

**DELAWARE RIVER JOINT TOLL BRIDGE
COMMISSION**

Financial Statements
and
Supplementary Information

December 31, 2008 and 2007

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

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INDEPENDENT AUDITORS' REPORT

To the Board of Commissioners of
Delaware River Joint Toll Bridge Commission
Morrisville, Pennsylvania

We have audited the accompanying financial statements of Delaware River Joint Toll Bridge Commission (the "Commission") as of and for the years ended December 31, 2008 and 2007. These financial statements are the responsibility of the Commission's management. Our responsibility is to express an opinion on these financial statements based on our audits.

We conducted our audits in accordance with auditing standards generally accepted in the United States of America and the standards applicable to financial audits contained in *Government Accounting Standards*, issued by the Comptroller General of the United States. Those standards require that we plan and perform the audits to obtain reasonable assurance about whether the financial statements are free of material misstatement. An audit includes examining, on a test basis, evidence supporting the amounts and disclosures in the financial statements. An audit also includes assessing the accounting principles used and significant estimates made by management, as well as evaluating the overall financial statement presentation. We believe that our audits provide a reasonable basis for our opinion.

In our opinion, the financial statements referred to above present fairly, in all material respects, the financial position of the Commission as of December 31, 2008 and 2007, and the changes in its financial position and its cash flows for the years then ended, in conformity with accounting principles generally accepted in the United States of America.

As described in Note H to the financial statements, the Commission adopted Governmental Accounting Standards Board Statement No. 45, *Accounting and Financial Reporting by Employers for Post-Employment Benefits Other Than Pensions*, for the year ended December 31, 2008.

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INDEPENDENT AUDITORS' REPORT (CONTINUED)

In accordance with *Government Auditing Standards*, we have also issued our report dated May 13, 2009, on our consideration of the Commission's internal control over financial reporting and our tests of its compliance with certain provisions of laws, regulations, bond resolutions, contracts, compact and other matters. The purpose of that report is to describe the scope of our testing of internal control over financial reporting and compliance and the results of that testing and not to provide an opinion on the internal control over financial reporting or on compliance. That report is an integral part of an audit performed in accordance with *Government Auditing Standards* and should be considered in assessing the results of our audits.

Management's discussion and analysis, as shown on pages 3-6, is not a required part of the basic financial statements but is supplementary information required by accounting principles generally accepted in the United States of America. We have applied certain limited procedures, which consisted principally of inquiries of management, regarding the methods of measurement and presentation of management's discussion and analysis. However, we did not audit the information, and we express no opinion on it.

Our audits were conducted for the purpose of forming an opinion on the basic financial statements of the Commission taken as a whole. The supplementary schedules on pages 25-33 are presented for purposes of additional analysis and are not a required part of the basic financial statements. Such information has been subjected to the auditing procedures applied in the audits of the basic financial statements and, in our opinion, is fairly stated in all material respects in relation to the basic financial statements taken as a whole.

Mercaderes, P.C.
Certified Public Accountants
May 13, 2009

MANAGEMENT'S DISCUSSION AND ANALYSIS

As management of the Delaware River Joint Toll Bridge Commission (the "Commission"), we offer readers of the Commission's financial statements this narrative overview and analysis of the financial activities of the Commission's fiscal years ended December 31, 2008 and 2007. We encourage readers to consider the information presented here in conjunction with the audited financial statements and supplementary information as a whole.

Financial Highlights

Total toll revenues for the Commission totaled \$86,159,106 for the year ended December 31, 2008, which represents an increase of 0.77% over the previous year. The increase in 2008 is primarily the result of a \$0.50 per axle toll adjustment on May 19, 2007, for all commercial vehicles of three axles and larger.

In 2008, net operating revenues totaled \$38,682,002 and change in net assets totaled \$5,911,613, as compared to \$42,927,641 and \$33,342,527, respectively, for 2007.

Overview of the Financial Statements

This discussion and analysis is intended to serve as an introduction to the Commission's financial statements, which are comprised of the financial statements, the notes to the financial statements, and certain required supplementary information. The supplementary information includes schedules of operations, expenses, cash and equivalent balances, investments and traffic and revenues.

Basic Financial Statements

The basic financial statements are designed to provide readers with a broad understanding of the Commission's finances, in a manner similar to that provided in the financial statements of private-sector businesses.

The statements of net assets present information on the Commission's assets and liabilities at December 31, 2008 and 2007, with the difference between the two reported as net assets. Over time, increases or decreases in net assets may serve as useful indicators of whether the financial position of the Commission is improving or deteriorating. At December 31, 2008, the Commission's net assets equaled \$385,921,973, as compared to \$380,010,360 in 2007 - an increase of 1.56%. Net assets increase when revenues exceed expenses.

The statements of revenues, expenses and changes in net assets present information showing how net assets changed during the fiscal year. All changes in net assets are reported as soon as the underlying event occurs, regardless of the timing of related cash flows. Thus, revenues and expenses are reported in these statements for some items that will not result in cash flows until future fiscal periods or for items that have resulted in cash flows in previous periods.

MANAGEMENT'S DISCUSSION AND ANALYSIS (CONTINUED)

Notes to Financial Statements

The notes provide additional information that is essential to a full understanding of the data provided in the basic financial presentation.

Other Information

In addition to the basic financial statements and accompanying notes, this report also presents certain supplementary information concerning expenses, investments and traffic.

Financial Analysis

Commission assets, consisting of restricted and unrestricted assets, totaled \$872,781,937. Unrestricted current assets, totaling \$8,032,324 (a decrease of \$4,586,985, or 36.35%), represents cash in the operating accounts, cash equivalent investments, and E-ZPass toll receivables. These unrestricted assets will be used to pay current expenses, to pay current debt service, or to be transferred to the general reserve fund. Restricted assets, totaling \$861,255,151, are broken into two categories. Restricted current assets of \$61,981,523 decreased 2.72% from the previous year end as a result of changes in investment security maturity terms. Total non-current assets totaled \$802,768,090, which represents an increase of \$13,959,308, or 1.77%, from the 2007 year-end balance. Restricted cash and investments totaling \$440,767,976, which represents a decrease of \$83,285,168, or 15.89%, from the previous year, are restricted under the Trust Indenture, to be used only for purposes listed on pages 11-12 of this report. These changes in restricted assets are the result of payments from the bond funds to fund the purchase of capital assets and the payment of Compact Authorized Investment grants. Capital assets totaling \$411,553,216 consist of land, infrastructure and equipment with an original value of approximately \$609.7 million less accumulated depreciation of approximately \$198.1 million. The land and infrastructure consist of twenty bridge crossings and related access roads spread over a 140-mile-long stretch of the Delaware River extending from Trenton, New Jersey north to Milford, Pennsylvania/Montague, New Jersey.

At December 31, 2008, the Commission had current and non-current liabilities of \$486,859,964, with the majority related to its series 2003, 2005A, and 2007A, and 2007B bond issues, which represents an increase of \$1,726,479 from 2007. The purpose of the 2003 issue was for the current refunding of the 1992 series, refunding of the 2002 Bond Anticipation Notes, financing of the first portion of the Commission's ten-year capital program, and related bond-issuance cost. The purpose of the 2005A issue was for the refunding of \$32,165,000 of the 2003 series bonds and the financing of the Commission's \$40 million Compact Authorized Investment program. The purpose of the 2007A and 2007B issues was to provide funds to pay for the cost of capital improvements related to the system, to make deposits into the debt service reserve fund and to fund capitalized interest, to pay insurance and cost of issuance associated with the series.

The following table contains condensed financial information derived from the December 31, 2008 and 2007 financial statements of the Commission:

MANAGEMENT'S DISCUSSION AND ANALYSIS (CONTINUED)

Financial Analysis (Continued)

	2008	2007
Net Assets		
Current and other assets	\$ 461,228,721	\$ 548,599,598
Capital assets	<u>411,553,216</u>	<u>316,544,247</u>
Total assets	<u>872,781,937</u>	<u>865,143,845</u>
Bond indebtedness	458,445,771	469,547,491
Other liabilities	<u>28,414,193</u>	<u>15,585,994</u>
Total liabilities	<u>486,859,964</u>	<u>485,133,485</u>
Net assets		
Investment in capital assets, net of related debt	205,515,765	198,100,157
Restricted	189,361,377	174,681,651
Unrestricted	<u>(8,955,169)</u>	<u>7,228,552</u>
Total net assets	<u>\$ 385,921,973</u>	<u>\$ 380,010,360</u>
Changes in Net Assets		
Operating revenues	\$ 86,159,106	\$ 85,503,496
Operating expenses	<u>(47,477,104)</u>	<u>(42,575,855)</u>
Net operating revenues	38,682,002	42,927,641
Depreciation	(13,665,224)	(13,198,186)
Non-operating revenues	20,529,000	17,207,134
Non-operating expenses	<u>(39,634,165)</u>	<u>(13,594,062)</u>
Change in net assets	5,911,613	33,342,527
Net assets, beginning of year	<u>380,010,360</u>	<u>346,667,833</u>
Net assets, end of year	<u>\$ 385,921,973</u>	<u>\$ 380,010,360</u>
Summary of Cash Flows		
	2008	2007
Cash provided by operating activities	\$ 45,542,892	\$ 40,219,606
Cash flows used in investing activities	(17,366,362)	(312,701,142)
Cash flows (used in) provided by financing activities	<u>(34,010,795)</u>	<u>268,254,426</u>
Net decrease in cash and cash equivalents	(5,834,265)	(4,227,110)
Cash and equivalents, beginning of the year	<u>67,406,157</u>	<u>71,633,267</u>
Cash and equivalents, end of the year	<u>\$ 61,571,892</u>	<u>\$ 67,406,157</u>

Significant Events

In December 2001, the Commission approved a long-term Capital Improvement Program that provides major bridge rehabilitation, bridge enhancement, and installation of traffic management systems, as well as state-of-the-art bridge security and surveillance.

A toll rate structure with phased increases and discount adjustments was approved by the Commission to fund its Capital Improvement Program for system protection, preservation, management and enhancement of the Commission's infrastructure including twenty bridges, seven toll plazas, and administration and maintenance facilities that it owns, operates and maintains, as well as operating expenses for the Commission.

MANAGEMENT'S DISCUSSION AND ANALYSIS (CONTINUED)

Significant Events (Continued)

On May 19, 2007, the Commission adjusted per axle tolls from \$2.75 per axle to \$3.25 per axle on all commercial vehicles three axles and larger.

On January 15, 2009, the following changes were made to the Commission's E-ZPass discount program: a 20% casual discount provided to all passenger vehicles using E-ZPass was eliminated, a 5% peak period discount for trucks and other commercial vehicles was eliminated, and a 15% off-peak discount for trucks and other commercial vehicles was reduced to 10%.

The Capital Improvement Program continues to evolve as the need for additional projects are identified, program costs are re-evaluated and the Commission undertakes new initiatives to fund transportation infrastructure programs in bridge host communities.

On September 20, 2007, the Commission issued three bond issues: Series 2007A, 2007B1, and 2007B2. The 2007A issue was a fixed rate totaling \$134,170,000. The two Series 2007B issues were auction rate securities totaling \$75,000,000 each and were hedged by an interest rate swap issued by two counterparties. In September 2008, the two Series 2007B issues were converted into variable rate securities and are hedged by an interest rate swap issued by two counterparties.

On January 1, 2008, the Commission adopted GASB Statement No. 45, "Accounting and Financial Reporting by Employers for Post-Employment Benefits Other Than Pensions," which requires that the unfunded accrued actuarial liability for post-employment benefits be recognized over a thirty-year amortization period. See Note H in the Notes to Financial Statements for more information.

Contacting the Commission's Financial Management

This financial report is designed to provide the citizens, taxpayers and legislators of New Jersey and Pennsylvania, and the users of the Commission's bridges, with a general overview of the Commission's finances and to demonstrate the Commission's accountability for the revenues that it receives. If you have questions about this report or need additional financial information, contact the Commission at (215) 295-5061 or visit its website at: www.drjtbc.org.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

STATEMENTS OF NET ASSETS

		December 31,	
		2008	2007
ASSETS			
Current Assets			
Unrestricted			
Cash and equivalents	\$	2,345,098	\$ 5,370,481
Other assets		1,129,632	2,161,765
E-ZPass clearing account		4,557,594	5,087,063
Total Unrestricted		<u>8,032,324</u>	<u>12,619,309</u>
Restricted			
Cash and equivalents		59,226,794	62,035,676
Accrued interest on investments		2,754,729	1,680,078
Total Restricted		<u>61,981,523</u>	<u>63,715,754</u>
Total Current Assets		<u>70,013,847</u>	<u>76,335,063</u>
Non-Current Assets			
Unrestricted			
Investments		3,494,462	3,794,910
Restricted			
Investments		381,541,182	462,017,468
Bond issuance costs		6,179,230	6,452,157
Capital assets		411,553,216	316,544,247
Total Restricted		<u>799,273,628</u>	<u>785,013,872</u>
Total Non-Current Assets		<u>802,768,090</u>	<u>788,808,782</u>
Total Assets		<u>\$872,781,937</u>	<u>\$865,143,845</u>
LIABILITIES AND NET ASSETS			
Current Liabilities			
Accounts payable and accrued expenses	\$	5,588,119	\$ 3,768,182
E-ZPass customer accounts		3,826,724	3,460,856
Accrued interest on bond indebtedness		7,932,238	6,400,327
Compensated absences - current portion		120,000	120,000
Accrued post-employment benefits obligation - current portion		2,193,000	-
Bridge system revenue bonds, series 2003, 2005A, 2007A, and 2007B - current portion		11,230,393	9,790,393
Premium on bonds - current portion		1,212,704	1,311,327
Total Current Liabilities		<u>32,103,178</u>	<u>24,851,085</u>
Non-Current Liabilities			
Compensated absences - non-current portion		1,872,912	1,836,629
Accrued post-employment benefits obligation - non-current portion		6,881,200	-
Bridge system revenue bonds, series 2003, 2005A, 2007A, and 2007B - non-current portion		437,590,752	448,821,145
Premium on bonds - non-current portion		8,411,922	9,624,626
Total Non-Current Liabilities		<u>454,756,786</u>	<u>460,282,400</u>
Total Liabilities		<u>486,859,964</u>	<u>485,133,485</u>
Net Assets (Deficit)			
Invested in capital assets, net of related debt		205,515,765	198,100,157
Restricted		189,361,377	174,681,651
Unrestricted deficit		(8,955,169)	7,228,552
Total Net Assets		<u>385,921,973</u>	<u>380,010,360</u>
Total Liabilities and Net Assets		<u>\$872,781,937</u>	<u>\$865,143,845</u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

STATEMENTS OF REVENUES, EXPENSES AND CHANGES IN NET ASSETS

	Year Ended December 31,	
	2008	2007
Operating Revenues		
Toll bridge operations		
Cash toll revenues, net	\$ 30,001,180	\$ 32,192,163
E-ZPass toll revenues, net	55,992,420	53,181,829
Miscellaneous revenues	165,506	129,504
Total toll revenues	86,159,106	85,503,496
Operating Expenses		
Toll bridge operating expenses		
Operating and maintenance expenses	30,256,993	27,315,631
Administrative expenses	8,317,635	7,334,720
Toll-supported bridge expenses	8,902,476	7,925,504
Total operating expenses	47,477,104	42,575,855
Net Operating Revenues	38,682,002	42,927,641
Non-Operating Revenues (Expenses)		
Investment return	16,667,333	15,801,031
Interest on bond indebtedness	(19,798,441)	(11,516,056)
Amortization of bond premium	1,311,326	1,169,147
Amortization and write-off of bond issuance costs	(1,224,466)	(253,989)
Amortization of loss on defeasance	(109,607)	(109,607)
Compact Authorized Investment program	(12,504,651)	(1,714,410)
Emergency repairs reimbursement (expense)	1,379,742	42,268
Depreciation	(13,665,224)	(13,198,186)
Amortization of post-employment benefit obligation	(5,997,000)	-
Gain on sale of fixed assets	1,170,599	194,688
Total other expenses	(32,770,389)	(9,585,114)
Change in net assets	5,911,613	33,342,527
Net assets, beginning of year	380,010,360	346,667,833
Net assets, end of year	\$ 385,921,973	\$ 380,010,360

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

STATEMENTS OF CASH FLOWS

	Year Ended December 31,	
	2008	2007
Cash Flows from Operating Activities		
Receipts from cash tolls	\$ 30,001,180	\$ 32,192,163
Receipts from E-ZPass	56,887,757	53,499,371
Payments to suppliers, employees and others	(41,511,551)	(45,601,432)
Other receipts	165,506	129,504
Net cash provided by operating activities	45,542,892	40,219,606
Cash Flows from Investing Activities		
Sales (purchases) of investments, net	84,405,331	(267,105,307)
Investment return	13,130,834	14,641,016
Compact Authorized Investment program expense	(12,504,651)	(1,714,410)
Emergency repairs reimbursement	1,379,742	42,268
Purchases of capital assets	(103,777,618)	(58,564,709)
Net cash used in investing activities	(17,366,362)	(312,701,142)
Cash Flows from Financing Activities		
Bond proceeds, including premium	-	289,133,086
Principal paid on bond and notes indebtedness	(9,900,000)	(5,795,000)
Interest paid on bond indebtedness	(23,163,106)	(10,847,085)
Bond issuance costs	(947,689)	(4,236,575)
Net cash (used in) provided by financing activities	(34,010,795)	268,254,426
Net decrease in cash and cash equivalents	(5,834,265)	(4,227,110)
Cash and equivalents, beginning of year	67,406,157	71,633,267
Cash and equivalents, end of year	\$ 61,571,892	\$ 67,406,157
Reconciliation of net operating revenues to net cash provided by operating activities		
Net operating revenues	\$ 38,682,002	\$ 42,927,641
Adjustment for normal cost of post-employment benefits	3,077,200	-
Changes in net assets and liabilities		
Other assets	1,032,133	(156,539)
E-ZPass clearing account	529,469	40,943
Accounts payable and accrued expenses	1,819,937	(2,952,427)
E-ZPass customer accounts	365,868	276,599
Compensated absences	36,283	83,389
Net cash provided by operating activities	\$ 45,542,892	\$ 40,219,606
Non-cash investing activities		
Unrealized gain on investments	\$ 2,461,849	\$ 1,316,049

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

A. AUTHORIZED LEGISLATION AND NATURE OF ORGANIZATION

The Delaware River Joint Toll Bridge Commission (the "Commission"), a body corporate and politic, was created in 1934 by a compact, subsequently amended and supplemented, between the Commonwealth of Pennsylvania (the "Commonwealth") and the State of New Jersey, with the approval of the Congress of the United States. The Commission is authorized and empowered, with federal government approval required in certain cases, to acquire, construct, administer, operate and maintain such bridges as the Commission deems necessary to advance the interests of the two states, to issue bonds and other obligations, and to make payment of interest thereon. The compact provides that Commission indebtedness shall not be deemed to constitute a debt or liability or a pledge of the faith and credit of the two states or any subdivision thereof.

In 1985, a proposed compact change was enacted and approved by the State of New Jersey that was similar to the legislation that had been enacted by the Commonwealth in 1984. This proposed compact change received the required consent of the Congress of the United States in early 1987. The compact, as approved, required the Commission to refinance its bonded indebtedness. In addition, the Commission was obligated to assume full financial responsibility for the cost of operating and maintaining the toll-supported bridges that were financed by appropriations from the Commonwealth and the State of New Jersey. Accordingly, on July 1, 1987, the Commission defeased all of its then-outstanding bonded indebtedness. Due to this compact change, the accompanying financial statements include the operations of the toll-supported bridges.

The Commission has jurisdiction for vehicular and pedestrian traffic across the Delaware River between the Commonwealth of Pennsylvania and the State of New Jersey from the Philadelphia/Bucks County line to the New York state line. The Commission's duties include the maintenance and operation of all the bridges over the Delaware River in its jurisdiction, with the following exceptions: the New Jersey-Pennsylvania Turnpike Bridge and the Burlington-Bristol Toll Bridge, both south of Trenton, and the Dingman's Ferry Toll Bridge, which is north of the Delaware Water Gap.

Effective with the issuance of the 1988 Bridge System and I-78 Revenue Bonds and pursuant to the respective bond resolutions, the financial activity of the I-78 Bridge was previously reported separately from that of the Commission. Due to the in-substance defeasance of the 1988 Bridge System and I-78 Revenue Bonds, effective with the 1992 financial statements, the financial activity of the I-78 Bridge is included with that of the Bridge System.

B. SUMMARY OF SIGNIFICANT ACCOUNTING POLICIES

Basis of Accounting

The financial statements of the Commission have been prepared under the economic resources measurement focus, on the accrual basis of accounting and in accordance with accounting principles generally accepted in the United States of America that are applicable to governmental proprietary-type funds. Revenues are recognized when earned, and expenses are recognized when incurred.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

B. SUMMARY OF SIGNIFICANT ACCOUNTING POLICIES (CONTINUED)

Basis of Accounting (Continued)

GASB Statement No. 20, *Accounting and Financial Reporting for Proprietary Funds and Other Governmental Entities that Use Proprietary Fund Accounting*, provides proprietary activities with a choice of authoritative guidance issued after November 30, 1989. The Commission has elected to follow GASB pronouncements exclusively after that date.

Revenues

Revenues consist primarily of cash tolls and E-ZPass revenues. Cash toll revenues are recognized as received. E-ZPass revenues are recognized when vehicles with E-ZPass utilize the Commission's toll bridges. Prepayments received from the Commission's E-ZPass customers are deferred and recognized as revenue as utilized at the Commission's toll bridges. Investment income is recognized when earned.

Basis of Investments

The Commission has adopted GASB No. 31, *Accounting and Financial Reporting for Certain Investments and for External Investment Pools*. Under GASB No. 31, investments in equity securities with readily determinable fair values, and all investments in debt securities, are reported at fair value, with gains and losses included in the statement of revenues, expenses and changes in net assets.

Cash Equivalents

For the purpose of the statement of cash flows, cash equivalents include certificates of deposit and all highly liquid debt instruments with original maturities of ninety days or less. Deposits are with contracted depository banks in interest-bearing accounts, which are insured pursuant to the requirements of Act 72 of the General Assembly of the Commonwealth of Pennsylvania, approved August 6, 1991.

Fund Groups

In accordance with the Bond Resolution relating to the Bridge System Revenue Bonds, Series 2003 and Series 2005A, and Series 2007A and B, the Commission has established the following funds and accounts:

Construction Fund – Bond proceeds for project costs are deposited into this fund.

Revenue Fund – All revenues received by the Commission are deposited in the Revenue Fund. No later than the last business day of each month, the Commission shall withdraw from the Revenue Fund and deposit to the Operating Fund the amount equal to (i) the amount shown by the annual operating budget to be necessary to pay current expenses for the ensuing month, and (ii) an amount determined by a Commission official as being reasonably necessary to pay current expenses which are expected for each month, after taking into account the amount on deposit in the Operating Account (including the amount described in clause (i) above), it being recognized that the annual operating budget may have to be amended accordingly.

Operating Account – Amounts on deposit in the Operating Account are used by the Commission to pay the Commission's operating expenses.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

B. SUMMARY OF SIGNIFICANT ACCOUNTING POLICIES (CONTINUED)

Fund Groups (Continued)

Debt Service Fund – Transfers are made from the Revenue Fund to the Debt Service Fund to provide for the debt service on all series of bonds. Payments are made from the Debt Service Fund for interest on the bonds, for principal installments on the bonds, and for the redemption price for any bonds to be redeemed.

Debt Service Reserve Fund – Transfers are made to this fund from the Revenue Fund in an amount necessary to meet the Debt Service Reserve Requirement. Amounts held in the Debt Service Reserve Fund shall be used for the purpose of paying interest on maturing principal and mandatory sinking fund redemption price of Debt Service Reserve Fund Bonds whenever and to the extent that the monies held for the credit of the Debt Service Fund shall be insufficient for such purpose.

Reserve Maintenance Fund – On or before the last business day of each month, the Commission shall transfer the amount shown in the annual capital budget for the ensuing month from the Revenue Fund to the credit of the Reserve Maintenance Fund.

General Reserve Fund – On or before the last business day of each month (or more frequently, if desired) the Commission transfers from the Revenue Fund to the credit of the General Reserve Fund any funds which a Commission official determines to be in excess of the amount required to be reserved therein for future transfers to the Debt Service Fund.

Monies in the General Reserve Fund may be expended by the Commission to restore deficiencies in any funds or accounts created under the Trust Indenture and, absent any such deficiency, for any of the following purposes, with no one item having priority over any of the others:

- (a) To purchase or redeem bonds.
- (b) To secure and pay the principal or redemption price of and any interest on any subordinated indebtedness.
- (c) To make payments into the Construction Fund.
- (d) To fund improvements, extensions and replacements of the Bridge System.
- (e) As a self-insurance reserve.
- (f) To further any corporate purpose.

The Commission is authorized to apply monies on deposit in the General Reserve Fund for any of these purposes.

Rebate Fund - Amounts on deposit in the Rebate Fund may be used solely to make payments to the United States of America under Section 148 of the Internal Revenue Code and to pay costs related to the calculation of the amounts due. Upon satisfaction of the Commission's covenants to calculate and pay Section 148 requirements, any amounts remaining in the Rebate Fund shall be deposited in the General Reserve Fund.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

B. SUMMARY OF SIGNIFICANT ACCOUNTING POLICIES (CONTINUED)

Net Assets

Invested in Capital Assets, Net of Related Debt

The net assets invested in capital assets represent the cost basis of capital assets, less the related accumulated depreciation, less the bonds outstanding and unspent bond proceeds that were used to finance the acquisition of the capital assets.

Restricted

In accordance with the terms of the bond resolution, cash and equivalents of all funds required under such bond resolution are classified as restricted assets. The amounts by which the restricted assets exceed the corresponding liabilities they will liquidate constitute restrictions of net assets, as these excesses are not available for the payment of current operating expenses. Such net assets are restricted primarily for capital projects.

Unrestricted

The unrestricted net assets represent resources available for current operating expenses in compliance with legal restrictions.

