden County, eastward to Route 73 east of the New Jersey Turnpike in Mt. Laurel Township, Burlington County. Route is in planning stage.

ROUTE 92 FREEWAY  
ROUTE 1 TO HIGHTSTOWN

Mercer County  
Length - 9.0 Miles

Beginning at Route 1 north of the Penns Neck Circle and extending eastward to a connection with Route 33 near Hightstown, Engineering plans are being designed.



PART II

DUALIZATION PROJECTS CURRENTLY UNDERWAY

<u>YEAR</u>	<u>COUNTY</u>	<u>ROUTE</u>	<u>MILES</u>	<u>COST</u>
67-68	Bergen	17	3.8	\$2,572,098
67-68	Monmouth	33	2.3	942,789
67-68	Monmouth	35	.2	172,747
67-68	Monmouth	35	2.3	1,021,540
67-69	Monmouth	35	3.5	2,175,015
66-67	Gloucester	45	.8	342,871
67-68	Ocean	72	5.2	4,959,541
67-68	Bergen	208	2.1	1,208,419
67-69	Bergen & Passaic	208	1.5	2,144,645
		TOTAL	21.7	\$15,539,665

PART III

DUALIZATION PROJECTS UNDER DESIGN

<u>COUNTY</u>	<u>ROUTE</u>	<u>MILES</u>	<u>COST (MILLIONS)</u>
Middlesex	1	8.4	11.40
Bergen	3	4.0	11.70
Bergen	4	1.2	.70
Middlesex	9	4.9	5.50
Bergen	17	4.4	6.46
Essex	22	1.1	7.30
Essex Passaic	23	5.6	5.60
Monmouth	33	22.5	10.60
Monmouth	35	6.9	3.10
Monmouth	36	3.5	1.10
Ocean	37	5.2	7.40
Monmouth	38	4.3	3.50
Atlantic	40	2.2	1.60
Gloucester	45	2.6	2.50
Bergen	46	.6	.18
Cumberland	47	1.4	.45
Monmouth	66	2.8	.86
Camden	70	.3	.22
Camden	73	8.5	3.00
Bergen Passaic	208	6.8	3.00
	<u>TOTAL</u>	97.2	86.17

SUMMARY OF COSTS TO COMPLETE FREEWAYS  
AND DUALIZATIONS UNDER DESIGN

Route 18 Freeway (Middlesex County)	\$ 43,000,000	
Route 18 Freeway (Monmouth County)	22,500,000	
Route 20 Freeway	50,000,000	
Route 21 Freeway	34,200,000	
Route 24 Freeway	62,300,000	
Route 29 Freeway	29,200,000	
Route 35 Freeway	11,000,000	
Route 37 Freeway	34,200,000	
Route 55 Freeway	23,500,000	
Route 75 Freeway	67,700,000	
Route 81 Freeway	35,000,000	
Route 90 Freeway	42,000,000	
Route 92 Freeway	33,600,000	
Route 440 Freeway (Middlesex County)	39,250,000	
Route 440 Freeway (Hudson County)	<u>39,000,000</u>	\$566,450,000
Dualization Projects Under Design		<u>86,170,000</u> *
TOTAL COST		\$652,620,000

\* Projects listed by Route on Page 55

WIDENINGS AND IMPROVEMENTS

<u>ROUTE</u>	<u>STATUS</u>	<u>COUNTY</u>	<u>LENGTH</u>	<u>COST</u>
Route 1 Pierson Ave. - Route 278	Design Planning	Middlesex Union	10.4	\$ 26,400,000
Route 1 12th St. Viaduct 14th St. Viaduct	Design Planning	Hudson	-	1,100,000
Route 1 Grade Separation at Route 130	Planning	Middlesex	-	4,000,000
Route 3	Design Planning	Bergen Passaic	16.0	9,000,000
Route 4	Planning	Bergen	10.0	4,000,000
Route 4 Interchange with Route 17	Planning	Bergen	-	5,000,000
Route 4 Interchange with Route 208	Planning	Bergen	-	3,000,000
Route 7	Design	Hudson	6.0	2,000,000
Route 9 Ramp at Route 35	Planning	Middlesex	-	6,000,000
Route 9	Planning	Cape May	-	2,900,000
Route 17 Route 3 to Route 46 Route 46 to NYS Line	Design Planning	Bergen	24.0	20,000,000
Route 22	Design	Essex	0.9	10,000,000
Route 22	Planning	Union & Somerset	21.0	15,000,000
Route 22	Planning	Hunterdon	-	1,000,000
Route 23	Design	Morris	3.5	2,000,000
Route 23	Design	Passaic	4.0	2,500,000
Route 23	Planning	Essex	-	6,000,000
Route 24	Planning	Warren	19.0	6,000,000
Route 27	Planning	Somerset, Middlesex	18.4	10,000,000
Route 27	Planning	Middlesex	0.5	4,000,000

WIDENINGS AND IMPROVEMENTS

<u>ROUTE</u>	<u>STATUS</u>	<u>COUNTY</u>	<u>LENGTH</u>	<u>COST</u>
Route 28 Somerville Circle	Planning	Somerset	-	\$ 5,000,000
Route 29	Planning	Hunterdon	2.0	3,500,000
Route 29	Planning	Cape May	3.8	800,000
Route 30 Collingswood Circle	Planning	Camden	-	3,500,000
Route 33 Collingwood Circle	Planning	Monmouth	-	4,000,000
Route 35	Planning	Monmouth	0.6	2,000,000
Route 40	Planning	Salem	-	2,000,000
Route 45 Woodbury to Redwood Avenue - W. Deptford Township	Design			
Redwood Avenue to Route 322	Planning	Gloucester	9.0	4,500,000
Route 45	Planning	Gloucester, Salem	19.6	8,400,000
Route 46 Midland Avenue	Design	Bergen	-	200,000
Route 46	Planning	Bergen	3.7	15,000,000
Route 46 - Baldwin Ave.	Design	Morris	-	800,000
Route 46 Denville-Parsippany	Design	Morris	6.0	5,000,000
Route 70	Design	Camden	-	100,000
Route 73 From Berlin to At- lantic City Expry.	Design	Camden		
Atlantic City Expry. to Folsom	Planning	Atlantic	16.3	2,960,000
Route 73 Marlton Circle	Planning	Burlington	-	3,700,000
Route 77	Planning	Gloucester, Cumberland	22.6	8,800,000

WIDENINGS AND IMPROVEMENTS

<u>ROUTE</u>	<u>STATUS</u>	<u>COUNTY</u>	<u>LENGTH</u>	<u>COST</u>
Route 87	Design	Atlantic	-	\$ 4,100,000
Route 88	Design & Planning	Monmouth	8.5	3,600,000
Route 94 Columbia Foothills Freeway	Planning	Warren, Sussex	7.0	3,500,000
Route 94	Planning	Warren, Sussex	14.0	7,000,000
Route 130 Cooper River- Collingswood	Design	Camden	0.6	1,100,000
Route 130 Airport Circle	Design and Planning	Camden	-	3,000,000
Route 130	Design	Burlington	10.0	3,000,000
Route 172 Douglass College	Design	Middlesex	-	150,000
Route 202	Planning	Hunterdon	3.0	1,000,000
Route 206 Belle Mead	Design	Somerset	-	3,500,000
Route 444 County Road 29	Planning	Cape May	-	2,000,000
				<u>\$ 242,110,000</u>

SUMMARY OF PROJECTED COSTS

Freeways\* and  
Dualizations \$652,620,000

Widenings and  
Improvements 242,110,000

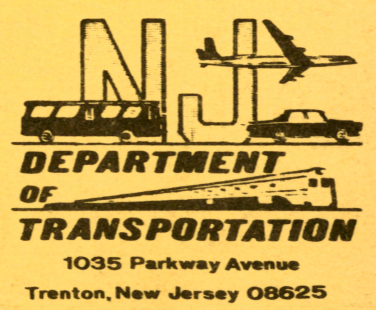
GRAND TOTAL \$894,730,000

\* Does not include costs of Interstate Freeways nor costs of additional freeways found on Page 50.

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R628

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DECEMBER, 1967