Capital Assets

Purchased or constructed capital assets are recorded at cost or estimated historical cost. Infrastructure assets acquired prior to January 1, 2003, are reported primarily at estimated historical cost using deflated replacement cost. The Commission capitalizes purchases of property and equipment of \$5,000 or more. Depreciation is provided over the estimated useful lives of the assets using the straight-line method. The estimated useful lives are as follows:

Infrastructure	15-50 years
Vehicles	5-15 years
Office furniture and equipment	5-7 years

The cost of maintenance and repairs that do not add to the value of the asset or materially extend assets' lives are expensed when incurred.

Capitalization of Interest

The Commission capitalizes interest related to projects under construction. Capitalized interest amounted to \$4,896,576 and \$1,805,495 for 2008 and 2007, respectively.

Use of Estimates

The preparation of financial statements in conformity with accounting principles generally accepted in the United States of America requires management to make estimates and assumptions that affect certain reported amounts and disclosures. Accordingly, actual results could differ from those estimates. In addition, certain prior year amounts have been reclassified to conform to current year presentation.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

B. SUMMARY OF SIGNIFICANT ACCOUNTING POLICIES (CONTINUED)

Post-Employment Benefits Other Than Pensions ("OPEB")

In 2008, the Commission implemented Governmental Accounting Standards Board ("GASB") Statement No. 45, "Accounting and Financial Reporting by Employers for Post-Employment Benefits Other Than Pensions." This statement requires that employers recognize annual OPEB cost equal to the annual required contribution and recognize the unfunded accrued actuarial liability over an amortization period of thirty years.

Deferred Bond Costs

Costs related to the issuance of bonds, including legal, printing and financing costs, are capitalized and amortized by the interest method over the life of the bonds until maturity.

Rounding

Some schedules in the financial statements may have dollar differences due to rounding adjustments.

C. CASH AND EQUIVALENTS AND INVESTMENTS

General Information

The Commission's cash and equivalents and investments are summarized as follows:

	December 31,	
	2008	2007
Cash and equivalents	\$ 61,571,892	\$ 67,406,157
Investments	385,035,644	465,812,378
	<u>\$ 446,607,536</u>	<u>\$ 533,218,535</u>

Included in the above balances as of December 31, 2008 and 2007, respectively, are approximately \$258.4 million and \$339.8 million of unspent bond proceeds that are restricted by the trust indenture for use only in capital projects and debt service reserve requirements. Detailed supplementary information related to the above is included on pages 25-29 of these financial statements.

Investment Policy

The primary objectives of the Commission's investment policy are safety of principal, liquidity and yield.

Safety of principal is the foremost objective of the investment program. Investments are undertaken in a manner that seeks to ensure the preservation of capital in the overall portfolio. The objective is to mitigate credit risk and interest rate risk. The Commission's policies for limiting credit risk and interest rate risk are described below.

The portfolio is designed to remain sufficiently liquid to meet all requirements that may be reasonably anticipated. This is accomplished by structuring the portfolio so that securities mature concurrent with cash needs to meet anticipated demands. Since all possible cash demands cannot be anticipated, the portfolio consists largely of securities with active secondary or resale markets. Also, a portion of the portfolio is placed in money market mutual funds or local government investment pools, which offer same-day liquidity for short-term funds.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

C. CASH AND EQUIVALENTS AND INVESTMENTS (CONTINUED)

Investment Policy (Continued)

The investment portfolio is designed with the objective of attaining a market rate of return throughout budgetary and economic cycles, taking into account the investment risk constraints and liquidity needs. Return on investment is of secondary importance compared to the safety and liquidity objectives described above. The core of investments are limited to relatively low risk securities in anticipation of earning a fair return relative to the risk being assumed. Securities are not permitted to be sold prior to maturity except under the following conditions:

- 1) A security with declining credit may be sold early to minimize loss of principal.
- 2) A security swap would improve the quality, yield, or target duration in the portfolio.
- 3) Liquidity needs of the portfolio require that the security be sold.

Custodial Credit Risk - Deposits

Custodial credit risk is the risk that, in the event of a bank failure, the Commission's deposits may not be returned to it. The Commission does not have a deposit policy for custodial credit risk. As of December 31, 2008 and 2007, the Commission's cash balances were as follows:

	December 31, 2008		December 31, 2007	
	Financial Statement Balance	Bank Balance	Financial Statement Balance	Bank Balance
Amount insured by the FDIC or collateralized with securities held in its name by the Commission.	\$ 1,789,857	\$12,092,154	\$ 5,202,971	\$ 5,238,500
Amount collateralized with securities held by the pledging financial institution's trust department in the Commission's name.	59,708,185	59,708,185	62,129,086	62,129,086
Uninsured Petty cash and collectors' change funds	73,850	-	74,100	-
	<u>\$61,571,892</u>	<u>\$71,800,339</u>	<u>\$67,406,157</u>	<u>\$67,367,586</u>

Credit Risk - Investments

The Commission minimizes credit risk, which is the risk of loss due to the failure of the security issuer or backer by limiting investments to the safest type of securities, pre-qualifying the financial institutions, broker/dealers, intermediaries, and advisors with

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

C. CASH AND EQUIVALENTS AND INVESTMENTS (CONTINUED)

which the Commission will do business, and diversifying the investment portfolio so that potential losses on individual securities will be minimized. As of December 31, 2008, the Commission's investments were rated AAA by Standard & Poor's, AAA by Fitch Ratings, and Aaa by Moody's Investors Service. The Commission historically has not experienced any credit related losses with respect to their investment in these securities. U.S. Treasury notes are explicitly guaranteed by the U.S. government and are not subject to credit risk or custodial credit risk. The Commission's investment in the Pennsylvania Investment Fund is also excluded from credit risk and custodial credit risk as a pooled investment.

Interest Rate Risk

The Commission minimizes the risk that the market value of securities in the portfolio will fall due to changes in general interest rates by structuring the investment portfolio so that securities mature to meet any cash requirements associated with individual funds, which avoids selling the security prior to maturity. The Commission also invests operating funds primarily in shorter-term securities, money market mutual funds, or local government investment pools.

As of December 31, 2008, the Commission had the following investments and maturities:

Investment Type	Fair Value	Investment Maturities (in Years)			
		Less Than 1	1-5	6-10	More Than 10
FFCB	\$ 1,015,000	\$ 1,015,000	\$ -	\$ -	\$ -
FHLB	161,046,452	116,831,594	44,214,858	-	-
FHLMCDN	139,912,737	98,222,037	41,690,700	-	-
GECC	4,925,750	4,925,750	-	-	-
FNMADN	69,628,724	41,408,754	28,219,970	-	-
U.S. TBILLS	3,482,093	3,482,093	-	-	-
PA INVEST	5,024,888	5,024,888	-	-	-
Total	<u>\$385,035,644</u>	<u>\$270,910,116</u>	<u>\$114,125,528</u>	<u>\$ -</u>	<u>\$ -</u>

As of December 31, 2007, the Commission had the following investments and maturities:

Investment Type	Fair Value	Investment Maturities (in Years)			
		Less Than 1	1-5	6-10	More Than 10
FFCB	\$ 1,001,250	\$ -	\$ 1,001,250	\$ -	\$ -
FHLB	110,674,755	50,524,395	60,150,360	-	-
FHLBCDN	59,861,000	59,861,000	-	-	-
FHLBDN	99,765,000	99,765,000	-	-	-
FHLMCDEBS	6,178,672	6,178,672	-	-	-
FHLMCDN	71,464,290	71,464,290	-	-	-
FHLMCMTN	4,994,250	4,994,250	-	-	-
FHLMCN	9,002,920	-	9,002,920	-	-
FNMADEBS	7,711,938	-	7,711,938	-	-
FNMA	7,272,950	4,967,200	2,305,750	-	-
FNMADN	86,711,600	86,711,600	-	-	-
PA INVEST	1,173,753	1,173,753	-	-	-
Total	<u>\$465,812,378</u>	<u>\$385,640,160</u>	<u>\$ 80,172,218</u>	<u>\$ -</u>	<u>\$ -</u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

D. CAPITAL ASSETS

Capital assets activities for the year ended December 31, 2008, were as follows:

	<u>December 31,</u> <u>2007</u>	<u>Additions</u>	<u>Reductions</u>	<u>December 31,</u> <u>2008</u>
Non-Depreciable Assets				
Land	\$129,619,844	\$ 268,322	\$ -	\$129,888,166
Infrastructure in progress	85,849,253	106,057,023	14,207,148	177,699,128
Depreciable Assets				
Bridges/road network	258,361,828	13,910,473	-	272,272,301
Equipment	<u>27,577,053</u>	<u>2,645,523</u>	<u>380,086</u>	<u>29,842,490</u>
Total at Historical Cost	<u>501,407,978</u>	<u>122,881,341</u>	<u>14,587,234</u>	<u>609,702,085</u>
Less Accumulated Depreciation				
Bridge/road network	170,193,357	9,868,882	-	180,062,239
Equipment	<u>14,670,374</u>	<u>3,796,342</u>	<u>380,086</u>	<u>18,086,630</u>
Total Accumulated Depreciation	<u>184,863,731</u>	<u>13,665,224</u>	<u>380,086</u>	<u>198,148,869</u>
Total Capital Assets	<u>\$316,544,247</u>	<u>\$109,216,117</u>	<u>\$ 14,207,148</u>	<u>\$411,553,216</u>
Depreciation expense was as follows:				
Bridges/road networks	\$ 9,868,882			
Equipment	<u>3,796,342</u>			
Total Depreciation Expense	<u>\$ 13,665,224</u>			

Capital assets activities for the year ended December 31, 2007, were as follows:

	<u>December 31,</u> <u>2006</u>	<u>Additions</u>	<u>Reductions</u>	<u>December 31,</u> <u>2007</u>
Non-Depreciable Assets				
Land	\$129,619,844	\$ -	\$ -	\$129,619,844
Infrastructure in progress	29,025,138	58,965,002	2,140,887	85,849,253
Depreciable Assets				
Bridges/road network	256,220,941	2,140,887	-	258,361,828
Equipment	<u>26,851,718</u>	<u>1,405,203</u>	<u>679,868</u>	<u>27,577,053</u>
Total at Historical Cost	<u>441,717,641</u>	<u>62,511,092</u>	<u>2,820,755</u>	<u>501,407,978</u>
Less Accumulated Depreciation				
Bridges/road network	160,619,693	9,573,664	-	170,193,357
Equipment	<u>11,725,263</u>	<u>3,624,522</u>	<u>679,411</u>	<u>14,670,374</u>
Total Accumulated Depreciation	<u>172,344,956</u>	<u>13,198,186</u>	<u>679,411</u>	<u>184,863,731</u>
Total Capital Assets	<u>\$269,372,685</u>	<u>\$ 49,312,906</u>	<u>\$ 2,141,344</u>	<u>\$316,544,247</u>
Depreciation expense was as follows:				
Bridges/road networks	\$ 9,573,664			
Equipment	<u>3,624,522</u>			
Total Depreciation Expense	<u>\$ 13,198,186</u>			

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

E. BONDS PAYABLE

The following is a summary of bonds payable:

<u>Bonds Payable</u>	<u>Maturity Dates</u>	<u>Interest Rate</u>	<u>Bonds Outstanding (in thousands) December 31, 2007</u>	<u>Additions</u>	<u>Reductions</u>	<u>Bonds Outstanding (in thousands) December 31, 2008</u>	<u>Amounts due within one year</u>
2003 series revenue bonds	2003-2024	3.00%-5.25%	\$ 75,680	\$ -	\$ 5,115	\$ 70,565	\$ 5,370
2003 series revenue bonds	2025-2028	5.00%	29,390	-	-	29,390	-
2005A series revenue bonds	2005-2025	4.00%-5.50%	57,665	-	965	56,700	1,005
2005A series revenue bonds	2026-2030	4.50%	12,825	-	-	12,825	-
2007A series revenue bonds	2008-2027	4.25%-5.00%	40,200	-	470	39,730	1,615
2007A series revenue bonds	2028-2031	5.00%	13,100	-	-	13,100	-
2007A series revenue bonds	2032-2035	5.00%	47,730	-	-	47,730	-
2007A series revenue bonds	2036-2037	4.50%	33,140	-	-	33,140	-
2007B series revenue bonds	2008-2032	variable	75,000	-	1,675	73,325	1,675
2007B series revenue bonds	2032	variable	<u>75,000</u>	<u>-</u>	<u>1,675</u>	<u>73,325</u>	<u>1,675</u>
Total bond principal payable			459,730	-	9,900	449,830	11,340
Loss on defeasance			<u>(1,118)</u>	<u>-</u>	<u>(109)</u>	<u>(1,009)</u>	<u>(110)</u>
Net bonds payable			<u>\$ 458,612</u>	<u>\$ -</u>	<u>\$ 9,791</u>	<u>\$ 448,821</u>	<u>\$ 11,230</u>

Debt service requirements on bonds outstanding at December 31, 2008, are as follows (in thousands):

	<u>Principal</u>	<u>Interest</u>	<u>Total</u>
2009	\$ 11,340	\$ 21,343	\$ 32,683
2010	11,740	20,743	32,483
2011	12,420	20,226	32,646
2012	13,015	19,564	32,579
2013	13,650	18,929	32,579
2014-2018	77,315	83,952	161,267
2019-2023	78,180	64,961	143,141
2024-2028	99,180	41,900	141,080
2029-2033	69,715	24,502	94,217
2034-2037	<u>63,275</u>	<u>7,514</u>	<u>70,789</u>
	<u>\$ 449,830</u>	<u>\$ 323,634</u>	<u>\$ 773,464</u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

E. BONDS PAYABLE (CONTINUED)

Defeasance of Series 2003 Bonds

In March 2005, the Commission issued \$72,645,000 Bridge System Revenue Bonds, Series 2005A. The proceeds of the bonds were used to advance-refund \$32,165,000 of the Commission's Bridge System Revenue Bonds, Series 2003. This refunding was done to achieve interest cost savings. Proceeds of the bonds were used to establish an irrevocable escrow account. Funds in the escrow account were invested in special direct obligations of the United States Treasury or other obligations of the United States government or its agencies. The escrow securities and their earnings are structured to pay the principal and interest on the refunded 2003 bonds as such payments become due, until the call dates of the respective refunded bonds, at which time the escrow account will pay the principal of the refunded bonds at a price of par plus accrued interest. Since these funds have been placed in an irrevocable trust, they are considered defeased for these financial statements.

Refunded Series 2003 bonds outstanding at December 31, 2008, consist of the following:

Maturity Date	Interest Rate	Principal Due
2014	5.25 %	\$ 3,920,000
2015	5.25 %	4,125,000
2016	5.25 %	4,345,000
2017	5.25 %	4,570,000
2018	5.25 %	4,815,000
2019	5.25 %	5,060,000
2020	5.25 %	5,330,000
		\$ 32,165,000

The advance-refunding resulted in a difference between the reacquisition price and the net carrying amount of the old debt of approximately \$1.4 million. The accumulated loss on defeasance is reported as a contra-liability on the statement of net assets and is being charged to net assets using a method which approximates the effective interest method over the shorter of the remaining life of the old debt or the life of the new debt. The accumulated capitalized loss on defeasance at December 31, 2008 and 2007, was \$1,008,855 and \$1,118,462, respectively.

Bridge System Revenue Bonds, Series 2007 (SWAP)

Objective of the swaps. In October of 2005, the Commission entered into two forward starting swaps with two counterparties to hedge against future interest rates. The intention of the swaps was to take advantage of the current historically low interest rate environment in advance of the issuance of bonds by the Commission (as authorized by its trust indenture) in 2007.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

E. BONDS PAYABLE (CONTINUED)

Bridge System Revenue Bonds, Series 2007 (SWAP) (Continued)

Terms. The swaps were entered into with Merrill Lynch Capital Services, Inc. ("MLCS") and Morgan Stanley Capital Services, Inc. ("MSCS"). The swaps were effective on October 1, 2007, and will mature on July 1, 2032. On the trade date, MLCS and MSCS were both rated AA- by Standard & Poor's Ratings Services ("S&P"), a division of The McGraw-Hill Companies, and Aa3 by Moody's Investors Service, Inc. ("Moody's"). The swaps were priced at a fixed rate of 4.231% based on an amortizing notional schedule with a combined \$150,000,000 initial notional amount. Under the swaps starting October 1, 2007, the Commission pays a fixed rate of 4.231% and receives a variable payment equal to the Bond Market Association Municipal Swap Index (the "BMA" Index). The bonds' variable-rate coupons, when issued, is based on a remarketing rate that is highly correlated to the BMA Index. As part of the swap transactions, the Commission also purchased two interest rate swap insurance policies dated October 6, 2005, issued by MBIA Insurance Corporation for the account of the Commission, as principal, and the counterparties, as beneficiary. The insurance policies provide for risk mitigation and limit the need for the Commission to post eligible collateral.

Fair Value. As of December 31, 2008 and 2007, the swaps had a negative fair value of \$23,997,604 and \$9,319,466, respectively. The fair value was estimated using the zero-coupon method. This method calculates the future net settlement payments required by the swap, assuming that the current forward rates implied by the yield curve correctly anticipate future spot interest rates. These payments are then discounted using the spot rates implied by the current yield curve for hypothetical zero-coupon bonds due on the date of each future net settlement of the swap.

Credit Risk. As of December 31, 2008 and 2007, the Commission was not exposed to credit risk because the swaps had a negative fair value. Should interest rates change and the fair value of the swaps become positive, the Commission would be exposed to credit risk in the amount of the swaps' fair value. Agreed upon collateral threshold levels per the Credit Support Annex ("CSA") require collateral to be posted based on counterparty ratings as set forth in the CSA.

Termination Risk. The swaps are governed by the International Swap Dealers Association Master Agreement, which includes standard termination events. In addition, the swaps may be terminated if the long-term, unenhanced rating on the bonds issued by the Commission is withdrawn, suspended or falls below Baa3 as determined by Moody's, or BBB- as determined by S&P. Furthermore, the swaps may be terminated if the counterparties' credit support provider fails to have any rated long-term, unsecured, unenhanced senior debt or if the rating of the senior debt is withdrawn, suspended or falls below Baa2 as determined by Moody's, or BBB as determined by S&P.

In connection with the aforementioned swaps, no amounts are recorded in the financial statements other than the prepaid cost of issuance of the swaps.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

F. PENSION PLAN

The Commission contributes to the Commonwealth of Pennsylvania State Employees' Retirement System (the "System"). The System is the administrator of a cost-sharing, multiple-employer, defined-benefit retirement system. The System was established by the Commonwealth to provide retirement, death and disability benefits for employees of state government and certain independent agencies. Ad hoc cost-of-living adjustments are provided at the discretion of the General Assembly. Article II of the Commonwealth's Constitution assigns the authority to establish and amend the benefit provisions of the plan to the General Assembly. The System issues a publicly available financial report that includes financial statements and required supplementary information for the retirement plan. That report may be obtained by writing to the Commonwealth of Pennsylvania State Employees' Retirement System, 30 North Third Street, P.O. Box 1147, Harrisburg, PA 17108-1147 or by calling 717-787-9657. Employees of the Commission are required to pay 5.00%-6.25% of their salaries into the System, and the Commission is required to contribute at an actuarially determined rate. The rate is computed based upon actuarial valuations on the System's fiscal year end of December 31 and applied to the Commonwealth based on its fiscal year end of June 30. Therefore, the employer contribution rate in effect for the System's year end of December 31 reflects a blended average of calculated rates. The contribution requirements of plan members and the Commission are established and may be amended by the System's board of trustees.

The Commission also has four employees who participate in the Public Employees' Retirement System of New Jersey ("PERS"). PERS is a part of the Division of Pensions in the Department of the Treasury, State of New Jersey. PERS is funded annually based on the projected benefit method with aggregate level normal cost and frozen initial unfunded accrued liability. PERS, which covers public employees throughout the state, does not maintain separate records for each reporting unit, and accordingly, the actuarial data for the employees of the Commission who are members of PERS is not available. The Division of Pensions issues a publicly available financial report for PERS, including financial statements and required supplementary information. Please refer to the New Jersey state website www.state.nj.us for more information regarding the plan. The PERS financial report may be obtained by writing to the State of New Jersey, Department of Treasury, Division of Pensions and Benefits, P.O. Box 295, Trenton, New Jersey 08625-0295.

The Commission's pension contribution for the years ended December 31, 2008 and 2007, was \$620,367 and \$602,219, respectively, which equaled the required contribution.

G. SELF INSURANCE

The Commission self-insures the risk for health insurance claims. In addition to the self-insured risk, the Commission carries a stop-loss policy that limits its exposure to a maximum of \$150,000 per plan year per individual and \$5,538,002 in the aggregate for all active and retired employees under the age of 65.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

H. POST-EMPLOYMENT BENEFITS

The Commission provides certain post-employment life and health insurance benefits to its employees if they retire while working for the Commission. As of December 31, 2007 and prior, in accordance with the provisions of Statement No. 12 of the Governmental Accounting Standards Board ("GASB"), "Disclosure of Information on Post-Employment Benefits Other Than Pension Benefits by State and Local Governmental Employers," expenditures for post-employment life and health insurance benefits were recognized on a pay-as-you-go basis and were approximately \$1,971,464 in 2007. As of December 31, 2008 and 2007, 135 and 124 retired employees, respectively, were eligible for both life and health insurance benefits. As of December 31, 2008, three other retired employees were eligible for health insurance benefits only. An additional 33 and 37 retired employees were eligible for life insurance benefits only as of December 31, 2008 and 2007, respectively, in a range of \$2,000-\$4,000 per person.

Effective for the year ended December 31, 2008, the Commission adopted the provisions of GASB Statement No. 45, "Accounting and Financial Reporting by Employers of Post-Employment Benefits Other Than Pensions", which requires that the unfunded accrued actuarial liability be recognized in the Commission's financial statements over a 30-year amortization period. The post-employment benefit plan does not issue stand-alone financial statements and is not included in the report of another entity.

Funding Policy

It is the Commission's policy at this time to fund the post-employment benefits plan on a pay-as-you-go basis.

Annual Post-Employment Benefits Cost and Net Post-Employment Benefits Obligation

The Commission's annual post-employment benefits cost and net post-employment benefits obligation to the plan for the year ended December 31, 2008, were:

Service cost (with interest)	\$ 4,890,000
Accrued actuarial liability amortization	<u>5,997,000</u>
Total accrual	10,887,000
Contributions made	<u>1,812,800</u>
Increase in net post-employment benefits obligation	9,074,200
Net post-employment benefits obligation, beginning of year	<u>-</u>
Net post-employment benefits obligation, end of year	<u><u>\$ 9,074,200</u></u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

H. POST-EMPLOYMENT BENEFITS (CONTINUED)

The annual required contribution for the current year was determined as part of the January 1, 2008 actuarial valuation using the entry age actuarial cost method. The actuarial assumptions included (a) 3.5% interest rate since the plan is not currently funded, and (b) medical trend rate of 11% grading down to 5% in year 2020 and thereafter. The actuarial value of assets was determined using techniques that smooth the effects of short-term volatility in the market value of investments over a five year period. The unfunded actuarial accrued liability is being amortized on the level dollar amortization method over a thirty year period. The remaining amortization period at December 31, 2008, was 29 years.

Trend Information

<u>Year Ended</u> <u>December 31,</u>	<u>Annual Post- Employment</u> <u>Benefits Cost</u>	<u>Percentage of</u> <u>Annual</u> <u>Cost Contributed</u>	<u>Net Post- Employment</u> <u>Benefits Obligation</u>
2008	\$ 10,887,000	17 %	\$ 9,074,200

Funded Status

The funded status of the plan as of the most recent valuation date of January 1, 2008, is as follows:

<u>Actuarial</u> <u>Valuation</u> <u>Date</u>	<u>Actuarial</u> <u>Value</u> <u>of Assets</u>	<u>Actuarial</u> <u>Accrued</u> <u>Liability (AAL)</u>	<u>Unfunded</u> <u>AAL</u> <u>(UAAL)</u>	<u>Funded</u> <u>Ratio</u>	<u>Covered</u> <u>Payroll</u>	<u>UAAL as a %</u> <u>of Covered</u> <u>Payroll</u>
January 1, 2008	\$ -	\$ 110,300,000	\$ 110,300,000	0%	\$18,000,000	613 %

I. COMMITMENTS AND CONTINGENCIES

The Commission is involved in various claims and lawsuits arising in the normal course of business, including claims for right-of-way acquisition, handicapped discrimination, construction contract disputes, personal injury and hiring practices. In the opinion of management, the ultimate outcome of these claims and lawsuits will not have a material adverse effect on the Commission's financial position.

In 2004, the Commission established a \$40 million dollar program, which is included in restricted net assets, to provide funding for transportation infrastructure related projects in New Jersey and Pennsylvania communities that host its bridges. As of December 31, 2008, the Commission had committed \$37,400,447 in grants to municipalities participating in the Compact Authorized Investment ("CAI") program, of which \$23,571,658 was unexpended at December 31, 2008. Examples of appropriate projects that would be considered for funding under the CAI program include installation of upgrades to traffic signalization around Commission facilities, road widening in areas affected by Commission crossings, bicycle or pedestrian paths leading up to Commission facilities, park and ride facilities, safety lighting, and right of way renovation, protection, or beautification.

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

NOTES TO FINANCIAL STATEMENTS

I. COMMITMENTS AND CONTINGENCIES (CONTINUED)

In 2001, the Commission approved a 10-year, \$526 million Capital Improvement Program for the protection, preservation, management and enhancement of the 20 bridges it owns, maintains and operates. With the addition of the CAI program, along with additions and changes in the original projects, the Capital Improvement Program currently stands well in excess of the original amount approved in 2001. As of December 31, 2008, the Commission has approved more than \$307.1 million in contracts to study and improve various facilities and systems as part of that program. At December 31, 2008, the Commission had approved contracts that had not yet been completed or paid totaling approximately \$96.8 million.

J. ARBITRAGE RULES

The Commission is subject to certain arbitrage rules pursuant to current federal income tax law and in accordance with the Trust Indenture. Under these rules, interest earnings on certain investments of proceeds of the Commission's bonds are subject to the limitations imposed by the arbitrage provisions of the Internal Revenue Code. The Commission is required to rebate certain arbitrage profits on non-purpose investments at least once every five years. At December 31, 2008 and 2007, there were no material arbitrage profits subject to rebate.

K. NEW ACCOUNTING PRONOUNCEMENT

GASB Statement No. 53, *Accounting and Financial Reporting for Derivative Instruments*, requires that governments report the fair value of derivative instruments, with the exception of synthetic guaranteed investment contracts, in the financial statements. The changes in fair value of the instruments are recognized in the period to which they relate. The changes in fair value of hedging derivative instruments do not affect investment revenue but are reported as deferrals. On the other hand, the changes in fair value investment derivative instruments (which include ineffective hedging derivative instruments) are reported as part of investment revenue in the current reporting period.

The Commission is required to implement GASB Statement No. 53 for the year ending December 31, 2010.

SUPPLEMENTARY INFORMATION

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION
SCHEDULES OF CASH AND EQUIVALENT BALANCES

December 31, 2008

	Revenue Fund	Operating Fund	Clearing Fund	Construction Fund	Reserve Maintenance Fund	Debt Service Fund	Debt Service Reserve Fund	General Reserve Fund
Total	\$ 59,708,185	\$ 481,391	\$ -	\$ 16,048,807	\$ 1,229,142	\$ 9,089,110	\$ 32,157,981	\$ 701,754
TD Bank	-	887,249	-	-	-	-	-	-
Wachovia Bank	-	902,608	-	-	-	-	-	-
Petty cash and collectors' change funds	-	-	-	-	-	-	-	-
Total cash and equivalent balances	\$ 887,249	\$ 1,457,849	\$ -	\$ 16,048,807	\$ 1,229,142	\$ 9,089,110	\$ 32,157,981	\$ 701,754

December 31, 2007

	Revenue Fund	Operating Fund	Clearing Fund	Construction Fund	Reserve Maintenance Fund	Debt Service Fund	Debt Service Reserve Fund	General Reserve Fund
Total	\$ 62,129,085	\$ 93,409	\$ -	\$ 15,767,626	\$ 207,488	\$ 8,908,187	\$ 26,849,708	\$ 10,302,667
TD Bank	-	5,114,425	-	-	-	-	-	-
Wachovia Bank	-	88,547	-	-	-	-	-	-
Petty cash and collectors' change funds	-	-	-	-	-	-	-	-
Total cash and equivalent balances	\$ 5,114,425	\$ 256,056	\$ -	\$ 15,767,626	\$ 207,488	\$ 8,908,187	\$ 26,849,708	\$ 10,302,667

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

SCHEDULES OF INVESTMENTS

December 31, 2008

<u>Construction Fund</u>						
Security Description	<u>Investment Description</u>			Maturity Date	Cost	Market Value
	Face Value	Rate	Yield			
FHLB	\$ 2,849,000	0.00%	0.66%	02-18-09	\$ 2,844,679	\$ 2,847,148
FNMA	1,654,000	0.00%	0.86%	03-27-09	1,649,275	1,652,263
FNMA	1,583,000	0.00%	1.08%	04-27-09	1,575,982	1,580,546
FHLMC	1,583,000	0.00%	1.17%	05-26-09	1,573,847	1,579,597
FHLB	6,000,000	0.00%	1.08%	05-29-09	5,968,377	5,988,523
FHLMC	1,555,000	0.00%	1.33%	06-26-09	1,543,096	1,550,646
FNMA	1,317,000	0.00%	1.45%	07-24-09	1,304,554	1,312,127
FNMA	1,087,000	0.00%	1.49%	08-24-09	1,075,135	1,081,945
FNMA	710,000	0.00%	1.54%	09-25-09	701,036	706,024
FNMA	710,000	0.00%	1.59%	10-26-09	699,790	705,422
FHLB	1,327,000	0.00%	1.75%	11-16-09	1,304,754	1,317,180
FNMA	11,000,000	0.00%	2.82%	01-09-09	10,886,563	10,900,524
FHLB	29,600,000	2.88%	2.89%	01-30-09	29,597,040	29,655,648
FHLMC	30,000,000	2.81%	2.81%	01-30-09	30,000,000	30,053,400
FHLB	20,000,000	2.88%	2.88%	02-04-09	20,000,000	20,043,800
FHLMC	11,000,000	0.00%	2.83%	02-06-09	10,863,035	10,916,054
FNMA	10,638,000	0.00%	2.72%	02-17-09	10,483,572	10,553,361
FHLB	11,000,000	2.92%	2.92%	09-04-09	11,000,000	11,180,455
FHLB	11,000,000	2.25%	2.80%	10-02-09	10,935,100	11,130,625
FHLMC	11,000,000	3.02%	3.02%	11-03-09	11,000,000	11,040,700
FHLMC	11,000,000	3.07%	3.07%	12-02-09	11,000,000	11,039,380
FHLB	11,000,000	3.01%	3.01%	01-05-10	11,000,000	11,046,420
FNMA	11,000,000	3.10%	3.05%	02-04-10	11,007,370	11,264,715
FHLMC	11,000,000	3.20%	3.20%	03-02-10	11,000,000	11,041,800
Total Construction Fund					<u>\$209,013,205</u>	<u>\$210,188,303</u>

<u>Operating Fund</u>						
Security Description	<u>Investment Description</u>			Maturity Date	Cost	Market Value
	Face Value	Rate	Yield			
FHLB	3,500,000	0.00%	1.14%	04-14-09	\$ 3,484,647	\$ 3,494,462
Total Operating Fund					<u>\$ 3,484,647</u>	<u>\$ 3,494,462</u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

SCHEDULES OF INVESTMENTS (CONTINUED)

December 31, 2008

General Reserve Fund						
Investment Description						
Security Description	Face Value	Rate	Yield	Maturity Date	Cost	Market Value
PA INVEST	5,024,888	1.73%	1.73%	01-01-09	\$ 5,024,888	\$ 5,024,888
FHLB	15,000,000	2.88%	2.89%	01-30-09	14,998,500	15,028,200
FHLMC	15,000,000	2.81%	2.81%	01-30-09	15,000,000	15,026,700
FHLB	3,500,000	2.88%	2.88%	02-04-09	3,500,000	3,507,665
GECC	5,000,000	0.00%	3.01%	03-16-09	4,925,750	4,925,750
FHLMC	4,000,000	3.76%	3.76%	03-18-09	3,999,560	4,027,520
FNMA	5,000,000	0.00%	2.63%	03-18-09	4,935,644	4,971,919
FHLMC	5,000,000	2.50%	2.50%	04-21-09	5,000,000	5,005,250
FHLMC	8,000,000	0.00%	1.30%	05-04-09	7,954,204	7,982,791
FHLB	2,500,000	2.65%	2.65%	06-04-09	2,500,000	2,524,613
FFCB	1,000,000	3.75%	3.76%	06-10-09	999,850	1,015,000
FHLB	5,000,000	2.60%	2.55%	06-17-09	5,002,400	5,051,550
FHLB	5,000,000	2.88%	2.87%	07-01-09	5,000,000	5,061,725
UST Bills	1,500,000	0.00%	2.33%	07-02-09	1,465,723	1,489,905
UST Bills	2,000,000	0.00%	1.53%	08-27-09	1,971,524	1,992,188
FNMA	8,000,000	0.00%	0.96%	12-01-09	7,926,111	7,944,622
FHLB	5,000,000	2.38%	2.20%	04-30-10	5,012,300	5,103,125
FHLB	5,000,000	3.00%	2.95%	06-11-10	5,005,000	5,157,050
FHLB	10,000,000	2.75%	1.72%	06-18-10	10,152,000	10,273,450
FHLMC	10,000,000	2.88%	2.91%	06-28-10	9,979,150	10,264,300
FHLMC	2,500,000	3.25%	3.31%	07-16-10	2,497,150	2,832,800
FHLMC	7,500,000	3.25%	2.92%	07-16-10	7,538,978	7,500,000
FNMA	7,000,000	3.25%	3.28%	08-12-10	6,995,940	7,252,665
FHLMC	10,000,000	2.00%	2.00%	12-30-10	10,000,000	10,051,800
FNMA	2,000,000	2.35%	2.35%	12-30-10	2,000,000	2,002,190
FNMA	7,500,000	2.75%	2.83%	04-11-11	7,410,975	7,700,400
FHLB	12,500,000	3.70%	3.70%	05-13-11	12,500,000	12,634,813
Total General Reserve Fund					<u>169,295,647</u>	<u>171,352,879</u>
Total Investments					<u>\$381,793,499</u>	<u>\$385,035,644</u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

SCHEDULES OF INVESTMENTS (CONTINUED)

December 31, 2007

Construction Fund						
Security Description	Investment Description			Maturity Date	Cost	Market Value
	Face Value	Rate	Yield			
FNMA	\$ 5,000,000	0.00 %	5.11 %	01-18-08	\$ 4,816,867	\$ 4,991,500
FNMA	5,000,000	0.00 %	4.35 %	01-22-08	4,983,950	4,989,000
FHLMC	2,900,000	0.00 %	4.44 %	01-23-08	2,890,571	2,893,330
FHLB	3,000,000	5.25 %	5.24 %	02-01-08	3,000,000	3,000,930
FHLB	2,000,000	5.20 %	5.20 %	02-15-08	2,000,000	2,001,260
FHLMC	5,000,000	4.30 %	5.07 %	05-05-08	4,972,650	4,994,250
FNMA	5,000,000	0.00 %	4.18 %	07-28-08	4,851,483	4,889,500
FHLB	5,000,000	5.15 %	5.15 %	08-15-08	5,000,000	5,004,700
FHLB	5,000,000	4.50 %	4.50 %	11-05-08	5,000,000	5,006,250
FHLB	5,000,000	4.50 %	4.50 %	11-07-08	5,000,000	5,006,250
FHLB	50,000,000	0.00 %	4.29 %	01-22-08	49,841,750	49,890,000
FNMA	50,000,000	0.00 %	4.35 %	01-22-08	49,839,500	49,890,000
FHLB	50,000,000	0.00 %	4.41 %	01-23-08	49,837,250	49,885,000
FHLMC	55,000,000	0.00 %	4.41 %	01-23-08	54,821,181	54,873,500
FHLB	50,000,000	0.00 %	4.42 %	01-25-08	49,825,194	49,875,000
Total Construction Fund					<u>\$ 296,680,396</u>	<u>\$ 297,190,470</u>

Operating Fund						
Security Description	Investment Description			Maturity Date	Cost	Market Value
	Face Value	Rate	Yield			
FNMA	1,000,000	0.00 %	4.35 %	01-22-08	\$ 996,790	\$ 997,800
FHLMC	1,800,000	0.00 %	4.41 %	01-23-08	1,794,148	1,795,860
FHLB	1,000,000	4.50 %	4.50 %	11-05-08	1,000,000	1,001,250
Total Operating Fund					<u>\$ 3,790,938</u>	<u>\$ 3,794,910</u>

Reserve Maintenance Fund						
Security Description	Investment Description			Maturity Date	Cost	Market Value
	Face Value	Rate	Yield			
FNMA	1,000,000	0.00 %	4.35 %	01-22-08	\$ 996,790	\$ 997,800
FHLMC	1,000,000	0.00 %	4.41 %	01-23-08	996,749	997,700
Total Reserve Maintenance Fund					<u>\$ 1,993,539</u>	<u>\$ 1,995,500</u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

SCHEDULES OF INVESTMENTS (CONTINUED)

December 31, 2007

General Reserve Fund						
Security Description	Investment Description			Maturity Date	Cost	Market Value
	Face Value	Rate	Yield			
PA INVEST	1,173,753	4.26 %	4.26 %	01-01-08	\$ 1,173,753	\$ 1,173,753
FHLB	5,000,000	5.13 %	5.20 %	01-16-08	4,997,050	5,001,550
FNMA	5,000,000	0.00 %	5.11 %	01-18-08	4,816,867	4,991,500
FNMA	5,000,000	0.00 %	4.35 %	01-22-08	4,983,950	4,989,000
FHLB	5,000,000	0.00 %	4.41 %	01-23-08	4,983,725	4,988,500
FHLMC	5,000,000	0.00 %	4.41 %	01-23-08	4,983,744	4,988,500
FNMA	5,000,000	0.00 %	4.35 %	01-24-08	4,983,356	4,988,000
FHLB	5,000,000	0.00 %	4.42 %	01-25-08	4,982,519	4,987,500
FNMA	5,000,000	0.00 %	4.35 %	01-25-08	4,982,761	4,987,500
FHLB	8,000,000	4.10 %	4.10 %	03-14-08	8,000,000	7,992,480
FHLMC	6,000,000	0.00 %	4.39 %	05-05-08	5,872,395	5,915,400
FHLB	5,000,000	4.42 %	4.42 %	05-08-08	5,000,000	4,996,900
FHLMC	6,200,000	3.88 %	4.17 %	06-15-08	6,190,204	6,178,672
FHLB	5,000,000	5.25 %	5.25 %	08-13-08	5,000,000	5,004,700
FNMA	5,000,000	3.25 %	4.10 %	08-15-08	4,970,400	4,967,200
FHLB	6,500,000	4.50 %	4.50 %	11-06-08	6,500,000	6,508,125
FNMA	2,700,000	5.25 %	5.24 %	01-29-09	2,700,000	2,702,538
FHLB	5,000,000	5.25 %	5.24 %	02-05-09	5,000,000	5,006,250
FNMA	5,000,000	5.30 %	5.30 %	02-20-09	5,000,000	5,009,400
FHLMC	5,000,000	5.30 %	5.32 %	02-27-09	4,998,438	5,010,400
FHLMC	4,000,000	3.76 %	3.76 %	03-18-09	3,999,560	3,992,520
FNMA	2,300,000	5.25 %	5.25 %	04-15-09	2,300,000	2,305,750
FFCB	1,000,000	3.75 %	3.76 %	06-10-09	999,850	1,001,250
FHLB	5,000,000	5.30 %	5.30 %	08-14-09	5,000,000	5,007,800
FHLB	5,000,000	5.13 %	5.13 %	08-28-09	5,000,000	5,009,400
FHLB	4,000,000	4.50 %	4.50 %	11-05-09	4,000,000	4,013,760
FHLB	6,000,000	4.55 %	4.55 %	11-20-09	6,000,000	6,015,000
FHLB	3,000,000	4.25 %	4.25 %	12-03-09	3,000,000	3,008,430
FHLB	5,000,000	5.40 %	5.42 %	02-12-10	4,997,656	5,007,800
FHLB	5,000,000	4.60 %	4.60 %	05-05-10	5,000,000	5,015,650
FHLB	5,000,000	4.50 %	4.50 %	05-14-10	5,000,000	5,017,200
FHLB	2,000,000	4.48 %	4.48 %	05-14-10	2,000,000	2,006,880
FHLB	2,000,000	4.63 %	4.63 %	11-05-10	2,000,000	2,008,760
FHLB	5,000,000	4.80 %	4.80 %	11-05-10	5,000,000	5,012,500
FHLB	5,000,000	4.75 %	4.75 %	11-08-10	5,000,000	5,012,500
FHLB	3,000,000	4.80 %	4.80 %	11-16-10	3,000,000	3,008,430
Total General Reserve Fund					<u>162,416,228</u>	<u>162,831,498</u>
Total Investments					<u>\$464,881,101</u>	<u>\$465,812,378</u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION
SCHEDULE OF OPERATIONS
 Year Ended December 31, 2008
 (With Comparative Totals for the Year Ended December 31, 2007)

	Total		New Hope- Lambertville Bridge	Trenton- Morrisville Bridge	Easton- Phillipsburg Bridge	Portland- Columbia Bridge	Delaware Water Gap Bridge	Milford- Montague Bridge
	2007	2008						
Cash toll revenues	\$ 32,223,503	\$ 30,005,141		\$ 2,958,656	\$ 3,348,886	\$ 699,229	\$ 9,722,031	\$ 544,668
Cash toll (shortage) overage	(31,340)	(3,961)		(7,331)	249	(347)	178	707
E-ZPass revenues	53,980,640	56,769,876		6,279,461	6,077,688	1,045,913	15,635,291	646,740
E-ZPass discounts and allowances	(798,811)	(777,456)		(190,135)	(118,595)	(35,194)	(153,030)	(38,810)
Miscellaneous revenues	129,504	165,506		-	-	-	-	-
Total Toll Revenues	<u>85,503,496</u>	<u>86,159,106</u>		<u>9,040,651</u>	<u>9,308,228</u>	<u>1,709,601</u>	<u>25,204,470</u>	<u>1,153,305</u>
Operating and maintenance expenses								
Salaries and wages								
Toll	4,012,910	4,217,004		522,398	948,111	338,756	1,133,432	252,786
Maintenance	2,955,301	3,076,074		418,795	718,580	207,058	621,011	292,546
Clerical	913,875	961,329		128,643	176,929	90,995	231,972	55,595
Supervision	2,662,938	2,778,945		523,000	518,287	218,717	541,004	234,885
Total salaries and wages	10,545,024	11,033,352		1,592,836	2,361,907	855,526	2,527,419	835,812
Social security taxes	803,182	841,673		121,365	180,221	64,446	193,543	62,663
Pension contributions	344,446	357,973		48,228	86,428	28,551	78,796	27,452
Group insurance	2,949,091	3,203,398		495,155	317,080	215,168	800,934	212,899
Retirees' costs	1,211,966	2,971,662		462,619	294,394	210,281	686,593	210,281
Heat, light and power	546,116	671,442		148,889	110,570	60,687	75,450	55,273
Office expenses	32,380	22,985		1,495	4,882	1,684	2,985	1,557
Information technology & communications	171,112	225,858		39,215	27,468	14,123	50,254	23,384
Travel, meetings and education expense	13,594	11,284		1,317	1,650	1,425	1,528	1,431
Operating supplies and expenses	84,824	60,791		15,207	11,911	3,988	10,822	6,392
E-ZPass operating expenses	3,796,492	4,132,982		731,951	1,139,050	143,828	1,035,312	138,455
State police bridge security	2,441,126	2,215,651		481,141	559,265	79,251	538,232	81,880
Maintenance supplies and expenses								
Automotive	214,779	271,068		52,100	68,884	15,396	41,723	22,257
Buildings and grounds	450,651	466,794		97,623	73,584	27,969	52,523	44,776
Toll collection equipment and								
E-ZPass maintenance	1,207,745	1,212,386		200,959	233,603	102,351	264,692	104,698
Roadways, sidewalks and approaches	457,672	365,132		45,088	84,424	30,899	56,292	34,840
Insurance	2,045,431	2,192,562		339,723	601,172	155,068	465,367	125,102
Total Operating and Maintenance Expenses	<u>27,315,631</u>	<u>30,256,993</u>		<u>4,874,911</u>	<u>4,823,285</u>	<u>2,010,641</u>	<u>6,882,465</u>	<u>1,989,152</u>
Net revenues from toll bridges before administrative expenses and toll-supported bridge expenses	<u>58,187,865</u>	<u>55,902,113</u>		<u>\$ 4,165,740</u>	<u>\$ 4,484,943</u>	<u>\$ (301,040)</u>	<u>\$ 18,322,005</u>	<u>\$ (835,847)</u>
Administrative expenses	7,334,720	8,317,635						
Toll-supported bridge expenses	7,925,504	8,902,476						
Total administrative and toll-supported bridge expenses	<u>15,260,224</u>	<u>17,220,111</u>						
Net operating revenues	<u>\$ 42,927,641</u>	<u>\$ 38,682,002</u>						

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

SCHEDULES OF ADMINISTRATIVE EXPENSES

	<u>Year Ended December 31,</u>	
	<u>2008</u>	<u>2007</u>
Salaries and wages	\$ 3,873,862	\$ 3,456,431
Social security taxes	327,210	279,105
Pension contributions	151,039	153,558
Group insurance	929,782	809,561
Retirees' costs	881,451	336,726
Unemployment compensation	49,569	71,274
Office expenses	195,073	183,391
Commission expenses	47,391	44,148
Information technology and communications	328,732	392,471
Travel, meetings and education expense	163,970	152,973
Automotive repairs and expenses	4,500	3,793
Professional service fees	1,173,963	1,107,890
Advertising and marketing	32,529	58,550
Insurance	158,564	284,849
	<u>\$ 8,317,635</u>	<u>\$ 7,334,720</u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

SCHEDULES OF TOLL-SUPPORTED BRIDGE EXPENSES

	Year Ended December 31,			
	2008			2007
	Total	Southern Division Bridges	Northern Division Bridges	Total
Salaries and wages	\$3,464,394	\$1,900,169	\$1,564,225	\$3,319,983
Social security taxes	258,031	142,095	115,936	249,405
Pension contributions	111,354	60,775	50,579	104,215
Group insurance	1,113,087	601,767	511,320	1,014,693
Retirees' costs	1,037,562	574,606	462,956	423,381
Heat, light and power	73,688	45,101	28,587	71,976
Office expenses	273	273	-	547
Information technology and communication	14,640	6,875	7,765	25,569
Travel, meetings and education expense	20	-	20	72
Operating supplies and expenses	19,180	15,049	4,131	33,934
State police bridge security	1,540,326	1,166,231	374,095	1,603,152
Maintenance supplies and expenses				
Automotive	16,791	3,477	13,314	10,680
Buildings and grounds	52,216	43,670	8,546	39,803
Roadways, sidewalks and approaches	91,071	69,464	21,607	76,686
Insurance	1,109,843	725,805	384,038	951,408
	<u>\$8,902,476</u>	<u>\$5,355,357</u>	<u>\$3,547,119</u>	<u>\$7,925,504</u>

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION
SCHEDULE OF TOLL BRIDGE TRAFFIC AND REVENUES

Year Ended December 31, 2008
 (With Comparative Totals for the Year Ended December 31, 2007)

	Total		Trenton- Morrisville Bridge	New Hope- Lambertville Bridge	I-78 Bridge	Easton- Phillipsburg Bridge	Portland- Columbia Bridge	Delaware Water Gap Bridge	Mifflin- Montague Bridge
	2007	2008							
Number of Vehicles Crossing Toll Bridges, by Class									
Passenger Cars, Vans, and Pickups	33,030,906	32,182,282	6,107,545	1,758,949	7,559,187	5,925,210	1,274,654	8,290,964	1,265,773
Two-Axle Trucks, Buses, and Tractors	853,782	830,366	175,125	55,234	230,345	159,893	29,250	153,827	26,692
Three-Axle Trucks, Buses, Tractor and Trailer	391,516	384,083	85,360	24,051	103,599	61,563	12,887	92,137	4,486
Four-Axle Trucks, Tractor and Trailer	332,493	321,303	65,417	6,669	116,043	60,317	8,791	62,497	1,569
Five-Axle Trucks, Tractor and Trailer	3,512,936	3,375,594	171,432	25,391	1,831,467	215,992	32,306	1,090,089	8,917
Six-Axle Trucks, Tractor and Trailer	72,839	80,012	1,776	749	49,371	3,055	295	24,637	129
Vehicles Requiring Special Permits	47	58	-	1	8	-	-	49	-
Seven-or-More-Axle Trucks, Tractor and Trailer	2,449	2,562	33	23	1,379	71	12	1,036	8
Total Number of Vehicles	38,196,968	37,176,260	6,606,688	1,871,067	9,891,399	6,426,101	1,358,195	9,715,236	1,307,574
Toll Revenues of Vehicles Crossing Toll Bridges, by Class									
Passenger Cars, Vans, and Pickups	\$ 22,259,125	\$ 21,608,176	\$ 4,092,808	\$ 1,155,779	\$ 5,091,741	\$ 3,945,491	\$ 862,883	\$ 5,604,915	\$ 854,559
Two-Axle Trucks, Buses, and Tractors	4,103,175	3,980,645	837,534	265,142	1,102,612	768,210	140,839	738,225	128,083
Three-Axle Trucks, Buses, Tractor and Trailer	3,417,816	3,542,538	787,320	223,007	953,440	569,425	120,207	847,156	41,983
Four-Axle Trucks, Tractor and Trailer	3,819,669	3,907,522	800,855	81,343	1,398,665	739,050	109,119	758,876	19,614
Five-Axle Trucks, Tractor and Trailer	51,029,259	51,943,085	2,620,571	392,139	28,147,828	3,325,631	501,321	16,817,709	137,886
Six-Axle Trucks, Tractor and Trailer	1,248,698	1,453,903	32,793	13,835	891,612	56,648	5,615	450,982	2,418
Vehicles Requiring Special Permits	2,374	2,791	-	26	199	-	-	2,566	-
Seven-or-More-Axle Trucks, Tractor and Trailer	58,121	63,475	680	516	33,414	1,537	309	26,839	180
Total Toll Revenues from Vehicles	85,938,237	86,502,135	9,172,561	2,131,787	37,619,511	9,405,992	1,740,293	25,247,268	1,184,723
E-ZPass discounts and allowances and other adjustments	(434,741)	(343,029)	(131,910)	(48,671)	40,224	(97,764)	(30,692)	(42,798)	(31,418)
Total Toll Revenues	\$ 85,503,496	\$ 86,159,106	\$ 9,040,651	\$ 2,083,116	\$ 37,659,735	\$ 9,308,228	\$ 1,709,601	\$ 25,204,470	\$ 1,153,305

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INDEPENDENT AUDITORS' REPORT ON INTERNAL CONTROL OVER
FINANCIAL REPORTING AND ON COMPLIANCE AND OTHER MATTERS
BASED ON AN AUDIT OF FINANCIAL STATEMENTS PERFORMED IN
ACCORDANCE WITH *GOVERNMENT AUDITING STANDARDS*

To the Board of Commissioners of
Delaware River Joint Toll Bridge Commission

We have audited the financial statements of Delaware River Joint Toll Bridge Commission (the "Commission") as of and for the year ended December 31, 2008, and have issued our report thereon dated May 13, 2009. We conducted our audit in accordance with auditing standards generally accepted in the United States of America and the standards applicable to financial audits contained in *Government Auditing Standards*, issued by the Comptroller General of the United States.

Internal Control over Financial Reporting

In planning and performing our audit, we considered the Commission's internal control over financial reporting as a basis for designing our auditing procedures for the purpose of expressing our opinion on the financial statements but not for the purpose of expressing an opinion on the effectiveness of the Commission's internal control over financial reporting. Accordingly, we do not express an opinion on the effectiveness of the Commission's internal control over financial reporting.

A control deficiency exists when the design or operation of a control does not allow management or employees, in the normal course of performing their assigned functions, to prevent or detect misstatements on a timely basis. A significant deficiency is a control deficiency, or combination of control deficiencies, that adversely affects the entity's ability to initiate, authorize, record, process, or report financial data reliably in accordance with generally accepted accounting principles such that there is more than a remote likelihood that a misstatement of the entity's financial statements that is more than inconsequential will not be prevented or detected by the entity's internal control.

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HOLLAND, PA 18966
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- CPA USA NETWORK
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INDEPENDENT AUDITORS' REPORT ON INTERNAL CONTROL OVER FINANCIAL REPORTING AND ON COMPLIANCE AND OTHER MATTERS BASED ON AN AUDIT OF FINANCIAL STATEMENTS PERFORMED IN ACCORDANCE WITH *GOVERNMENT AUDITING STANDARDS* (CONTINUED)

Internal Control over Financial Reporting (Continued)

A material weakness is a significant deficiency, or combination of significant deficiencies, that results in more than a remote likelihood that a material misstatement of the financial statements will not be prevented or detected by the entity's internal control.


Our consideration of internal control over financial reporting was for the limited purpose described in the first paragraph of this section and would not necessarily identify all deficiencies in internal control that might be significant deficiencies or material weaknesses. We did not identify any deficiencies in internal control over financial reporting that we consider to be material weaknesses, as defined above.

Compliance and Other Matters

As part of obtaining reasonable assurance about whether Delaware River Joint Toll Bridge Commission's financial statements are free of material misstatement, we performed tests of its compliance with certain provisions of laws, regulations, contracts, bond resolutions and compact, noncompliance with which could have a direct and material effect on the determination of financial statement amounts. However, providing an opinion on compliance with those provisions was not an objective of our audit, and accordingly, we do not express such an opinion. The results of our tests disclosed no instances of noncompliance or other matters that are required to be reported under *Government Auditing Standards*.

We noted certain matters that we reported to management of the Commission in a separate letter dated May 13, 2009.

This report is intended solely for the information and use of the audit committee, the Board of Commissioners, management, and others within the Commission and is not intended to be and should not be used by anyone other than these specified parties.


Mercader, P.C.
Certified Public Accountants
May 13, 2009



**Delaware River
Joint Toll Bridge
Commission**

Preserving Our Past, Enhancing Our Future

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2008 TOLL-SUPPORTED BRIDGE INSPECTION REPORT

FEBRUARY 2009



TOLL BRIDGES

Trenton-Morrisville	Portland-Columbia
New Hope-Lambertville	Delaware Water Gap
Interstate 78	Milford-Montague
Easton-Phillipsburg	

TOLL-SUPPORTED BRIDGES

Lower Trenton	Uhlerstown-Frenchtown
Calhoun Street	Upper Black Eddy-Milford
Scudder Falls	Riegelsville
Washington Crossing	Northampton Street
New Hope-Lambertville	Riverton-Belvidere
Centre Bridge-Stockton	Portland-Columbia
Lumberville-Raven Rock	

Prepared by



TranSystems | Lichtenstein

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February 10, 2009

Mr. Frank G. McCartney
Executive Director
Delaware River Joint Toll Bridge Commission
110 Wood Street
Morrisville, PA 19067

**RE: DRJTBC Contract No. C-07-02B; Capital Project 0710A
General Engineering Consultant – 2008 Annual Inspections
2008 Toll-Supported Bridge Inspections – Annual Inspection Report
Our Project Number 708080010**

Dear Mr. McCartney:

It is with great pleasure that we are submitting the Consulting Engineer's Seventy-First Annual Inspection Report (2008 Toll Supported Bridge Inspection Bridge Inspections) for the Commission's following facilities:

- A. The Thirteen (13) Toll-Supported (Non-Toll) Bridges
- B. The Seven (7) Toll Bridges
- C. The Thirty-Six (36) approach bridges and roadways serving the above bridges
- D. The Commission's buildings and grounds
- E. The Commission's vehicles and equipment

This Annual Inspection Report summarizes our findings and recommendations based upon the 2008 inspection of the Toll-Supported Facilities. An update of the 2007 inspections of the Toll Facilities was completed to indicate any material changes in the conclusion and recommendation report sections. All facilities are in operating condition.

The Twelfth Annual Maintenance Report which defines activities to be undertaken by the Commission's maintenance staff is published separately.

The report identifies certain ongoing capital projects and their estimated costs for 2009 and 2010. The estimated expenditure for capital projects in 2009 is \$141,190,000. In addition, an estimated expenditure of \$1,402,000 is recommended for new vehicle and equipment purchases in 2009. Therefore, the total amount of ongoing capital projects and vehicle and equipment expenditures in 2009 is estimated to be \$142,592,000. The estimated expenditure for ongoing capital projects and vehicle and equipment expenditures for 2010 is \$165,431,000.

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I, William Clark, P.E., do hereby certify to the best of my knowledge, information, and belief that the information contained in the accompanying inspection report has been prepared in accordance with accepted engineering practice. This inspection and report conform to applicable requirements, criteria, guidelines, and standards as stated in the "Bridge Inspectors Reference Manual", FHWA NHI 03-001 – 2002, "Inspection of Fracture Critical Bridge Members" – 1986, as published by FHWA and the AASHTO "Manual for Condition Evaluation of Bridges" – 1994, including all Interims and is true and correct at the time of the inspection. This report has been reviewed using appropriate Quality Assurance guidelines in accordance with generally accepted engineering practice.

It has been a pleasure to serve the Commission. Please contact us if you require any additional information.

Very truly yours,

TranSystems|Lichtenstein

A handwritten signature in black ink, appearing to read "William Clark".

William Clark, P.E.
Project Manager/Associate

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DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

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Chairman

HONORABLE DONALD HART

HONORABLE HARRY ZIKAS, JR.

HONORABLE WILLIAM J. HODAS

VACANT

PENNSYLVANIA

HONORABLE GAETAN J. ALFANO, ESQ.
Vice Chairman

HONORABLE MELISSA HELLER

HONORABLE JAMES L. BROUGHAL, ESQ.

HONORABLE BERNARD A. GRIGGS, JR.

HONORABLE JOHN PREVOZNIK, ESQ.
Secretary - Treasurer

DELAWARE RIVER JOINT TOLL BRIDGE COMMISSION

PROFESSIONAL ASSOCIATES

CONSULTING ENGINEERS

TRANSYSTEMS|LICHTENSTEIN
Paramus, New Jersey

LEGAL COUNSEL

FOX ROTHSCHILD, LLP
Philadelphia, Pennsylvania

FLORIO, PERRUCCI, STEINHARDT & FADER
Phillipsburg, New Jersey

LABOR COUNSEL

STRADLEY, RONON, STEVENS & YOUNG
Philadelphia, Pennsylvania

WOLFF & SAMSON
West Orange, New Jersey

AUDITORS

MERCADIEN
Princeton, New Jersey

FINANCIAL ADVISOR

VACANT

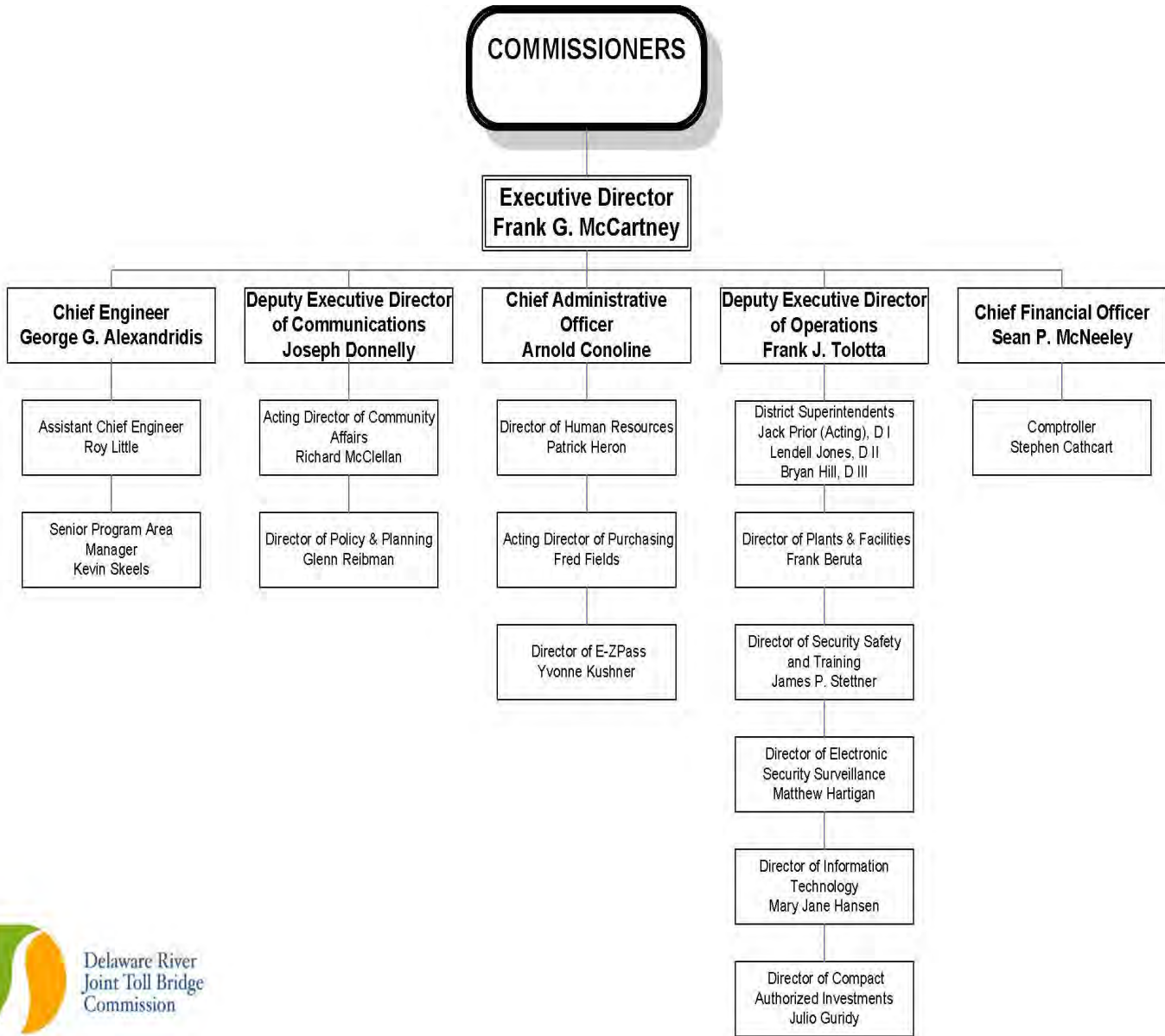
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BELLEVUE COMMUNICATIONS
Philadelphia, Pennsylvania

INVESTMENT MANAGEMENT

COMMERCE CAPITAL MARKETS
Philadelphia, Pennsylvania

COMMISSION STAFF



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INTRODUCTION

In accordance with Federal Highway Administration (FHWA) regulations, all bridges must be inspected at least once every two (2) years, more often if warranted, due to condition. Under the Commission's 2003 Bond Resolution, all bridges and toll facilities are to be inspected once every two (2) years. The Commission will inspect its Toll-Supported Bridges in even years (2008, 2010, etc.) and the Toll Bridges in odd years (2009, 2011, etc.). The associated facilities and grounds will be inspected in the year the bridge is inspected.

This Seventy-first Annual Inspection Report of bridges and facilities owned and operated by the Delaware River Joint Toll Bridge Commission contains the findings of the 2008 inspections of the Toll-Supported Bridges. This year's inspections consisted of thirteen (13) Toll-Supported Bridges and any accompanying facilities and approach structures. The conclusions and recommendations concerning the Toll Bridges are based on the 2007 inspections. Any changes to the 2007 conclusions or recommendations for the Toll Bridges are indicated by text that is ***bold and italicized***. The inspection findings shown for the Toll Bridges is for informational purposes.

Commission District foremen and maintenance personnel provided our inspection crew with support services and access equipment necessary for performing the inspections. Several maintenance personnel also assisted in providing a valuable 'walk through' of the bridges, prior to beginning the inspections, highlighting the major areas of concern and any previous work done on the structure.

The equipment used to access the majority of the bridges (underdeck) consisted of various length ladders, Commission owned lift trucks, an under-bridge unit called The Bridgemaster and rigging.

The following report highlights the significant findings observed during the inspections, including recommended measures of repairing or improving noted deficiencies, either by Commission maintenance forces or by a future contract. This report, however, does not discuss routine preventative maintenance items regularly performed by Maintenance forces. Any maintenance force level deficiencies which have been identified during the annual inspection can be found in the *Twelfth Annual Maintenance Report*, published under a separate cover, which has been prepared to expedite communication of repair work to the maintenance staff. In general these maintenance tasks include, but are not limited to, the following:

- Removal of accumulated debris from the deck, deck joints, inlets, catch basins, and drainage pipes
- Annual cleaning of structures (bridge flushing)
- Monitoring and repair of lighting and electrical work
- Removal of vegetation from substructures
- Removal of graffiti from bridges and retaining walls
- Patching concrete spalls and asphalt potholes
- Sealing roadway and bridge deck cracks
- Localized cleaning and painting of rusted steel/bearings
- Deck joint rehabilitation
- Guide rail repairs
- Miscellaneous steel repairs

A consistent numbering system was used to identify the bridge spans. Span numbering generally begins at the westernmost location of the bridge and increases to the east. However, a specific numbering system was not utilized for the individual structural members. The locations for individual members (stringers, floorbeams, etc.) are referenced by their relationship to known fixed points, such as bridge fascias and piers.

Several capital improvement projects were completed beyond the past two years. Among these projects are the following:

Projects Completed 2001 - 2008 (> \$25,000)	Program Cost
E-ZPass Implementation	\$ 18,023,146
Centre Bridge - Stockton TSB Rehabilitation	\$ 9,730,805
New Hope - Lambertville TB Plaza & Bridge Rehab	\$ 9,671,373
Riverton - Belvidere TSB Rehabilitation	\$ 9,288,815
New Hope - Lambertville TSB Rehabilitation	\$ 7,700,991
Northampton Street Bridge Rehabilitation	\$ 7,364,066
New Hope - Lambertville TB Administration Building Additions & Renovations	\$ 5,950,610
Uhlerstown-Frenchtown Rehabilitation	\$ 5,779,187
Power Upgrades - All facilities+Struct Wiring+Telephone	\$ 4,760,754
Cleaning & Painting of the LT TSB & Sign Replacement	\$ 4,567,205
Easton - Phillipsburg TB Sign Struct Replacements, Repair & Signage Upgrades	\$ 2,778,496
Easton - Phillipsburg TB Sidewalk Replacement	\$ 1,705,247
Scudder Falls TSB Deck Joint Replacement	\$ 1,446,418
High Priority Structural Steel Repairs at the SFTSB	\$ 968,625
I-78 Expansion Dam Replacement	\$ 867,788
District 3 Roof Replacements	\$ 781,634
Milford - Montague TB Water Supply Upgrade	\$ 754,195
Emergency and Priority Repair Contract (all Bridges) -T/TS 389	\$ 749,233
New Hope - Lambertville TB Terne Roof Replacement	\$ 685,101
Northerly Corridor Congestion Mitigation Study	\$ 647,376
Easton - Phillipsburg TB Replace Roof System on Admin Bldg and Garage	\$ 599,782
I-80 NJ Repaving (NJDOT)	\$ 581,442
RGL End Floorbeam Bearings	\$ 565,563
Southerly Crossing Corridor Study	\$ 544,643
Easton - Phillipsburg Pavement of Bridge Approaches (PennDOT)	\$ 517,090
I-78 Salt Storage Bldg	\$ 485,681
Substructure & Scour Remediation	\$ 482,299
Calhoun Street TSB Interim Repair Contract (Structural Steel Repairs)	\$ 445,913
Washington Crossing TSB Deck Joint Replacement @ Pier 1,2,4 & 5	\$ 407,885
Emergency and Priority Repair Contract (all Bridges) -I-80/NHTSB	\$ 367,116
Portland - Columbia TSB Handicap Accessible Ramp	\$ 305,656
Portland - Columbia TSB Deck Repairs and Drainage Modifications	\$ 290,998
I-80 NJ Service Road Repair & Repaving	\$ 239,885

Replace Overhead Sign (by NJDOT)	\$	230,309
Northampton Street TSB Inspection/Access Cable/Lifeline	\$	222,044
Alternative Analysis Study - Additional Capacity at Calhoun Street	\$	200,343
Furnishings and Equipment for Addition and Renovation	\$	200,000
Wide Area Network (WAN)	\$	192,957
I-78 Roadway Restriping	\$	184,898
Emergency Management Studies (Phase 1 & 2)	\$	184,000
Riegelsville TSB Pier Apron Repair	\$	166,755
NHLTSB Emergency Sidewalk Repair	\$	156,083
I-95/Scudder Falls TSB Bridge Lighting Upgrade	\$	126,131
Elevator Upgrade	\$	106,455
I-95/Scudder Falls TSB Guiderail Replacement (By NJDOT)	\$	103,000
DWG Impact Attenuators Design (see 438, Constr. Cost included in 440)	\$	66,788
Trenton - Morrisville Admin Building Space Plan	\$	56,544
Milford - Montague TB Impact Attenuators Design, see 438 (Constr. cost incl. in 430)	\$	33,394
Portland Columbia TB Impact Attenuators Design, see 438 (Constr. cost incl. in 441)	\$	28,257
Total =	\$	102,312,976

The capital improvement projects shown below are underway and are either being developed, studied, designed, or constructed:

Projects Underway (> \$25,000)	Program Cost
I-95/Scudder Falls Improvement Project (Design, CM/CI, Construction)	\$ 309,061,539
Trenton - Morrisville TB Rehab + One Aux. NB Lane	\$ 102,383,648
I-78 Roadway Rehabilitation	\$ 57,623,806
I-80/Delaware Water Gap Toll Bridge Open Road Tolling	\$ 39,175,455
Electronic Surveillance Detection System	\$ 22,697,087
Milford - Montague TB Rehabilitation	\$ 19,078,000
I-78 Open Road Tolling (ORT) Lanes	\$ 15,927,690
Calhoun Street TSB Rehabilitation	\$ 13,216,900
Upper Black Eddy - Milford TSB Rehabilitation	\$ 12,570,288
In-Lane System Integrator	\$ 8,041,296
Riegelsville TSB Rehabilitation	\$ 6,954,151
District 1, 2 & 3 Substructure & Scour Remediation	\$ 6,818,997
I-80/Delaware Water Gap Toll Bridge Bearing Remediation and Deck Study	\$ 12,522,440
New Hope - Lambertville TB - Floorbeam Bracket Improvements	\$ 5,671,000
Phase 1 Rehabilitation & Concept Study for the Washington Crossing TSB	\$ 3,429,152
Financial Management System	\$ 2,521,919
Riverton - Belvidere TSB Water Street Repaving & Improvements	\$ 1,302,261
Portland - Columbia TB Locust Street Improvements	\$ 1,098,695
Customer Service Center / Violations Processing Center	\$ 996,725

Trenton - Morrisville TB Buildings Roof Replacement	\$	730,620
Phase 1 DWG Toll Bridge ORT Study	\$	500,000
Trenton - Morrisville TB Elevator Modernization	\$	456,440
NJDEP & PADEP Municipal Stormwater Regulation Compliance at Toll Facilities	\$	286,000
2008 Long Term Traffic Projections	\$	249,999
System Wide IT and Telephone Upgrade	\$	242,000
Portland - Columbia Pedestrian Bridge, PA Approach Vehicle Access	\$	94,154
	Total =	\$ 643,650,262

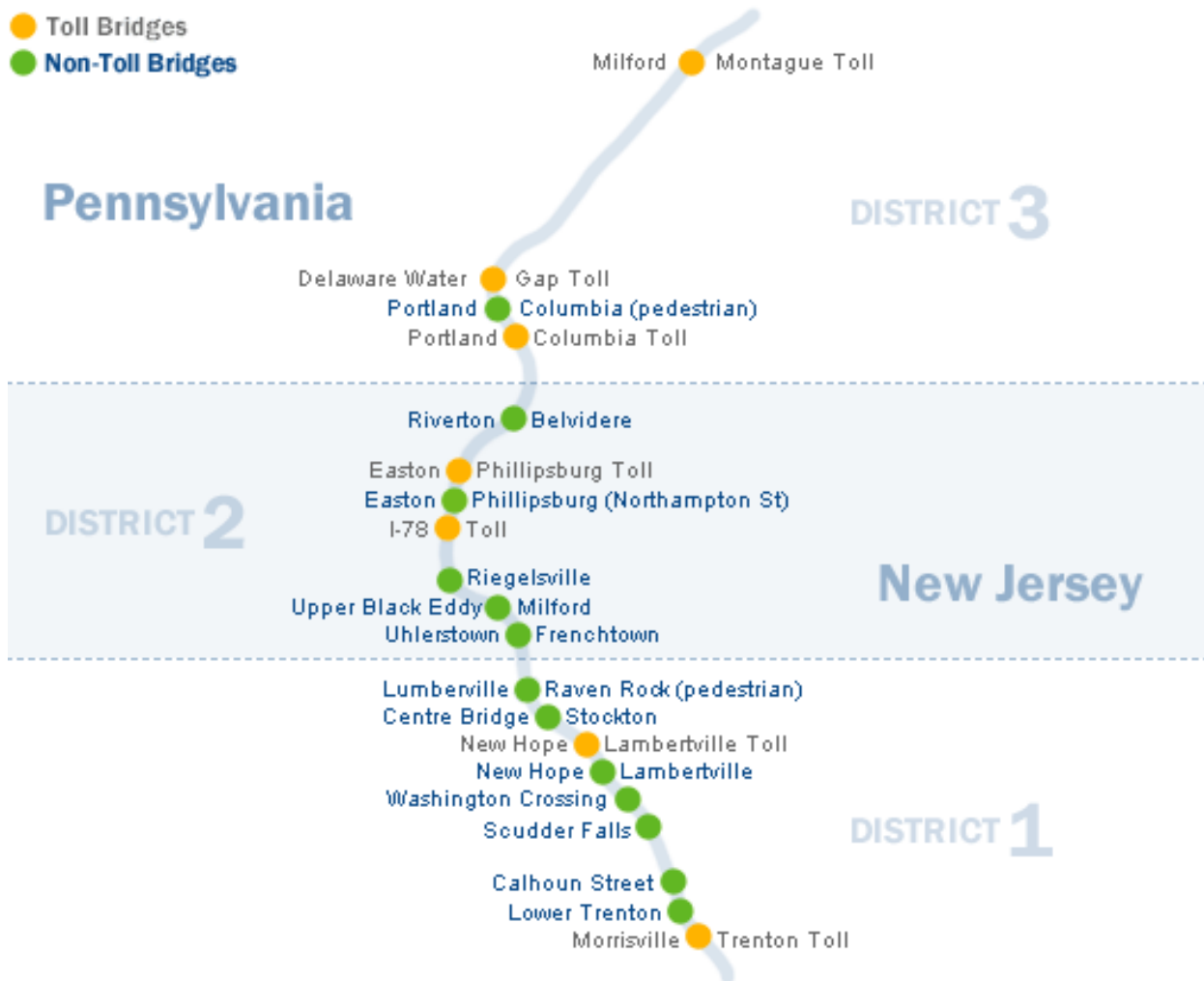
In 2000 the Commission adopted a “fix it right” philosophy for its Capital Program as compared to the previous “fix what’s broken” approach. The “fix it right” approach is based on the premise that whenever a project requires a bridge closure for implementation, that project must be designed so that no additional repair projects requiring a closure will be necessary for a subsequent period of at least 15 years. The estimated costs of the recommended improvements included in this report account for all costs of design, construction, construction management and inspection, and contract administration, are consistent with the Commission’s “fix it right” approach. It is also noted that the general findings and estimated repair costs developed from the 2006 Underwater Inspection Report, have been included in this report.

The format of the cost sheets for the Seventy-first Annual Inspection Report reflects the estimated cost of recommended improvements funded by the General Reserve in 2009 and 2010. Cost sheets for the Toll Bridges have been updated to reflect anticipated costs in 2009 and 2010. In addition the cost sheets provide the total program cost of the projects (Design, CMCI and Construction). The total in each section does not include the cost of completed projects.

The following report will summarize significant findings, recommendations, and associated estimated costs at the end of each section for each structure. Following the main reports are the recommendations for equipment and vehicle inspections and their associated repair/replacement costs. Finally, the Schedule of Insurance is provided towards the end of this report.

KEY SHEET

- Toll Bridges
- Non-Toll Bridges



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***COMMISSION INITIATIVES
AND SYSTEM-WIDE PROJECTS***

Commission Initiatives and System-Wide Projects

In addition to addressing the findings of the 2008 annual inspection, the Commission has instituted in its Capital Program a number of "Commission Initiatives and System-Wide Projects". These initiatives increase the safety and security of patrons, increase the Commission's responsiveness to emergencies, identify needed future capacity improvements, and allow for increased control of projects and equipment.

The following is a partial listing of Commission Initiatives and System-Wide Projects that have begun or will begin in the near future:

Project Description	* Program Cost	General Reserve Fund	
		2009	2010
<u>Compact Authorized Investments</u>	\$40,000,000	\$14,160,000	\$2,069,000
<u>Compact Authorized Investment Consultants</u>	\$2,000,000	\$400,000	\$253,000
<p>In order to maintain and enhance the bridge infrastructure the Commission has programmed projects to include Compact Authorized Expenditures for host community transportation infrastructure improvements. These expenditures will be geared toward improving throughout at the Commission's facilities. This initiative is currently ongoing.</p>			
<u>Capitalized Engineering Department Labor</u>	\$10,188,000	\$720,000	\$745,000
<p>This Commission initiative will track the in-house engineering department's efforts on all capital projects. The total programmed amount is shown as well as the expected expenditures in 2009 and 2010.</p>			
<u>Capitalized Capital Prgm Mgmt Consultant Expenditures</u>	\$22,758,000	\$1,500,000	\$1,551,000
<p>The Capital Program Management Consultant has enabled the Commission to continue to move the Capital Program forward by managing design, construction and construction management contracts associated with the capital program. Additional project managers have been provided under this contract and this cost is being tracked as a capital expense.</p>			

* Note: The Program Cost includes the costs from 2001 to 2018

Commission Initiatives and System-Wide Projects

Project Description	* Program Cost	General Reserve Fund	
		2009	2010
<p><u>Electronic Surveillance Detection System</u></p> <p>This project involves the planning, design, installation and maintenance of an electronic surveillance and detection system to provide for the security of the Commission's bridges, roadways, toll plazas, and support facilities. The program will also include upgrades to the Commission's existing radio communications system. A consulting firm will be responsible for program management including the administration, planning, development, and coordination of the implementation of an electronic system designed to deter and detect impacts of threats to Commission assets. The project is currently underway.</p>	\$22,698,000	\$7,747,000	\$0
<p><u>System Wide IT and Telephone Upgrade</u></p> <p>This project involves the installation and maintenance of improvements, including switches and routers, to the Commission's Information Technology (IT) and Telephone systems in order to enhance the quality, security and reliability of the facility and inter-facility communications.</p>	\$320,000	\$70,000	\$0
<p><u>Stormwater Compliance @ Toll Facilities</u></p> <p>On April 1, 2004 the Commission was issued the New Jersey Pollutant Discharge Elimination System (NJPDES) Permit Number NJG0153052 Authorization to Discharge (Authorization) as a R12 – Highway Agency Storm water General Permit. This initiative will continue to provide for the compliance program bringing the Commission into conformance with the New Jersey permit. Although the Commonwealth of Pennsylvania has not yet adopted a formal permit process, the Commission will address and comply with the policies set forth by the Commonwealth of Pennsylvania Department of Environmental Protection's (PADEP) Phase II Storm water Program. The project is ongoing.</p>	\$286,000	\$138,000	\$0

* Note: The Program Cost includes the costs from 2001 to 2018

Commission Initiatives and System-Wide Projects

Project Description	* Program Cost	General Reserve Fund	
		2009	2010
<u>In-Lane System Integration DBM (CAPITAL COSTS ONLY)</u>			
	\$8,042,000	\$5,052,000	\$1,624,000
<p>The existing toll lanes are comprised of automatic lanes, manual/attended lanes and dedicated Electronic Toll Collection (ETC) lanes. The current toll collection system has no Violation Enforcement System (VES) and all enforcement is performed via manual means, toll gates.</p> <p>As part of the toll collection system expansion, the Commission will implement a three (3) lane Open Road Tolling (ORT) system at the I-78 Toll Bridge and also equip the existing conventional lanes with VES. In addition to the installation of the ORT and VES at the I-78 Toll Bridge, the Commission intends to install VES at the remaining six (6) toll bridges. Included in this project is the design, build and maintenance of the ORT, VES and the maintenance of the existing ETC system.</p>			
<u>Customer Service Center / Violation Processing Center (CSC/VPC) DBOM (CAPITAL COSTS ONLY)</u>			
	\$997,000	\$893,000	\$0
<p>As part of the Commission's toll collection system expansion, the Commission plans to implement an ORT system and to equip numerous conventional lanes with VES. This project includes the CSC/VPC design, development, installation, integration and testing. This project also includes the replacement of the existing CSC with a new CSC that also provides violation processing capability. The CSC/VPC System shall interface with the existing ETC system, the ORT system and the VES system to obtain transaction data and violation images to post transactions and pursue toll evaders.</p>			
<u>Financial Management System</u>			
	\$2,522,000	\$1,750,000	\$672,000
<p>The Commission proposes to address the increasing scale of expenditures and complexity of the Capital Improvement Program and improve enterprise resource management by upgrading from the existing accounting system and implementing a comprehensive financial management system. The Commission will assess needs and implement a solution that addresses some or all of the following areas: accounting, general ledger, accounts payable, project accounting, capital program tracking and analysis, budgeting, cash management, and purchasing.</p>			

* Note: The Program Cost includes the costs from 2001 to 2018

Commission Initiatives and System-Wide Projects

Project Description	* Program Cost	General Reserve Fund	
		2009	2010
<p><u>District 1, 2 & 3 Substructure & Scour Remediation</u></p> <p>Professional engineering services are required to perform the Substructure & Scour Remediation Repairs for the Commission’s bridges. The Consultant will be responsible for preparing a Concept Study, providing preliminary, final and post design services and compiling construction documents. The need for the proposed scour remediation and substructure repair work stems from the findings of the 2005 Underwater Inspection, and the more recent assessment of substructure damage as a result of the flood experienced in 2006.</p>	\$6,819,000	\$1,246,000	\$4,923,000
<p><u>ITS Improvement @ (DWG, E-P, I-78, T-M, S-F) - ROM</u></p> <p>The Commission proposes to implement Intelligent Transportation System (ITS) improvements to monitor real-time traffic conditions and disseminate traveler information at the Trenton-Morrisville, Easton-Phillipsburg, I-78 and Delaware Water Gap Toll Bridge Facility and the I-95 Scudder Falls Toll supported Bridge. Dissemination of information could improve travel time and safety during recurring and non-recurring congestion. ITS efforts could include deploying incident detection/management devices using roadway sensors for vehicle and incident detection. Incident verification/management using CCTV can be accomplished by deploying cameras at each facility. Dissemination of real-time traveler information can be accomplished through kiosks at major traffic generators / rest stops / visitor centers, as well as DMS/HAR installed along the roadway prior to major decision points that will allow motorists to use alternative routes.</p>	\$4,260,000	\$0	\$447,000
<p><u>Fire Protection Systems All Communications / IT Rooms</u></p> <p>The Commission has planned the design and installation of fire protection/suppression systems in the communication equipment rooms at all of the Commission’s Administration Buildings.</p>	\$457,000	\$255,000	\$202,000

* Note: The Program Cost includes the costs from 2001 to 2018

Commission Initiatives and System-Wide Projects

Project Description	* Program Cost	General Reserve Fund	
		2009	2010
<u>Asset Management System (Incl Maint Mgmt Track, BMS)</u>			
	\$611,000	\$300,000	\$311,000
<p>The Commission will develop a system to track the Commission's assets and provide the ability to show how, when, and why resources were committed by the Commission. The purpose of a Bridge Management System (BMS) is to provide a centralized location for pertinent information related to each bridge including providing a link between inspection, maintenance, design and construction data. A BMS should satisfy the FHWA requirements for the proper safety inspection and evaluation of highway bridges. Critical components of a BMS include monitoring the existing condition of the Commissions Bridges; maintain current records of structural capacity, anticipated fatigue life, seismic vulnerability, scour vulnerability and the functional assessment of each bridge. A BMS will help to better manage the Capital Program and plan for maintenance and rehabilitation costs each year.</p>			
<u>Update General Information Documents</u>			
	\$50,000	\$50,000	\$0
<p>The Commission will update its current General Information Documents. These two (2) documents entitled "General Information on Toll Bridges" and "General Information on Non-Toll Bridges" were last revised in June of 1996 and March of 1995, respectively. The goals of this Commission Initiative are to update the content of documents with current information, update the presentation of documents with graphics and color and produce electronic versions of the documents.</p>			
<u>2008 Long Term Traffic Projections</u>			
	\$250,000	\$50,000	\$0
<p>A 10 year investment grade traffic and revenue study, including the effects of deploying ORT at the I-78 and I-80/DWG Toll Bridges.</p>			

* Note: The Program Cost includes the costs from 2001 to 2018

Commission Initiatives and System-Wide Projects

Project Description	* Program Cost	General Reserve Fund	
		2009	2010
<p><u>Portland - Columbia TB Sewer Force Main Conn. & Pk Lot Paving (inc. in 441A)</u></p> <p>This project has been incorporated into Contract 441 Locust Street Improvements.</p>	\$0	\$0	\$0
<p><u>District 1 Bridge Repairs</u></p> <p>Various improvements to be down throughout District 1 that may not be large enough to warrant their own contract or that have been removed from current projects. The Commission should be prepared to package miscellaneous bridge and facility repair items for one (1) district into one (1) construction contract. This will allow the Commission to receive a competitive price completing various minor miscellaneous items. It is envisioned that one (1) contract will be completed each year and each district should be placed on a three (3) year cycle. Expenditures are expected to occur from 2010 to 2017.</p>	\$5,613,000	\$300,000	\$2,275,000
<p><u>District 2 Bridge Repairs</u></p> <p>Various improvements to be down throughout District 2 that may not be large enough to warrant their own contract or that have been removed from current projects. The Commission should be prepared to package miscellaneous bridge and facility repair items for one (1) district into one (1) construction contract. This will allow the Commission to receive a competitive price completing various minor miscellaneous items. It is envisioned that one (1) contract will be completed each year and each district should be placed on a three (3) year cycle. Expenditures are expected to occur from 2010 to 2017.</p>	\$5,802,000	\$0	\$311,000

* Note: The Program Cost includes the costs from 2001 to 2018

Commission Initiatives and System-Wide Projects

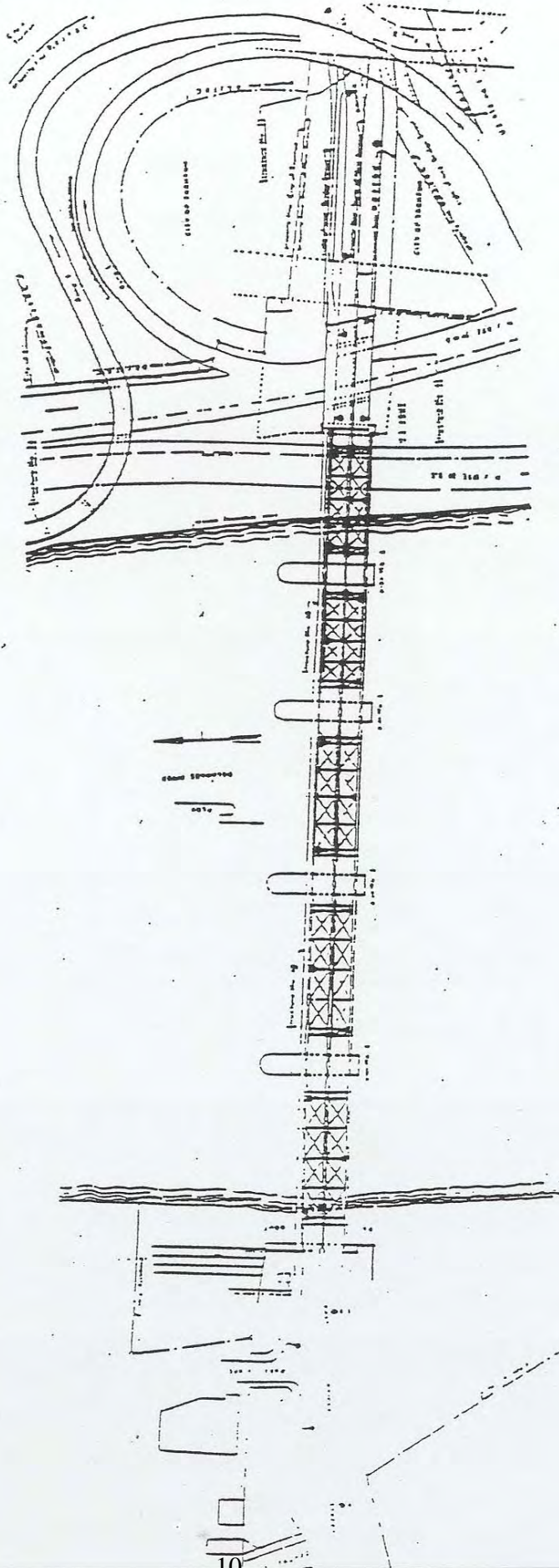
Project Description	<i>* Program Cost</i>	General Reserve Fund	
		2009	2010
<p><u>District 3 Bridge Repairs</u></p> <p>Various improvements to be down throughout District 3 that may not be large enough to warrant their own contract or that have been removed from current projects. The Commission should be prepared to package miscellaneous bridge and facility repair items for one (1) district into one (1) construction contract. This will allow the Commission to receive a competitive price completing various minor miscellaneous items. It is envisioned that one (1) contract will be completed each year and each district should be placed on a three (3) year cycle. Expenditures are expected to occur from 2010 to 2017.</p>	\$5,905,000	\$0	\$0
<p><u>District 3 Maintenance Deicing Study and Implementation</u></p> <p>The 69th Annual Inspection Report prepared by Schoor DePalma Associates, recommended a study to be performed to determine the District's deicing requirements. The study will include determining salt storage capacity, location, alternatives for deicing materials and additional deicing needs. It is anticipated that this study will be accomplished through a Task Order Assignment.</p>	\$1,189,000	\$75,000	\$1,114,000
<p>Total for all of the above Commission Initiatives and System-wide Projects:</p>	\$140,767,000	\$34,706,000	\$16,497,000

* Note: The Program Cost includes the costs from 2001 to 2018

LOWER TRENTON
TOLL-SUPPORTED BRIDGE
(Structure No. 40)

STATE OF NEW JERSEY
COUNTY OF MERCER
CITY OF TRENTON

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF BUCKS
BOROUGH OF MORRISVILLE



LOWER TRENTON TOLL SUPPORTED BRIDGE

GENERAL

LOWER TRENTON TOLL-SUPPORTED BRIDGE

(5 span, subdivided warren truss)

The Lower Trenton Toll-Supported Bridge (Structure No. 40), also known as the “Trenton Makes” Bridge, carries Bridge Street traffic from Trenton, New Jersey to Morrisville, Pennsylvania; one of three bridges connecting these two towns.

The structure is a five span subdivided Warren Truss built in 1928, with a total length of approximately 1,022 feet. The roadway consists of two lanes, one lane in each direction separated by a center truss. The curb to curb width of each lane is 19 feet, 4 ½ inches. The substructure, originally built in 1804, widened and raised in 1874, consists of stone masonry.

The structure is currently posted for a 5 ton weight limit restriction and a 25 mph speed limit. The structure is also posted for a 10 foot vertical clearance.

The downriver truss displays the “Trenton Makes the World Takes” sign which is mounted to the truss members; hence, the nickname “The Trenton Makes Bridge”. The original sign was erected in 1935 and replaced in 1981. A new sign was installed in 2005.

The structure was cleaned and painted under Contract No. TS-398A in 2005. The officer's shelter located on the Pennsylvania side of the bridge was replaced in 2006.

The east approach bridge is NJDOT owned and was not part of the inspection.

LOWER TRENTON TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

At the northwest approach corner of the Lower Trenton Toll-Supported Bridge is a Commission owned Pennsylvania officer shelter.

SIGNIFICANT FINDINGS

LOWER TRENTON TOLL-SUPPORTED BRIDGE

(5 span, subdivided warren truss)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall satisfactory condition due to minor deterioration of structural elements.

The deck, approach roadway (Pennsylvania) and superstructure are in good condition.

The substructure is in satisfactory condition. The abutments and piers exhibit numerous areas of cracked and missing mortar. A few piers also exhibit loose and missing stones in isolated areas. Heavy scaling with exposed rebar is present at the Pier 4 concrete apron.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in fair condition due to undermining of the aprons at Piers 2 and 4.

LOWER TRENTON TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania officer shelter is in overall good condition. There are two small holes (1" diameter each) in the back siding. The concrete foundation exhibits minor spalls. The light standard adjacent to the shelter was reported to be not functioning. There are minor areas of loose and missing mortar at the retaining wall adjacent to the shelter. The shelter and bridge lighting electrical cabinet door does not properly close.

CONCLUSIONS

LOWER TRENTON TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall satisfactory condition due to minor deterioration of structural elements. The areas of missing and split stone masonry, hollow concrete and spalls throughout the substructure should be repaired with concrete. Riprap should be installed around Piers 1, 3, 4 and the northwest retaining wall. Areas of missing and deteriorated mortar in the stone masonry joints throughout the substructure should be repointed. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

LOWER TRENTON TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania officer shelter is in overall good condition. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

Lower Trenton Toll-Supported Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

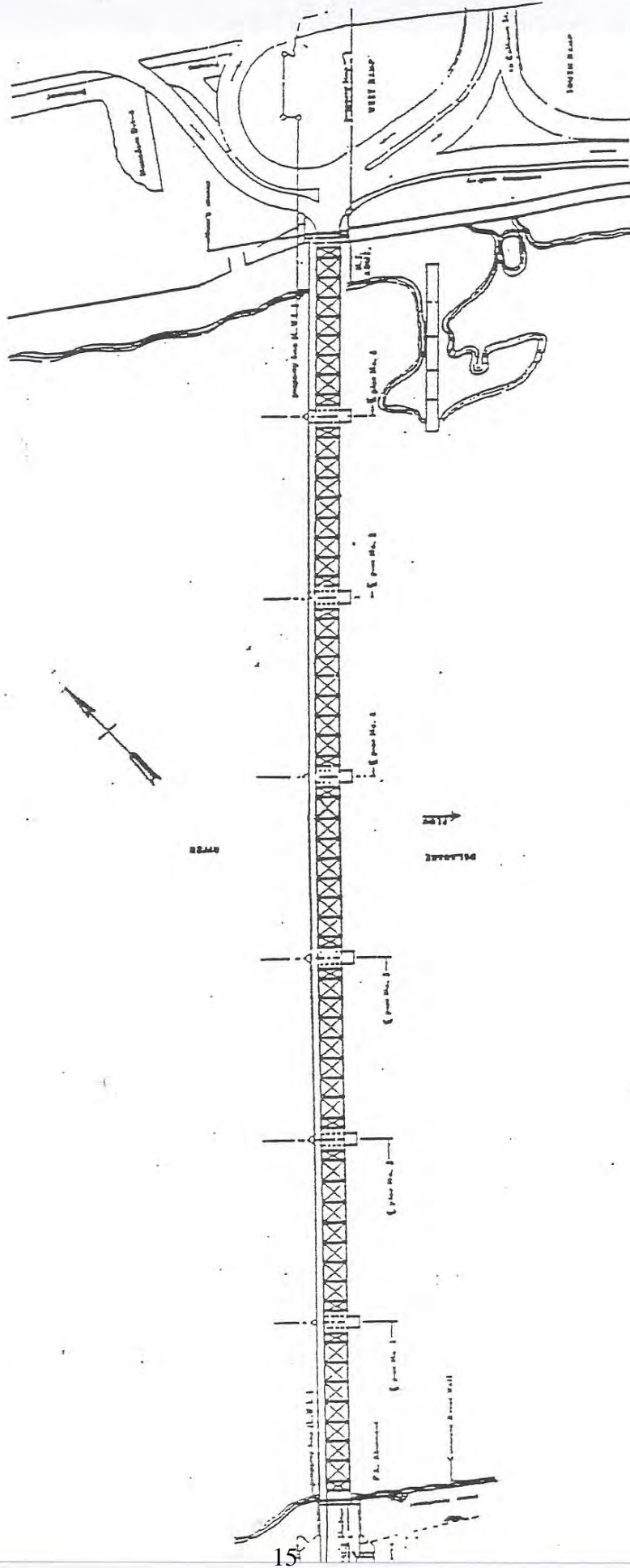
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
In 1997 this bridge was rehabilitated. In 2005, cleaning and painting were performed and the "TRENTON MAKES" sign was replaced.				
BRIDGES SUB TOTAL		\$0	\$0	\$0
<u>Facilities and Grounds</u>				
LTTSB	Miscellaneous Projects (less than \$100k each)	\$176,000	\$10,000	\$11,000
FACILITIES AND GROUNDS SUB TOTAL		\$176,000	\$10,000	\$11,000
TOTAL COST		\$176,000	\$10,000	\$11,000

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CALHOUN STREET
TOLL-SUPPORTED BRIDGE
(Structure No. 60)

STATE OF NEW JERSEY
COUNTY OF MERCER
CITY OF TRENTON

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF BUCKS
BOROUGH OF MORRISVILLE



CALHOUN STREET TOLL SUPPORTED BRIDGE

GENERAL

CALHOUN STREET TOLL-SUPPORTED BRIDGE

(7 span, wrought iron phoenix truss)

The Calhoun Street Toll-Supported Bridge (Structure No. 60) is one of three bridges constructed to connect Trenton, New Jersey and Morrisville, Pennsylvania. The truss was built in 1884 and the stone masonry substructure was built in 1859.

The structure is a seven span, wrought iron, pin connected Phoenix Pratt Truss with a total length of approximately 1,274 feet. The open steel grid deck provides a curb to curb width of 18 feet, 4 inches. A timber plank sidewalk is supported by the upriver truss on steel cantilever brackets.

The structure is currently posted for a 3 ton weight limit restriction and a 15 mph speed limit. The structure is also posted for an 8 foot vertical clearance.

A structural analysis of the structure was performed under Contract No. C-447A. The primary objective of this study was to understand the structural integrity of the bridge and determine the remaining useful life of the structure and the most economical and constructible structural remediation strategies. Findings are detailed in the "Concept Study Report" dated August 2008. The major work items recommended include; the complete replacement of the bridge roadway floor system and sidewalk, repair of truss members including heat straightening of damaged truss members. Also recommended is the replacement of the truss bearings, cleaning and painting, replacement of the bridge lighting systems and improvements to the approach roadways and sidewalks.

CALHOUN STREET TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

At the southwest and southeast approach corners of the Calhoun Street Toll-Supported Bridge are Commission owned Pennsylvania and New Jersey officer shelters.

SIGNIFICANT FINDINGS

CALHOUN STREET TOLL-SUPPORTED BRIDGE

(7 span, wrought iron phoenix truss)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall poor condition due to the superstructure and substructure.

The deck is in satisfactory condition. Several welds connecting the steel bars in the open steel grid deck have cracked and several bars were noted to be loose. Areas of moderate rust are present throughout the perimeter of the steel grid deck.

The approach roadway is in good condition.

The superstructure is in poor condition. The roadway stringers that are presumed to no longer carry load are deteriorating with extensive material losses noted at the webs, bottom flanges and connections to the floorbeams. In numerous locations, the lower portion of the stringer web is completely perforated and/or the bottom flange exhibits extensive width and thickness losses. Several of the bottom flanges throughout the floorsystem are detached from the stringer webs and are hanging. In 1998 under Contract No. 345 alternating lines of stringers were removed and replaced. It has been previously determined that the bridge can safely carry the posted load of 3 tons with these 1998 stringers carrying the vehicular loading. It has been determined previously that the bridge can safely support the posted vehicular loading of 3 tons. The roadway stringers carrying live load located adjacent to the heavily deteriorated members were found to be in generally satisfactory physical condition. The end floorbeams exhibit web holes and flange losses. The majority of the end floorbeams have been temporarily supported with timber blocking that bears on the pier caps. Numerous diagonal and vertical truss members were damaged by traffic impact prior to the installation of the existing bridge vehicular railings. The resulting damage consists of bent inboard and outboard members and some loose members that do not appear to be in tension. Many of the damaged members have been supplemented with wire cables wrapped around the top and bottom panel points. Supplementary rods have been installed at several locations. Several holes occurring on the north end of the upper chord sway bracing in all spans were found in the Phoenix members.

The substructure is in poor condition. There are widespread areas of large spalls with exposed rebar, delaminations, scaling and efflorescence noted at the vertical and horizontal surfaces of the concrete pier caps. The concrete caps at Piers 4, 5 and 6 appear to exhibit greater deterioration than the other piers. The stone masonry portions of the piers are generally in satisfactory condition with occasional fine cracks in the mortar joints.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in satisfactory condition with minor deterioration to the concrete and exposed pier footings.

CALHOUN STREET TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania officer shelter is in overall satisfactory condition. There is a wide crack at the east side of the concrete foundation. There is a missing access cover at the base of the light standard at the east side of the shelter.

The New Jersey officer shelter is in overall good condition. There is a disconnected hanging wire and utility conduit in the basement.

CONCLUSIONS

CALHOUN STREET TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall poor condition due to the superstructure and substructure. A comprehensive rehabilitation should be performed on the structure. The rehabilitation should include cleaning and painting the above deck superstructure, floorsystem and sidewalk replacement, bearing replacement, steel and substructure repairs. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has selected an engineering consultant to perform the above recommended rehabilitation under Contract No. C-447B and it is anticipated that the project will begin in 2009.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

CALHOUN STREET TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania officer shelter is in overall satisfactory condition. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The New Jersey officer shelter is in overall good condition. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

Calhoun Street Toll-Supported Bridge

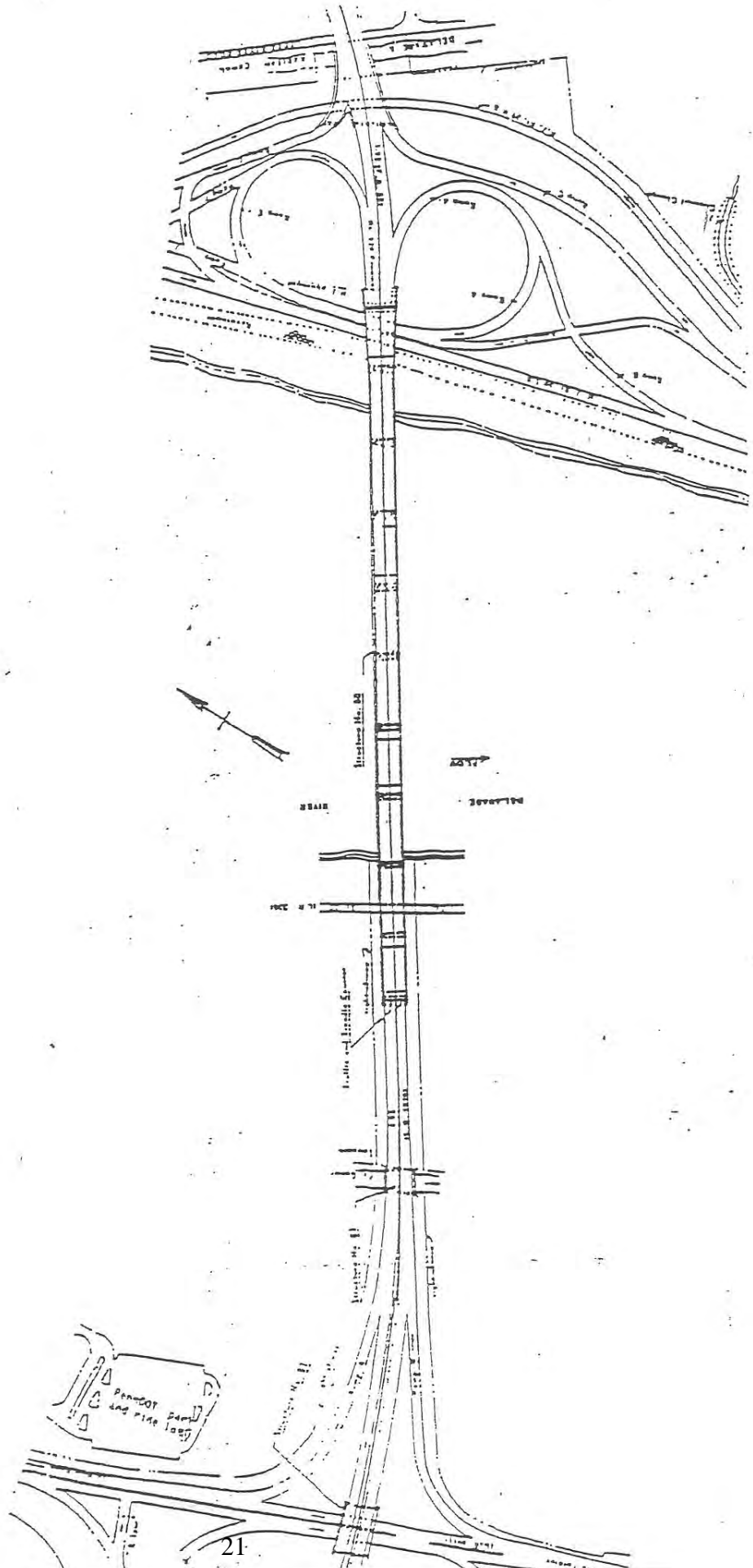
ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
447	CS TSB Rehabilitation Contract (Design / Construction)	\$13,217,000	\$578,000	\$12,596,000
	BRIDGES SUB TOTAL	\$13,217,000	\$578,000	\$12,596,000
<u>Facilities and Grounds</u>				
CSTSB	Miscellaneous Projects (less than \$100k each)	\$151,000	\$10,000	\$11,000
	FACILITIES AND GROUNDS SUB TOTAL	\$151,000	\$10,000	\$11,000
	TOTAL COST	\$13,368,000	\$588,000	\$12,607,000

SCUDDER FALLS
TOLL-SUPPORTED BRIDGES
(Structure Nos. 80, 81 & 82)

STATE OF NEW JERSEY
COUNTY OF MERCER
TOWNSHIP OF EWING

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF BUCKS
TOWNSHIP OF LOWER MAKEFIELD



SCUDDER FALLS TOLL SUPPORTED BRIDGE

GENERAL

SCUDDER FALLS TOLL-SUPPORTED BRIDGE

(10 span, riveted steel plate girder)

The Scudder Falls Toll-Supported Bridge (Structure No. 80) carries Interstate 95 over the Delaware River from Lower Makefield Township, Pennsylvania to Ewing Township, New Jersey.

The main river bridge is a ten span, riveted plate girder structure consisting of two span continuous deck girders and alternating cantilever spans. Built by the Commission in 1959 and opened to traffic on June 22, 1961, the bridge carries two dual roadways each having a curb to curb width of 27 feet with a concrete median barrier, and flanked by an upstream and downstream safety walk. The total length of the bridge is 1,740 feet. The substructure units are reinforced concrete, with stone facing on the piers.

The posted speed limit on the bridge approach roadways is 55 mph. The Commission's jurisdiction at this crossing also includes two Pennsylvania approach overpasses, one at the Pennsylvania Canal and the other at Taylorsville Road.

The Commission is moving forward with plans to improve the Scudder Falls Bridge based on conclusions contained in its Southerly Crossings Corridor Study. That study found that congestion and safety problems on the bridge were a result of its narrow configuration, the proximity of adjoining interchanges, and ramps merging onto I-95. The bridge carries more than 57,500 vehicles per day and operates at the worst level of service (LOS F) during peak rush hours. Over the next 25 years, traffic volumes are expected to increase an additional 35 percent. In cooperation with the New Jersey and Pennsylvania Departments of Transportation, the Commission is completing a preliminary engineering plan and an environmental assessment to select a preferred alternative that will improve safety and relieve anticipated congestion on the bridge and an approximate 4 mile stretch of I-95, from Route 332 in Bucks County, Pennsylvania to Bear Tavern Road in Mercer County, New Jersey. The assessment includes environmental studies, alternatives to improve safety and congestion, and preliminary engineering design. The Commission has communicated with the public regarding this project via public meetings, newsletters, and a website to reflect the current status.

PENNSYLVANIA CANAL OVERPASS

(1 span, simply supported, steel multi-stringer)

The Pennsylvania Canal Overpass (Structure No. 81) carries Interstate Route 95 over the Pennsylvania Canal in Lower Makefield Township, Pennsylvania. The structure is an approach bridge to the main Scudder Falls Toll-Supported Bridge that crosses the Delaware River.

The Pennsylvania Canal Overpass is a simple span, concrete deck, multi-stringer structure founded on reinforced concrete abutments on footings, which are supported by steel bearing piles. Opened to traffic on June 22, 1961, the bridge carries two dual roadways each with a curb to curb width of 27 feet with a concrete median barrier, and flanked by an upriver and downriver safety walk. The total span length of the bridge is 61 feet, 4 inches.

TAYLORSVILLE ROAD OVERPASS

(3 span, steel multi-stringer)

Taylorville Road Overpass (Structure No. 82) carries Interstate 95 over Taylorville Road in Lower Makefield Township, Pennsylvania and provides access to the main Scudder Falls Toll-Supported Bridge over the Delaware River. The bridge was built in 1959 and opened to traffic on June 22, 1961.

The structure is a three span, concrete deck, multi-stringer structure founded on reinforced concrete abutments and piers on footings that are supported by cast in place concrete piles. The bridge carries two dual roadways each with a curb to curb width of 27 feet with a concrete median barrier. The bridge is flanked by a north and south safety walk. The total span length of the bridge is 134 feet.

SIGNIFICANT FINDINGS

SCUDDER FALLS TOLL-SUPPORTED BRIDGE

(10 span, riveted steel plate girder)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall satisfactory condition due to minor deterioration of structural elements.

The deck is in good condition.

The approach roadways and associated ramps are in satisfactory condition. Deteriorated asphalt was noted in numerous locations more prevalent adjacent to the concrete headers. The approach roadways and ramps exhibit several small potholes and spalls.

The superstructure is in satisfactory condition. Several stringers exhibit horizontal cracks in the web. The 1st floorbeam to the east of Pier 5 exhibits a crack in the south tie plate. Sheared anchor bolts are present in the north tie plate at the 2nd floorbeam in Span 9.

The substructure is in good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in good condition with only minor spalls and minor exposure of the Pier 3 footing.

PENNSYLVANIA CANAL OVERPASS

(1 span, simply supported, steel multi-stringer)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall satisfactory condition due to minor deterioration of structural elements.

The deck is in good condition.

The approach roadway is in good condition.

The superstructure is in satisfactory condition. Heavy laminar rust is typical at the stringer ends and bearings with minor material losses.

The substructure is in good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in good condition with only fine random cracks throughout.

TAYLORSVILLE ROAD OVERPASS

(3 span, steel multi-stringer)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall fair condition due to minor deterioration of primary structural elements.

The deck is in good condition.

The approach roadway is in good condition.

The superstructure is in fair condition. Stringers exhibit moderate to heavy laminar rust at the bottom flange and lower web. Stringer 14 exhibits moderate impact damage over the right northbound lane. Heavy laminar rust is typical at the bearings with heavy debris accumulation surrounding the bearing seats.

The substructure is in satisfactory condition. The east abutment backwall exhibits a spall with exposed rebar at the north end. Medium vertical cracks are typical throughout. Several previous concrete patches have failed at Pier 2.

CONCLUSIONS

SCUDDER FALLS TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall satisfactory condition due to minor deterioration of structural elements. Under Contract 393, Interstate 95/Scudder Falls Toll-Supported Bridge Improvement

Project, the main river bridge and its approach roadways and bridges are expected to be replaced by 2013. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

PENNSYLVANIA CANAL OVERPASS

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall satisfactory condition due to minor deterioration of structural elements. Under Contract 393, Interstate 95/Scudder Falls Toll-Supported Bridge Improvement Project, the main river bridge and its approach roadways and bridges are expected to be replaced by 2013. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

TAYLORSVILLE ROAD OVERPASS

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall fair condition due to minor deterioration of primary structural elements. Under Contract 393, Interstate 95/Scudder Falls Toll-Supported Bridge Improvement Project, the main river bridge and its approach roadways and bridges are expected to be replaced by 2013. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

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Scudder Falls Toll-Supported Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS
FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
393A	I-95 / SF Improvement Project (Design, CM/CI, Construction)	\$308,660,000	\$17,139,000	\$79,879,000
BRIDGES SUB TOTAL		\$308,660,000	\$17,139,000	\$79,879,000
<u>Facilities and Grounds</u>				
SFTSB	Miscellaneous Projects (less than \$100k each)	\$176,000	\$10,000	\$11,000
FACILITIES AND GROUNDS SUB TOTAL		\$176,000	\$10,000	\$11,000
TOTAL COST		\$308,836,000	\$17,149,000	\$79,890,000

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WASHINGTON CROSSING
TOLL-SUPPORTED BRIDGE
(Structure No. 100)

GENERAL

WASHINGTON CROSSING TOLL-SUPPORTED BRIDGE

(6 span, double warren truss)

The Washington Crossing Toll-Supported Bridge (Structure No. 100) connects Mercer County Route 546 in Hopewell Township, New Jersey with PA Route 532 (George Washington Memorial Boulevard) in the Township of Taylorsville in Upper Makefield, Pennsylvania.

The structure is a six span double Warren Truss, with a total length of approximately 877 feet. The steel superstructure was built in 1904. The substructures, composed of rubble stone faced masonry, are from the original construction in 1831. The open steel grid deck provides a curb to curb width of 15 feet. The downstream side of the truss supports a cantilevered, wood planked sidewalk.

The structure is currently posted for a 3 ton weight limit restriction and a 15 mph speed limit. The structure is also posted for an 8 foot vertical clearance.

The deck joint support system was repaired under Contract No. TS-428A in 2005. This Contract consisted of repairing and replacing riser beams. High priority substructure repairs were also completed under this contract during 2005 due to post flood damage.

WASHINGTON CROSSING TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

At the southeast approach corner of the Washington Crossing Toll-Supported Bridge is a Commission owned New Jersey officer shelter.

SIGNIFICANT FINDINGS

WASHINGTON CROSSING TOLL-SUPPORTED BRIDGE

(6 span, double warren truss)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall poor condition due to the superstructure and substructure.

The deck is in good condition.

The approach roadway is in satisfactory condition. The pavement adjacent to the west abutment deck joint exhibits moderate surface wear and spalled areas. Medium transverse cracks are present throughout the roadways.

The superstructure is in poor condition. The lower chord exhibits impact damage at the north truss from panel points L2 to L4 in Span 3, L1 to L4 in Span 5 and L1 to L3 in Span 6. The south tie rod at Span 2, the south tie rod at Span 3, the north tie rod at Span 4, the north tie rod at Span 5 and the north tie rod at Span 6 have been removed. The west abutment truss bearing

appears over expanded and is in contact with the backwall. Light to moderate rust with minor section losses is typical throughout the floorsystem.

The substructure is in poor condition. The Pennsylvania abutment backwall is rotating causing the existing tooth deck joint to close completely and the concrete transition parapets to deteriorate at the base. Several wide diagonal cracks were noted at the north and south ends of the west abutment backwall from this movement and rotation.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were noted to be in satisfactory condition due to minor deterioration and undermining of the pier aprons and loss of pointing and cracks in the west abutment and wingwalls.

WASHINGTON CROSSING TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The New Jersey officer shelter is in overall good condition.

CONCLUSIONS

WASHINGTON CROSSING TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall poor condition due to the superstructure and substructure. An in-depth inspection and rating contract leading to a comprehensive rehabilitation is recommended. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has selected an engineering consultant to perform Phase 1 of the above recommended rehabilitation under Contract C-442A. Phase 2 is programmed to be started in 2012.

WASHINGTON CROSSING TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The New Jersey officer shelter is in overall good condition.

Washington Crossing Toll-Supported Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

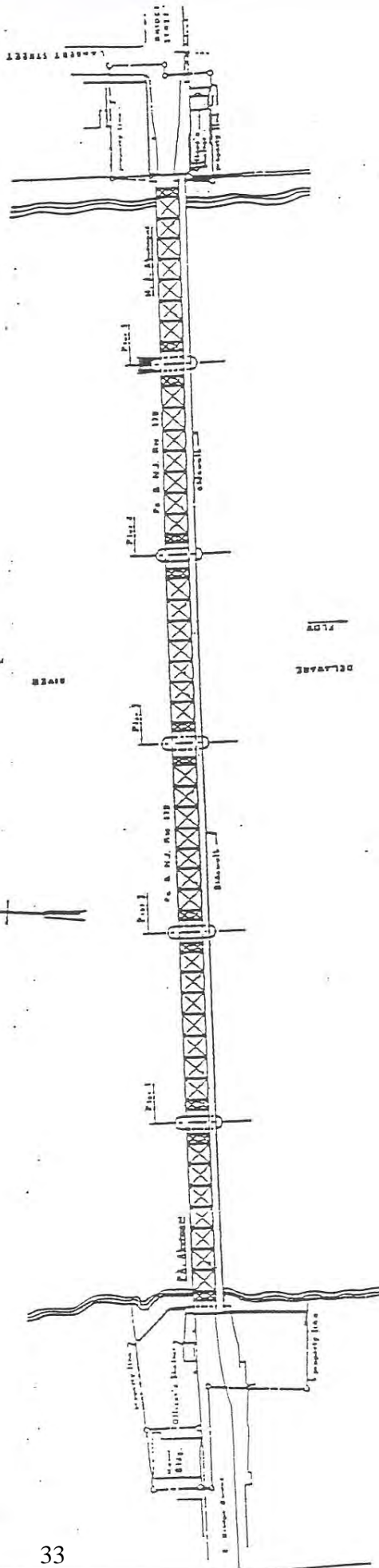
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
442A	Phase 1 Rehabilitation & Concept Study for the Washington Crossing TSB	\$3,430,000	\$1,300,000	\$1,967,000
442B	Washington Crossing TSB Phase 2 Rehabilitation (2012)	\$9,402,000	\$0	\$0
BRIDGES SUB TOTAL		\$12,832,000	\$1,300,000	\$1,967,000
<u>Facilities and Grounds</u>				
WCTSB	Miscellaneous Projects (less than \$100k each)	\$127,000	\$10,000	\$11,000
FACILITIES AND GROUNDS SUB TOTAL		\$127,000	\$10,000	\$11,000
TOTAL COST		\$12,959,000	\$1,310,000	\$1,978,000

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NEW HOPE-LAMBERTVILLE
TOLL-SUPPORTED BRIDGE
(Structure No. 120)

STATE OF NEW JERSEY
COUNTY OF HUNTERDON
CITY OF LAMBERTVILLE

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF DUCKS
BOROUGH OF NEW HOPE



NEW HOPE - LAMBERTVILLE TOLL SUPPORTED BRIDGE

GENERAL

NEW HOPE-LAMBERTVILLE TOLL-SUPPORTED BRIDGE

(6 span, pin connected pratt truss)

The New Hope-Lambertville Toll-Supported Bridge (Structure No. 120) connects Bridge Street in New Hope, Pennsylvania to Lambertville, New Jersey.

The structure, constructed in 1904, is a six span pin connected Pratt Truss with a total length of approximately 1,046 feet. The open steel grid deck provides a curb to curb width of 20 feet 7 inches. A timber plank sidewalk, installed in 1982, is supported on the downstream side by steel cantilever brackets. Abutments, wingwalls and piers are ashlar faced masonry; the piers are stone filled. All substructure units are from original construction in 1814.

The structure is currently posted for a 4 ton weight limit restriction and a 15 mph speed limit.

The bridge was rehabilitated under Contract No. TS-370A in 2004.

NEW HOPE-LAMBERTVILLE TOLL-SUPPORTED BRIDGE FACILITIES AND GROUND

At the northwest and southeast approach corners of the New Hope-Lambertville Toll-Supported Bridge are Commission owned Pennsylvania and New Jersey officer shelters. At the Pennsylvania side of the bridge is a Commission owned former firehouse that primarily functions as a storage facility for the Commission.

SIGNIFICANT FINDINGS

NEW HOPE-LAMBERTVILLE TOLL-SUPPORTED BRIDGE

(6 span, pin connected pratt truss)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall satisfactory condition due to minor deterioration of structural elements.

The deck and approach roadway are in good condition.

The superstructure is in satisfactory condition. Several north truss lower chord members in Span 5 exhibit impact damage. Many truss member's exhibit minor section losses that have been arrested by paint.

The substructure is in good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in satisfactory condition, exhibiting minor deterioration including undermining of the pier aprons.

NEW HOPE-LAMBERTVILLE TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania and New Jersey officer shelters are in overall good condition.

The former firehouse is in overall good condition. A detailed inspection of the former firehouse was not performed due to the facility being used for the storage of items from the New Hope – Lambertville Toll Bridge Administration Building Renovations and Addition, which was undergoing construction during the time of the inspection.

CONCLUSIONS

NEW HOPE-LAMBERTVILLE TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall satisfactory condition due to minor deterioration of structural elements. Riprap should be installed around the concrete aprons at all piers. Sections of the damaged concrete apron should be repaired. Cracks in the concrete aprons should be sealed. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

Priority repairs to Pier 2 were completed in 2007 under Contract No. DB-0457B

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

NEW HOPE-LAMBERTVILLE TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania and New Jersey officer shelters are in overall good condition.

The former firehouse is in overall good condition.

New Hope-Lambertville Toll-Supported Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

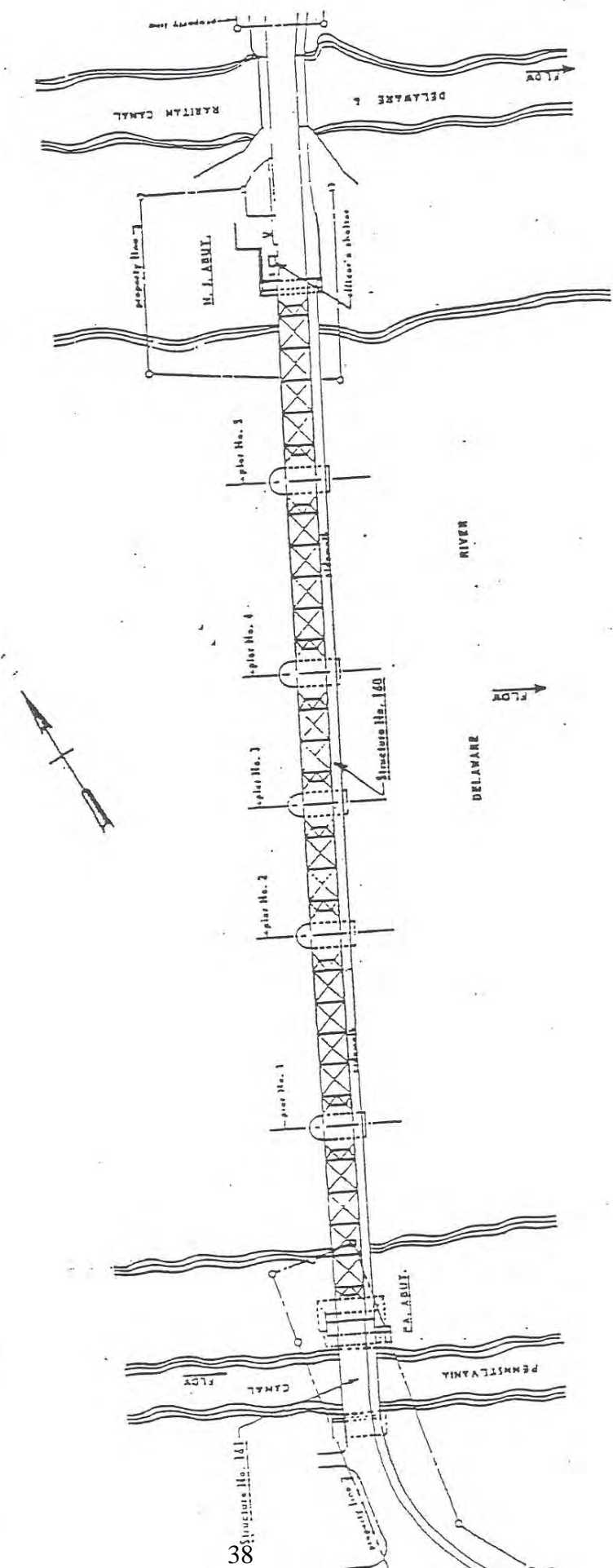
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
The bridge was rehabilitated in 2004				
BRIDGES SUB TOTAL		\$0	\$0	\$0
<u>Facilities and Grounds</u>				
NHLTSB	Miscellaneous Projects (less than \$100k each)	\$137,000	\$20,000	\$11,000
FACILITIES AND GROUNDS SUB TOTAL		\$137,000	\$20,000	\$11,000
TOTAL COST		\$137,000	\$20,000	\$11,000

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CENTRE BRIDGE-STOCKTON
TOLL-SUPPORTED BRIDGES
(Structure Nos. 160 & 161)

STATE OF NEW JERSEY
COUNTY OF HUNTERDON
TOWNSHIP OF DELAWARE
BOROUGH OF STOCKTON

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF BUICKS
TOWNSHIP OF SOLEBURY
CENTRE BRIDGE



CENTRE BRIDGE - STOCKTON TOLL SUPPORTED BRIDGE

GENERAL

CENTRE BRIDGE-STOCKTON TOLL-SUPPORTED BRIDGE

(6 span, riveted steel warren truss)

The Centre Bridge-Stockton Toll-Supported Bridge (Structure No. 160) connects PA Route 32 in Solebury Township, Pennsylvania to NJ Route 29 in Stockton, New Jersey.

The bridge, opened to traffic in 1927, is a six span, riveted steel Warren Truss structure, with a total length of approximately 825 feet. The open steel grid deck, provides a curb to curb width of 19 feet, 11 ½ inches. In addition, a six foot timber plank sidewalk, is supported on the downriver truss on steel cantilever brackets. The piers and abutments originally constructed in 1814 from random ashlar masonry are stone filled and rest upon timber crib foundations. In 1926 portions of the piers were encased with reinforced concrete.

The structure is currently posted for a 5 ton weight limit restriction and a 25 mph speed limit. The structure is also posted for a 12 feet vertical clearance.

A comprehensive rehabilitation of the Centre Bridge-Stockton Toll-Supported Bridge was completed in 2007 under Contract No. TS-429A.

CENTRE BRIDGE-STOCKTON TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

At the northeast approach corner of the Centre Bridge-Stockton Toll-Supported Bridge is a Commission owned New Jersey officer shelter.

PENNSYLVANIA CANAL OVERPASS

(1 span, prestressed concrete adjacent box beams)

The Pennsylvania Canal Overpass (Structure No. 161) carries traffic over the Pennsylvania Canal in Solebury Township, PA. The structure is an approach bridge to the main Centre Bridge-Stockton Toll-Supported Bridge that crosses the Delaware River.

The Pennsylvania Canal Overpass is a simple span, prestressed concrete adjacent box beam structure. The curb to curb width is 19 feet, 11 ½ inches and the span length is 63 feet.

A comprehensive rehabilitation of the Pennsylvania Canal Overpass was completed in 2007 under Contract TS-429A.

SIGNIFICANT FINDINGS

CENTRE BRIDGE-STOCKTON TOLL-SUPPORTED BRIDGE

(6 span, riveted steel warren truss)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall good condition.

The deck, approach roadway, superstructure and substructure are in good condition.

An underwater inspection was performed in 2006 under Contract NO. C-467D. The substructure units below the waterline were found to be in fair condition due breastwall deterioration but this condition was repaired under Contract No. TS-429A.

CENTRE BRIDGE-STOCKTON TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The New Jersey officer shelter is in overall good condition. There is a small spall at the rear of the retaining wall.

PENNSYLVANIA CANAL OVERPASS

(1 span, prestressed concrete adjacent box beams)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall good condition.

The deck, approach roadway and substructure are in good condition.

The superstructure is in very good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in good condition with only minor spalls and cracks.

CONCLUSIONS

CENTRE BRIDGE-STOCKTON TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall good condition. Riprap should be installed at the east face of Pier 1, the entire perimeter of Piers 2 and 3 and at the north and east faces of Piers 4 and 5. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

CENTRE BRIDGE-STOCKTON TOLL-SUPPORTED BRIDGE FACILITIES AND
GROUNDS

The New Jersey officer shelter is in overall good condition. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

PENNSYLVANIA CANAL OVERPASS

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall good condition. Unsound concrete should be removed from the north and south ends of the east and west abutment breastwalls and patch. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

Centre Bridge-Stockton Toll-Supported Bridge

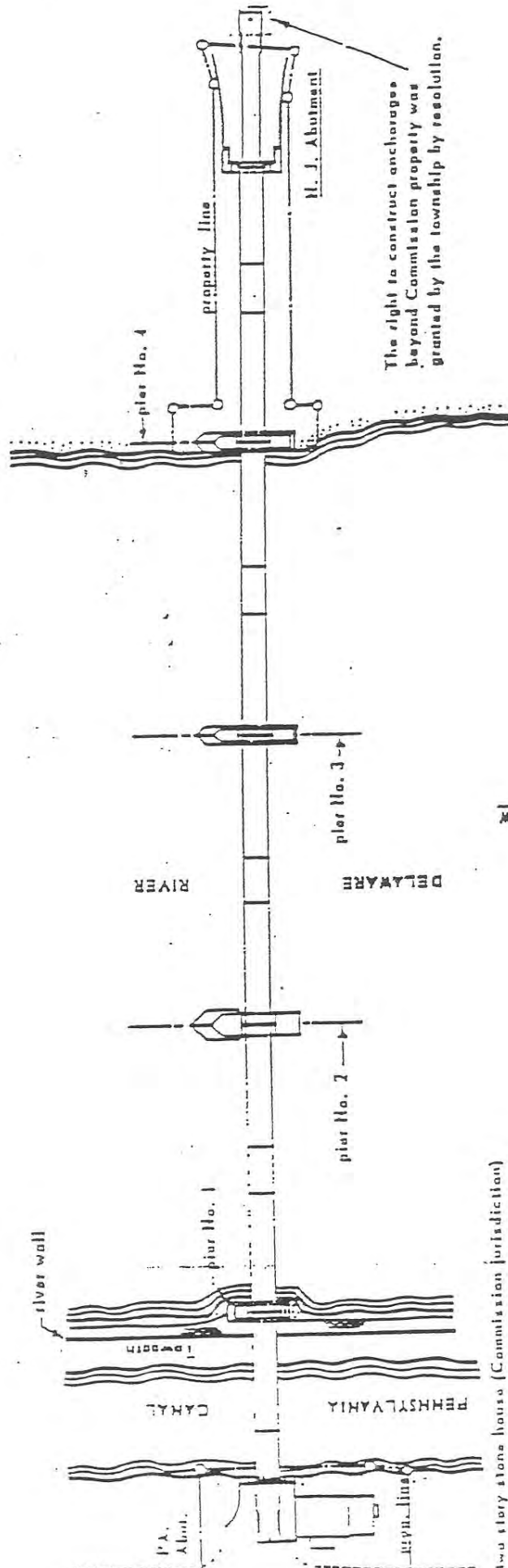
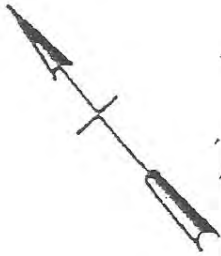
ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
The bridge was recently rehabilitated in 2007				
BRIDGES SUB TOTAL		\$0	\$0	\$0
<u>Facilities and Grounds</u>				
CBSTSB	Miscellaneous Projects (less than \$100k each)	\$64,000	\$5,000	\$6,000
FACILITIES AND GROUNDS SUB TOTAL		\$64,000	\$5,000	\$6,000
TOTAL COST		\$64,000	\$5,000	\$6,000

LUMBERVILLE-RAVEN ROCK
TOLL-SUPPORTED BRIDGE
(Structure No. 180)

STATE OF NEW JERSEY
COUNTY OF HURTERDON
TOWNSHIP OF DELAWARE
RAVEN ROCK

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF BUCKS
TOWNSHIP OF SOLEBURY
LUMBERVILLE



LUMBERVILLE - RAVEN ROCK TOLL SUPPORTED BRIDGE

GENERAL

LUMBERVILLE-RAVEN ROCK TOLL-SUPPORTED BRIDGE

(5 span, suspension)

The Lumberville-Raven Rock Toll-Supported Bridge (Structure No. 180) connects Solebury Township (Lumberville) in Pennsylvania with Delaware Township (Raven Rock) in New Jersey.

This pedestrian bridge is a five span suspension bridge with straight backstays and a precast waffle style concrete slab held together by longitudinal post tensioning web cables. The floor system is strengthened by cable trusses along each suspension cable. The width of the walkway is 7 feet, 7 inches and the structure length is approximately 688 feet.

The bridge was closed to vehicular traffic in February of 1944. In 1947, the superstructure was re-built on the original 1856 masonry substructure. A major rehabilitation contract was completed in 1993 that included a new deck slab, pier and abutment repointing, approach sidewalks and bridge lighting. The entire bridge was last painted in 1980 by Maintenance forces and the towers were again painted in 1990.

LUMBERVILLE-RAVEN ROCK TOLL-SUPPORTED BRIDGE FACILITY AND GROUNDS

At the southwest corner of the Lumberville-Raven Rock Toll-Supported Bridge is a Commission owned house. Adjacent to this Commission owned house and property is a retaining wall along the Pennsylvania Canal.

SIGNIFICANT FINDINGS

LUMBERVILLE-RAVEN ROCK TOLL-SUPPORTED BRIDGE

(5 span, suspension)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall poor condition due to the substructure.

The deck and approach walkways are in good condition.

The superstructure is in fair condition. Both fascia girders exhibit areas of light to moderate pack rust at the bottom flange more prevalent adjacent to the bridge scuppers due to water infiltration. Struts exhibit light to moderate pack rust at the fascia ends. Pitting with areas of up to 50% material loss is present at the pipe cross bracing. The suspension towers exhibit areas of light to severe pack rust at the tower base.

The substructure is in poor condition. The concrete aprons at Piers 1 and 4 exhibit random wide cracks. The concrete aprons at Piers 2 and 3 are missing several sections exposing the timber cribbing pier foundations. The remaining sections exhibit wide cracks and undermining.

An underwater inspection was performed in 2006 under Contract No.C-467D. The substructure units below the waterline were found to be in poor condition due to undermining and deteriorated concrete aprons at Piers 2 and 3.

LUMBERVILLE-RAVEN ROCK TOLL-SUPPORTED BRIDGE FACILITY AND GROUNDS

The house is in overall poor condition and exhibits exterior and interior paint peeling, deteriorated wood porch framing, failed window sealers, exposed wires and a leaning oil tank foundation.

The southwest retaining wall along the Pennsylvania Canal is partially collapsed and leaning. The stones have become loose throughout.

CONCLUSIONS

LUMBERVILLE-RAVEN ROCK TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall poor condition due to the substructure. The bridge should be cleaned and painted. Riprap should be installed at Piers 1, 2, 3 and the west abutment. The deteriorated portions of the concrete aprons at Piers 2 and 3 should be reconstructed. The wide crack in the concrete apron at Pier 1 should be sealed. Voids in the stone masonry at Piers 1 and 4 should be filled. Missing and deteriorated pointing throughout the substructure units should be replaced and the cracks in the stone masonry should be sealed. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

LUMBERVILLE-RAVEN ROCK TOLL-SUPPORTED BRIDGE FACILITY AND GROUNDS

The house and retaining wall are in overall poor condition. The Commission should consider undertaking a study to repair and upgrade the condition of the house and the adjacent stone retaining wall.

Lumberville-Raven Rock Pedestrian Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

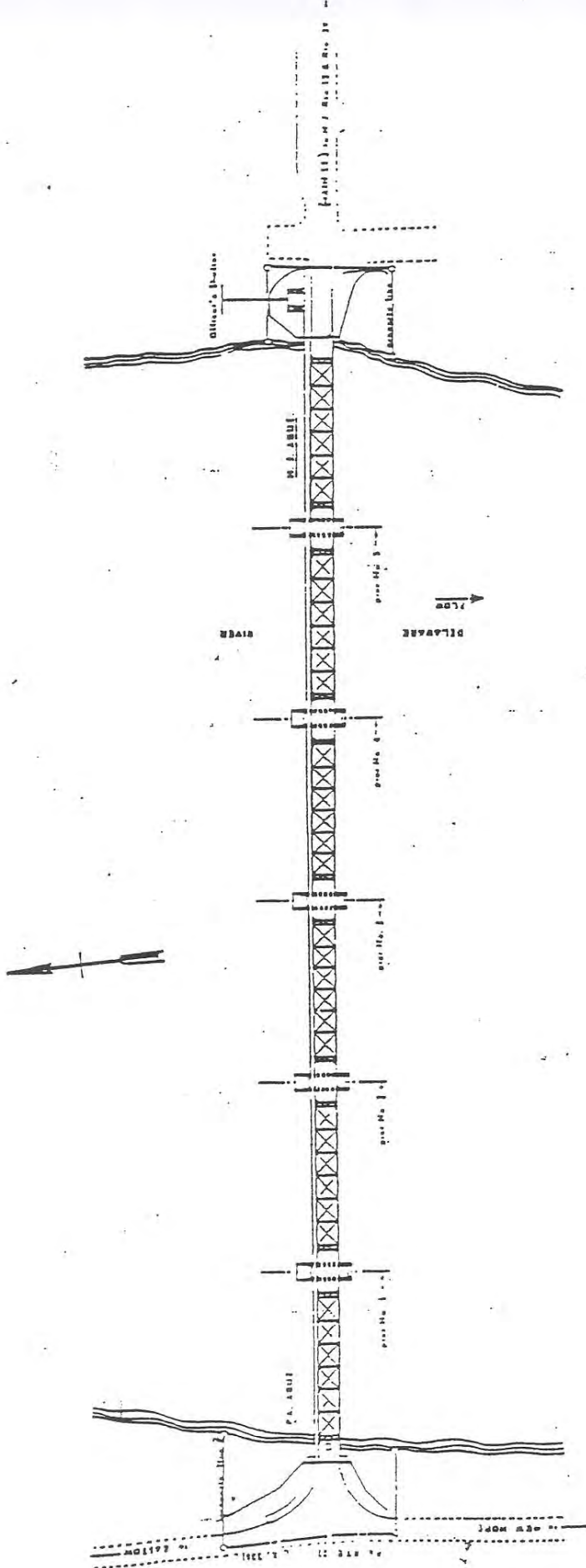
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
443	L-RR TSB Rehabilitation	\$2,532,000	\$208,000	\$1,021,000
525	L-RR TSB Retaining Wall Reconstruction & Bridge House Rehabilitation	\$1,239,000	\$238,000	\$806,000
BRIDGES SUB TOTAL		\$3,771,000	\$446,000	\$1,827,000
<u>Facilities and Grounds</u>				
LRRTSB	Miscellaneous Projects (less than \$100k each)	\$127,000	\$10,000	\$11,000
FACILITIES AND GROUNDS SUB TOTAL		\$127,000	\$10,000	\$11,000
TOTAL COST		\$3,898,000	\$456,000	\$1,838,000

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UHLERSTOWN-FRENCHTOWN
TOLL-SUPPORTED BRIDGE
(Structure No. 220)

STATE OF NEW JERSEY
COUNTY OF HUNTERDON
BOROUGH OF FRENCHTOWN

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF BUCKS
TOWNSHIP OF THIRDCUM
UHLERTOWN



UHLERTOWN - FRENCHTOWN TOLL SUPPORTED BRIDGE

GENERAL

UHLERSTOWN-FRENCHTOWN TOLL-SUPPORTED BRIDGE

(6 span, riveted steel warren truss)

The Uhlerstown-Frenchtown Toll-Supported Bridge (Structure No. 220) carries Bridge Street traffic from Uhlerstown, Tincum Township in Pennsylvania to Frenchtown, New Jersey.

The bridge, which rests on the original masonry substructure built in 1843, consists of a six span riveted steel Warren Truss structure, built in 1931. The open steel grid deck, added in 1949, provides a curb to curb width of 16 feet 6 inches. The structure is approximately 950 feet in length. A concrete filled steel grid sidewalk is supported by the upstream truss on steel cantilever brackets.

The structure is currently posted for a 15 ton weight limit restriction and a 15 mph speed limit. The structure is also posted for a 12 foot 6 inch vertical clearance.

The structure was rehabilitated in 2001 under Contract No. TS-363.

UHLERSTOWN-FRENCHTOWN TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

At the northeast approach corner of the Uhlerstown-Frenchtown Toll-Supported Bridge is a Commission owned New Jersey officer shelter.

SIGNIFICANT FINDINGS

UHLERSTOWN-FRENCHTOWN TOLL-SUPPORTED BRIDGE

(6 span, riveted steel warren truss)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall good condition.

The deck, approach roadway, superstructure and substructure are in good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in satisfactory condition exhibiting minor determination including undermining of the aprons.

UHLERSTOWN-FRENCHTOWN TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The New Jersey officer shelter is in overall good condition. There are cracks and spalls in the retaining wall coating adjacent to the shelter.

CONCLUSIONS

UHLERSTOWN-FRENCHTOWN TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall good condition. The undermining of the concrete aprons and the west abutment should be repaired. The cracks in the concrete aprons and pier caps should be repaired and the stone masonry joints in the east abutment and Pier 1 should be repaired. Riprap at the west abutment protection wall should be installed. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

UHLERSTOWN-FRENCHTOWN TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The New Jersey officer shelter is in overall good condition. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

Uhlerstown-Frenchtown Toll-Supported Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

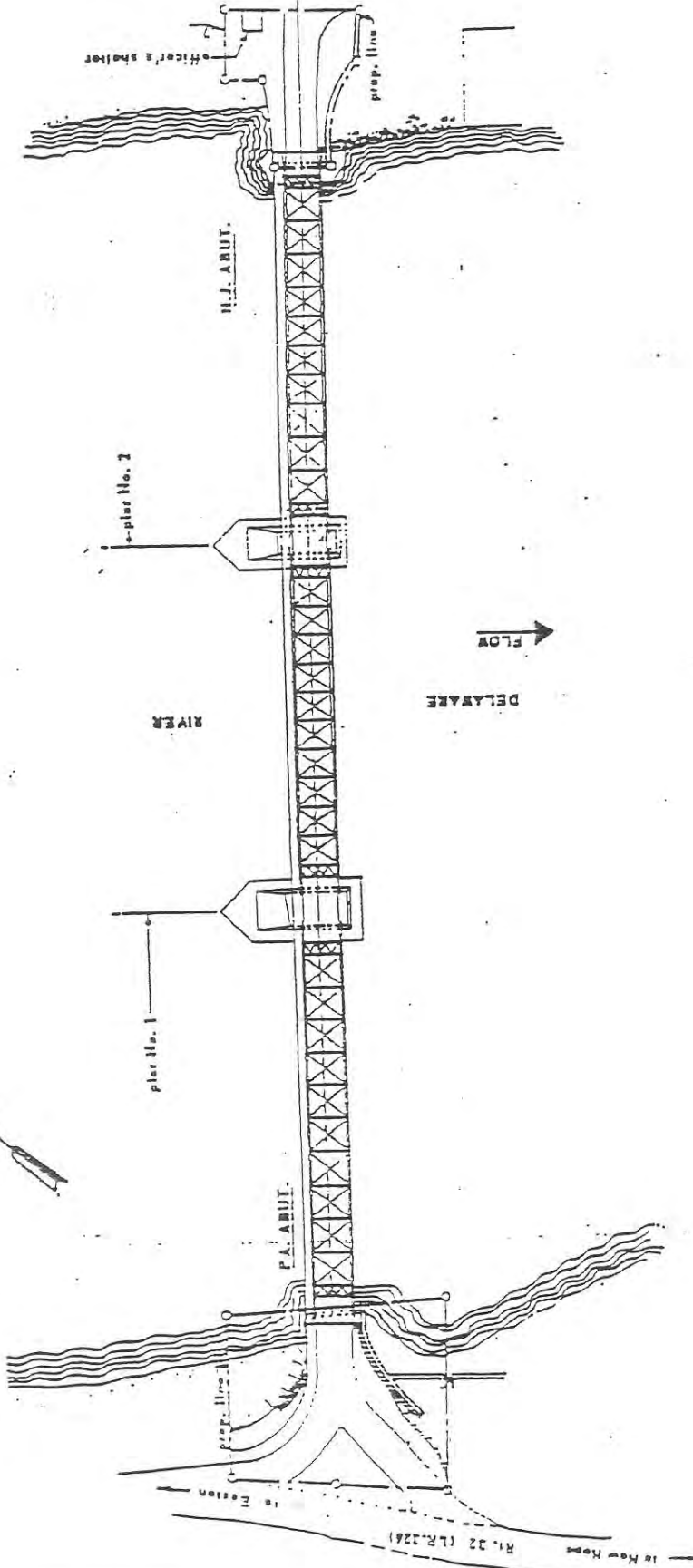
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
The bridge was rehabilitated in 2001.				
BRIDGES SUB TOTAL		\$0	\$0	\$0
<u>Facilities and Grounds</u>				
UFTSB	Miscellaneous Projects (less than \$100k each)	\$380,000	\$30,000	\$32,000
FACILITIES AND GROUNDS SUB TOTAL		\$380,000	\$30,000	\$32,000
TOTAL COST		\$380,000	\$30,000	\$32,000

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UPPER BLACK EDDY-MILFORD
TOLL-SUPPORTED BRIDGE
(Structure No. 240)

STATE OF NEW JERSEY
COUNTY OF HUNTERDON
BOROUGH OF MILFORD

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF BUCKS
TOWNSHIP OF BRIDGETON
UPPER BLACK EDDY



UPPER BLACK EDDY - MILFORD TOLL SUPPORTED BRIDGE

GENERAL

UPPER BLACK EDDY-MILFORD TOLL-SUPPORTED BRIDGE

(3 span, warren truss)

The Upper Black Eddy-Milford Toll-Supported Bridge (Structure No. 240) extends over the Delaware River and connects PA Route 32 and Hunterdon County Route 619 via Bridge Street from Upper Black Eddy, Bridgeton Township, Pennsylvania to Milford Borough, New Jersey.

The bridge, constructed in 1933, is a three span Warren Truss structure, with a total length of approximately 700 feet. The deck consists of concrete filled steel inverted "T's" and provides a curb to curb width of 20 feet. Both abutments, recapped with reinforced concrete following flood damage, were originally built in 1842 with rubble faced masonry. The piers, built in 1842, are stone filled having also been recapped with reinforced concrete.

The structure is posted for a 15 mph speed limit.

In 1996 a new galvanized plate sidewalk was added to the bridge and is supported on the upriver truss on steel cantilever brackets. Substructure units were repointed in 1998 under Contract 347.

UPPER BLACK EDDY-MILFORD TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

At the northeast approach corner of the Upper Black Eddy-Milford Toll-Supported Bridge is a Commission owned New Jersey officer shelter.

SIGNIFICANT FINDINGS

UPPER BLACK EDDY-MILFORD TOLL-SUPPORTED BRIDGE

(3 span, warren truss)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall fair condition due to minor deterioration of primary structural elements.

The deck is in satisfactory condition. The top of deck exhibits light to moderate wearing throughout all spans. The underside of the steel grid deck typically exhibits light to moderate rust with moderate to heavy rust below the deck joints and fascia stringers. The steel curbs exhibit light to moderate rust with heavy rust noted at the curb support.

The approach roadway is in satisfactory condition. The New Jersey approach roadway exhibits medium transverse and longitudinal cracks throughout. Steel plating is exposed adjacent to the east abutment deck joint due to deteriorated asphalt.

The superstructure is in satisfactory condition. The steel floorbeams and stringers typically exhibit light to moderate rust and peeling paint with areas of minor section losses. The top flange of the roadway stringers typically exhibit severe rust due to water leakage through the steel grid deck. Severe rust and significant pack rust build up is typical at the fascia stringer bearings. Random areas of severe rust with minor section losses exist on several truss members; however no significant material loss was noted. The paint is in overall moderate to poor condition throughout the structure. The truss rocker bearings are in minor expansion with moderate to severe rust throughout.

The substructure is in fair condition. The east and west abutments exhibit cracked and missing mortar and a few missing stones. The two piers exhibit random areas of missing mortar. Severe scaling and hollow sounding areas is typical at the concrete pier caps.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in satisfactory condition due to minor deterioration to the concrete at the abutments.

UPPER BLACK EDDY-MILFORD TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The New Jersey officer shelter is in overall satisfactory condition. The slope protection at the north side of the shelter is eroding. There are cracks in the shelter sidewalk. The roof trim exhibits minor peeling of paint.

CONCLUSIONS

UPPER BLACK EDDY-MILFORD TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall fair condition due to minor deterioration of primary structural elements. An in-depth inspection leading to a comprehensive rehabilitation including substructure and scour evaluation is scheduled under Contract No. C-444A. An engineering consultant has been selected for Contract No. C-444A and it is anticipated that this project will begin in 2009. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

UPPER BLACK EDDY-MILFORD TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The New Jersey officer shelter is in overall satisfactory condition. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

Upper Black Eddy-Milford Toll-Supported Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

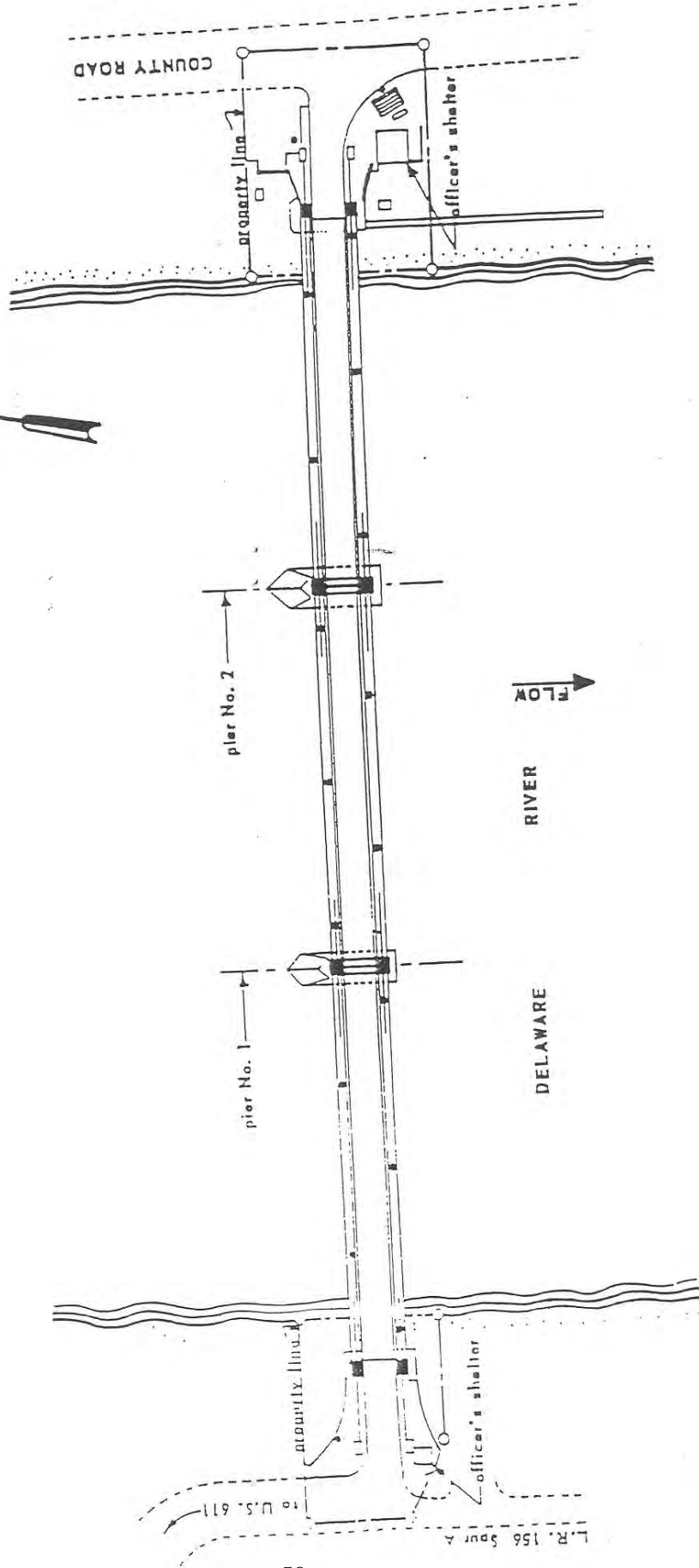
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
444	Upper Black Eddy - Milford TSB Rehabilitation	\$12,571,000	\$752,000	\$10,182,000
	BRIDGES SUB TOTAL	\$12,571,000	\$752,000	\$10,182,000
<u>Facilities and Grounds</u>				
UBEMTSB	Miscellaneous Projects (less than \$100k each)	\$190,000	\$15,000	\$16,000
	FACILITIES AND GROUNDS SUB TOTAL	\$190,000	\$15,000	\$16,000
	TOTAL COST	\$12,761,000	\$767,000	\$10,198,000

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RIEGELSVILLE
TOLL-SUPPORTED BRIDGE
(Structure No. 260)

STATE OF NEW JERSEY
COUNTY OF WARREN
BOROUGH OF RIEGELSVILLE

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF BUCKS
BOROUGH OF RIEGELSVILLE



RIEGLSVILLE TOLL SUPPORTED BRIDGE

GENERAL

RIEGELSVILLE TOLL-SUPPORTED BRIDGE

(3 span, suspension)

The Riegelsville Toll-Supported Bridge (Structure No. 260) connects Durham Township in Pennsylvania with Pohatcong Township in New Jersey.

The bridge, constructed in 1904, is a three span Cable Suspension bridge with straight backstays and a total length of approximately 577 feet. The open steel grid deck, supported by a king post floorbeam system, provides a curb to curb width of 15 feet 11 inches. A timber plank sidewalk rests on floorbeam cantilevers on both fascias. The sidewalk railing is actually a double Warren Truss, assisting in strengthening the bridge roadway. The substructure, originally built in 1835, was raised and built up in 1904.

The structure is currently posted for a 2 ½ ton weight limit restriction and a 15 mph speed limit.

Under Contract TS-391, the Riegelsville Toll-Supported Bridge has undergone the first step in a full rehabilitation, as part of the Commission's 10 year capital program addressing improvements to many of the bridges. Work consisted of strengthening towers on the river piers, replacement of hanger blocks connecting vertical hangers to the floorbeams, repair of floorbeam bearings at each end of the floorbeams of the three spans, concrete repair on pier two and concrete crack repairs at the anchorages. The bridge was last painted by contract in 1985. A cleaning and pointing contract was completed for the substructure in 1998. Contract TS-461A repaired the damaged concrete aprons and additional damage from the Flood of June 2006.

RIEGELSVILLE TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

At the southwest and southeast approach corners of the Riegelsville Toll-Supported Bridge are Commission owned Pennsylvania and New Jersey officer shelters.

SIGNIFICANT FINDINGS

RIEGELSVILLE TOLL-SUPPORTED BRIDGE

(3 span, suspension)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall poor condition due to the condition of the superstructure.

The deck is in good condition.

The approach roadway is in poor condition. The east approach roadway exhibits areas of moderate surface wear with uneven concrete typical near the curb lines. Subsequent to the inspection this roadway was repaved under a Compact Authorized Investment project.

The superstructure is in poor condition. The steel floorbeams exhibit severe corrosion. Approximately 40 percent of all floorbeams exhibit numerous web holes (up to full height of web). Heavy to severe rust with minor material losses is typical at the bottom flange of the floorbeams. Severe corrosion and material losses is typical at the cross bracing members. U-bolt connecting cables typically exhibit minor material losses.

The substructure is in satisfactory condition. The abutments exhibit several wide random cracks. Heavy scaling is typical at the east abutment and the Pier 1 and 2 bridge seats.

An underwater inspection was performed in 2006 by under Contract No. C-467D. The substructure units below the waterline were found to be in fair condition due to deteriorated concrete at the abutments and pier aprons.

RIEGELSVILLE TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania officer shelter is in overall good condition.

The New Jersey officer shelter is in overall fair to poor condition. The window frames exhibit cracks and paint peeling. The shelter floor exhibits areas of rot and decay and temporary timber supports have been installed to support the floor system.

CONCLUSIONS

RIEGELSVILLE TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall poor condition due to the superstructure. This bridge is currently scheduled for a comprehensive rehabilitation with design starting in 2009 under Contract No. 445. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

RIEGELSVILLE TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania officer shelter is in overall good condition.

The New Jersey officer shelter is in overall fair to poor condition. The Commission should consider undertaking a study to determine whether the shelter can be rehabilitated or should be replaced. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

Riegelsville Toll-Supported Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

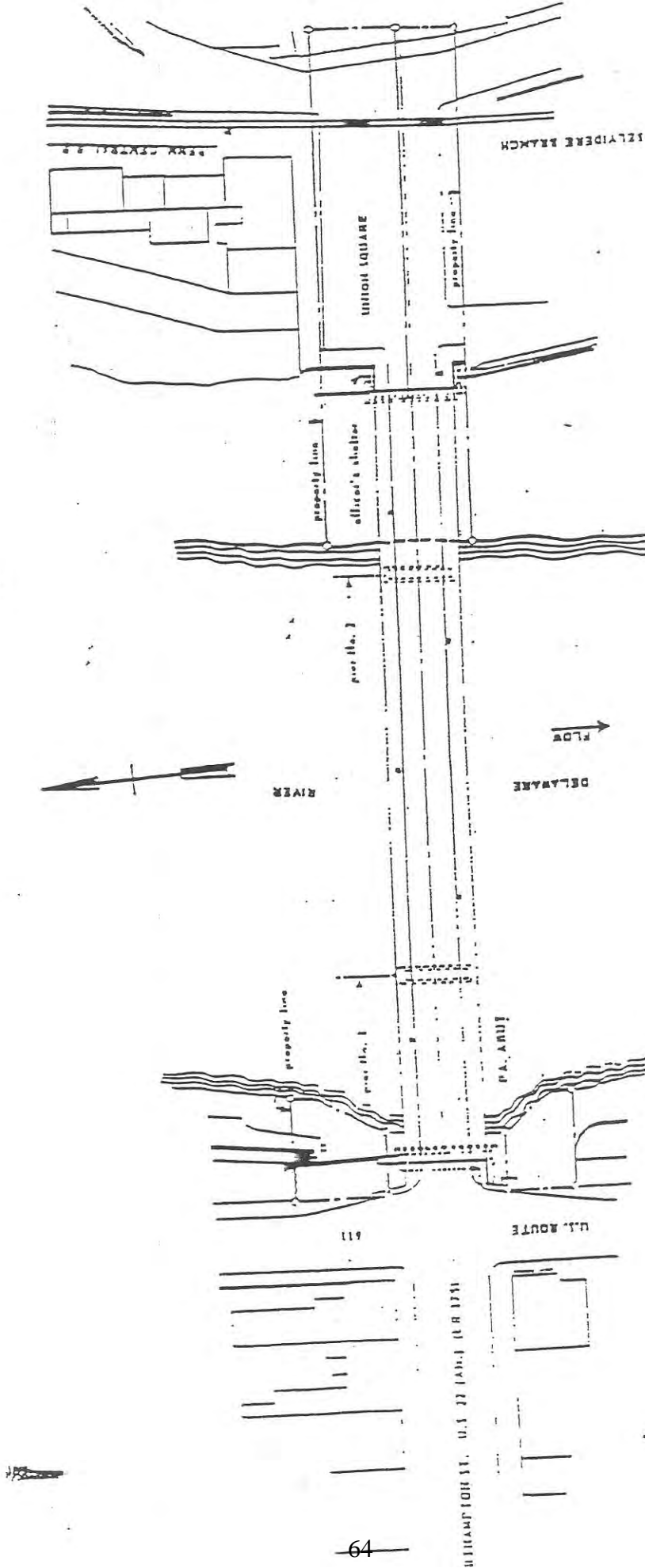
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
445	RGL Rehabilitation	\$6,955,000	\$742,000	\$6,213,000
BRIDGES SUB TOTAL		\$6,955,000	\$742,000	\$6,213,000
<u>Facilities and Grounds</u>				
RTSB	Miscellaneous Projects (less than \$100k each)	\$127,000	\$10,000	\$11,000
FACILITIES AND GROUNDS SUB TOTAL		\$127,000	\$10,000	\$11,000
TOTAL COST		\$7,082,000	\$752,000	\$6,224,000

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NORTHAMPTON STREET
TOLL-SUPPORTED BRIDGE
(Structure No. 280)

STATE OF NEW JERSEY
COUNTY OF WARREN
TOWN OF PHILLIPSBURG

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF NORTHAMPTON
CITY OF EASTON



NORTHAMPTON STREET TOLL SUPPORTED BRIDGE

GENERAL

NORTHAMPTON STREET TOLL-SUPPORTED BRIDGE

(3 span, cantilevered truss)

The Northampton Street Toll-Supported Bridge (Structure No. 280), just south of the Easton-Phillipsburg Toll Bridge, connects Easton, Pennsylvania to Phillipsburg, New Jersey.

The bridge, although aesthetically resembling a suspension bridge, is a double cantilever truss structure, adjoined by a center (main) suspended span. The three lane open steel grid deck provides a curb to curb width of 32 feet and a total bridge length of 550 feet. The current bridge was constructed in 1895, with a major rehabilitation in 2001 under Contract TS-365. Repairs were completed due to flood damages in 2005 and 2006.

The structure is currently posted for a 3 ton weight limit restriction and a 15 mph speed limit.

NORTHAMPTON STREET TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

At the southwest and northeast approach corners of the Northampton Street Toll-Supported Bridge are Commission owned Pennsylvania and New Jersey officer shelters.

SIGNIFICANT FINDINGS

NORTHAMPTON STREET TOLL-SUPPORTED BRIDGE

(3 span, cantilevered truss)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall satisfactory condition due to minor deterioration of structural elements.

The deck and substructure are in good condition.

The approach roadway is in satisfactory condition. The east approach roadway exhibits areas of medium to wide mapcracking and moderate wear with uneven pavement in the westbound lane. This roadway was resurfaced under Contract No. TS-499A.

The superstructure is in satisfactory condition. The floorbeams and stringers typically exhibit minor section losses. Several stringers exhibit minor impact damage. Stringer 9 at L9L10 is bent 5" to the south due to impact damage, and the 3rd riser beam from the west exhibits a 2 ½" cracked weld at the east side with 3 of 4 connection bolts missing. Impact damage is present at the lower chord in several locations. The eyebars and pin nuts at the suspended portion of Span 2 exhibit movement and the eyebars are loose. There has been no change in movement since the previous inspection. Previously in 2007 a special inspection was performed to determine the source of an audible noise reported by the bridge officers near the west abutment. No reports of this noise have been reported in 2008.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in satisfactory condition due to minor deterioration of mortar joints at the west abutment.

NORTHAMPTON STREET TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania and New Jersey officer shelters are in overall good condition. Lavatory facilities at the officer shelters have recently been upgraded by Commission Maintenance Forces.

CONCLUSIONS

NORTHAMPTON STREET TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall satisfactory condition due to minor deterioration in structural elements. The stone masonry joints in the abutments and wingwalls should be repaired. Cracks in the concrete aprons at Pier 1 and 2 should be pressure injected. The concrete apron at Pier 2 should be repaired and riprap installed around Pier 2. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

NORTHAMPTON STREET TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The Pennsylvania and New Jersey officer shelters are in overall good condition. Lavatory facilities at the officer shelters have been upgraded by Commission Maintenance Forces.

Northampton Street Toll-Supported Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

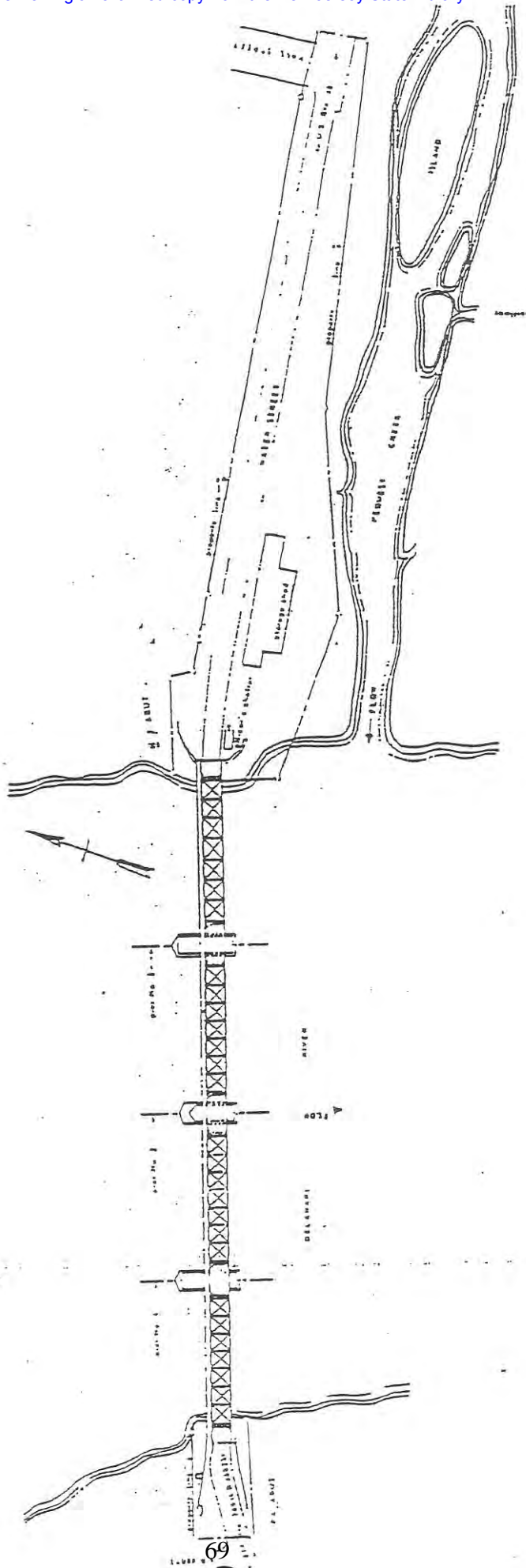
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
The bridge was rehabilitated in 2002.				
BRIDGES SUB TOTAL		\$0	\$0	\$0
<u>Facilities and Grounds</u>				
NHSTSB	Miscellaneous Projects (less than \$100k each)	\$647,000	\$50,000	\$52,000
FACILITIES AND GROUNDS SUB TOTAL		\$647,000	\$50,000	\$52,000
TOTAL COST		\$647,000	\$50,000	\$52,000

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RIVERTON-BELVIDERE
TOLL-SUPPORTED BRIDGE
(Structure No. 320)

STATE OF NEW JERSEY
COUNTY OF WARREN
TOWN OF BELVIDERE

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF NORTHAMPTON
TOWNSHIP OF LOWER MOUNT BETHEL
RIVERTON



RIVERTON - BELVIDERE TOLL SUPPORTED BRIDGE

GENERAL

RIVERTON-BELVIDERE TOLL-SUPPORTED BRIDGE

(4 span, riveted steel, double warren truss)

The Riverton-Belvidere Toll-Supported Bridge (Structure No. 320) carries Water Street across the Delaware River and connects Riverton, Lower Mount Bethel Township, Pennsylvania with the Town of Belvidere, New Jersey.

The bridge, constructed in 1904, is a four span, riveted steel, double Warren Truss structure, with a total length of approximately 653 feet. The open steel grid deck provides a curb to curb width of 16 feet, 4 inches. In addition, a concrete filled steel grid sidewalk is supported on the upriver truss with steel cantilever brackets.

The piers and the Pennsylvania abutment are rough ashlar faced masonry and stone filled. The piers are supported on timber cribs and lower portions are concrete filled steel sheet piling (1929-32). The New Jersey abutment, including its wingwalls, is constructed of concrete on timber piles.

The bridge is currently posted for an 8 ton weight limit restriction and a 15 mph speed limit.

A comprehensive bridge rehabilitation was completed under Contract No. TS-371A in 2007.

RIVERTON-BELVIDERE TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

At the southeast approach corner of the Riverton-Belvidere Toll-Supported Bridge is a Commission owned New Jersey officer shelter. A commission owned storage garage is also at the southeast end of the bridge.

SIGNIFICANT FINDINGS

RIVERTON-BELVIDERE TOLL-SUPPORTED BRIDGE

(4 span, riveted steel, double warren truss)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall good condition.

The deck, approach roadway, superstructure and substructure are in good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in satisfactory condition due to minor deterioration including concrete cracks at the abutments and undermining at Pier 2. The cracks were repaired under Contract No. TS-371A.

RIVERTON-BELVIDERE TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The New Jersey officer shelter is in overall poor condition. The bathroom floor exhibits areas of rotted timber and is uneven. The entire shelter is pitched towards the south.

The storage garage is in overall poor condition. There are numerous holes in the roof causing water leakage throughout the garage floor. The roof is deteriorating and exhibits vegetation growth throughout.

CONCLUSIONS

RIVERTON-BELVIDERE TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting the posted load.

The structure is in overall good condition. Scour protection including the installation of riprap at the east and west abutments and Pier 2 is recommended. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

RIVERTON-BELVIDERE TOLL-SUPPORTED BRIDGE FACILITIES AND GROUNDS

The New Jersey officer shelter is in overall poor condition. The Commission should consider undertaking a study to determine whether the shelter can be rehabilitated or if it should be replaced. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The storage garage is in overall poor condition. The Commission should consider undertaking a study to repair and upgrade the condition of the roof. This work is currently scheduled to be included under Contract No. 505 Water Street Repairs.

Riverton-Belvidere Toll-Supported Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

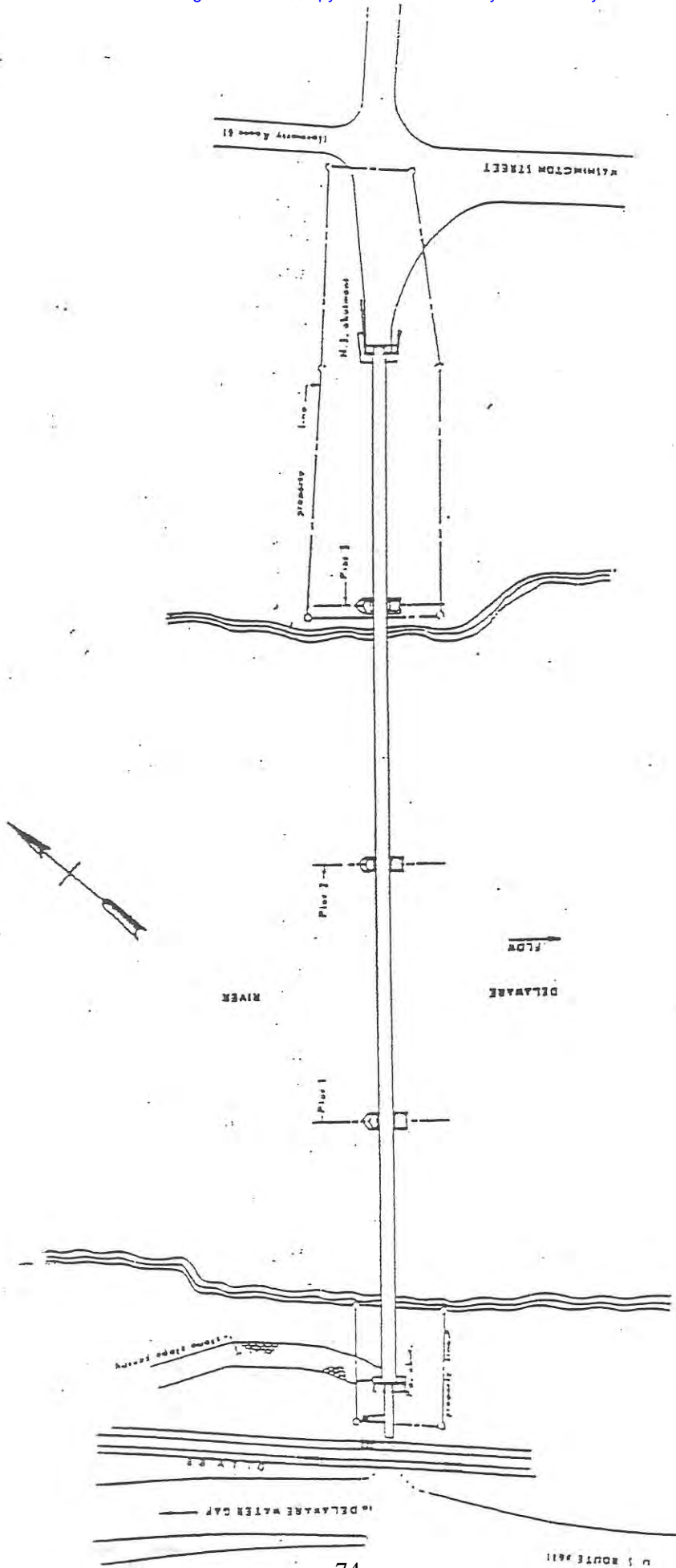
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
371	R-B TSB Rehabilitation Contract (Design / Construction)	\$9,289,000	\$35,000	\$0
505	R-B Water Street Improvements	\$1,303,000	\$76,000	\$1,227,000
BRIDGES SUB TOTAL		<i>\$10,592,000</i>	\$111,000	\$1,227,000
<u>Facilities and Grounds</u>				
RBTSB	Miscellaneous Projects (less than \$100k each)	\$5,000	\$5,000	\$6,000
FACILITIES AND GROUNDS SUB TOTAL		<i>\$5,000</i>	\$5,000	\$6,000
TOTAL COST		<i>\$10,597,000</i>	\$116,000	\$1,233,000

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PORTLAND-COLUMBIA
TOLL-SUPPORTED BRIDGE
(Structure No. 360)

STATE OF NEW JERSEY
COUNTY OF WARREN
TOWN OF COLUMBIA

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF NORTHAMPTON
BOROUGH OF PORTLAND



PORTLAND - COLUMBIA TOLL SUPPORTED BRIDGE

GENERAL

PORTLAND-COLUMBIA TOLL-SUPPORTED BRIDGE

(4 span, continuous, steel thru-deck girder)

The Portland-Columbia Toll-Supported Bridge (Structure No. 360) connects Portland Borough (Upper Mount Bethel Township), Pennsylvania with Columbia (Knowlton Township), New Jersey, just north of the Portland-Columbia Toll Bridge.

This Pedestrian Bridge is a four span continuous, thru-deck steel girder system, with a concrete deck and built up girders with a total length of 770 feet. The width of the walkway is 9 feet, 6 inches between girder centers. The present bridge was reconstructed in 1958, following the flood of 1955, and original vehicular traffic was diverted to the main river bridge.

This bridge was last cleaned and painted in 1998 under Contract 346. In 2003, the construction of a handicap accessible ramp at the west approach and bridge deck modifications was completed under Contract No. TS-388. In 2004, drainage and deck modifications were done under Contract No. TS-388A to alleviate ponding of water and corrosion due to improper drainage.

SIGNIFICANT FINDINGS

PORTLAND-COLUMBIA TOLL-SUPPORTED BRIDGE

(4 span, continuous, steel thru-deck girder)

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall poor condition due to the substructure.

The deck is in satisfactory condition. The top of deck exhibits light to moderate scaling throughout with fine to medium transverse cracks. Several incipient spalls and spalls with exposed rebar are present at the deck underside. The underside of deck also exhibits fine to medium transverse cracks with efflorescence and water stains.

The approach walkways and superstructure are in good condition.

The substructure is in poor condition. The north retaining wall is fractured adjacent to the west abutment breastwall and is displaced 2" towards the east. The top of the northeast retaining wall is displaced 8" towards the west. The east abutment breastwall exhibits spalled and hollow sounding concrete along the base. The east abutment backwall exhibits spalled and hollow sounding concrete patches with medium mapcracking at several locations. Fine to wide cracks are typical throughout the substructure units.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure units below the waterline were found to be in good condition with only hairline cracks and minor undermining at the pier aprons.

CONCLUSIONS

PORTLAND-COLUMBIA TOLL-SUPPORTED BRIDGE

Based on the findings of the 2008 inspections, the bridge is capable of safely supporting all legal loads.

The structure is in overall poor condition due to the substructure. Unsound concrete should be removed, exposed rebar should be cleaned and areas of incipient spalling throughout the underdeck should be patched. Undermined areas at all the piers should be repaired and riprap should be installed around all the piers. Broken areas of stone at the southeast corner of Pier 2 and cracked areas in the aprons at all the piers should be repaired. Riprap should be installed along the northwest wingwall at the east abutment drainage outfall. For a list of maintenance repair items, see the *Twelfth Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

Portland-Columbia Pedestrian Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
412A-10	Portland - Columbia Pedestrian Bridge, PA Approach Vehicle Access	\$95,000	\$60,000	\$0
BRIDGES SUB TOTAL		\$95,000	\$60,000	\$0
<u>Facilities and Grounds</u>				
PCTSB	Miscellaneous Projects (less than \$100k each)	\$149,000	\$10,000	\$11,000
FACILITIES AND GROUNDS SUB TOTAL		\$149,000	\$10,000	\$11,000
TOTAL COST		\$244,000	\$70,000	\$11,000

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TRENTON-MORRISVILLE

TOLL BRIDGE FACILITY

(Structure No. 20)

GENERAL

TRENTON-MORRISVILLE TOLL BRIDGE

(12 span, simply supported, composite steel multi-girder)

The Trenton-Morrisville Toll Bridge (Structure No. 20) carries US Route 1 over the Delaware River between Trenton, New Jersey and Morrisville, Pennsylvania. The main bridge is a twelve span, simply supported, composite steel girder structure with an overall length of 1,324 feet. The substructure consists of reinforced concrete abutments and piers with granite facing on the piers. The bridge was originally constructed by the Commission in 1952 as a four (4) lane roadway, and widened to six (6) lanes in 1965 for a total roadway width of 62 feet. In 1983 an aluminum barrier was erected across the bridge, creating three southbound and two northbound lanes. The posted speed limit in the northbound direction is 40 mph and 50 mph in the southbound direction until midspan, where the speed limit is reduced to 20 mph approach to toll plaza.

The Commission is currently investing more than \$100 million in a multi-year project for the widening and rehabilitation of the Route 1 corridor. This work includes the main river bridge and approach structures in New Jersey and Pennsylvania. The main river bridge is being widened from the piers up, to provide an extra lane in the northbound direction. The widening also includes a full deck replacement. Construction on this project began in late 2006 and is expected to be completed in late 2009.

TRENTON-MORRISVILLE TOLL BRIDGE APPROACH STRUCTURES

The New Jersey approach consists of eight approach structures. The Pennsylvania approach consists of two approach structures.

TRENTON-MORRISVILLE TOLL BRIDGE FACILITY AND GROUNDS

The one-way toll plaza, located at the Pennsylvania approach, has six toll lanes. The tollbooths are erected on concrete islands and are protected by an overhead canopy. Each lane is equipped for E-ZPass. The construction project underway includes rehabilitating the existing toll plaza. The new toll plaza will consist of five toll collection lanes, all equipped with E-ZPass, and a service tunnel for the toll collection staff.

The 2007 inspection included the accessible portions (due to construction) of the main river bridge, two approach bridges, and the facility and grounds.

SIGNIFICANT FINDINGS

TRENTON-MORRISVILLE TOLL BRIDGE

(12 span, simply supported, composite steel multi-girder)

The Trenton-Morrisville Toll Bridge is currently under construction. The toll bridge is being widened to accommodate an added lane in the northbound direction.

ROUTE 29 OVERPASS (NJ)

(3 span, prestressed concrete spread box beams)

This bridge is currently under construction. The structure is being reconstructed to accommodate an added off-ramp lane from Route 1.

RAMP C OVER NJ ROUTE 29 (NJ)

(3 span, steel multi-girder)

This bridge was constructed in 2008 and is scheduled to be inspected in 2009.

RAMP N OVERPASS (NJ)

(1 span, steel mutli-girder)

This bridge is currently under construction. The structure is being widened to accommodate an added lane in the northbound direction.

RAMP IY OVERPASS (NJ)

(3 span, steel multi-girder)

This bridge is currently under construction for replacement of the deck.

RAMP Y OVERPASS (LONG RAMP) (NJ)

(4 span, steel multi-girder)

The structure is in overall good condition. The bridge will be cleaned and painted and the barrier parapets will be replaced as part of Contract No. T-380B.

UNION STREET OVERPASS (NJ)

(1 span, steel multi-girder)

This bridge is currently under construction. The southbound structure is being widened to accommodate an added lane in the northbound direction. The entire deck is being replaced as part of Contract No. T-380B.

CENTER STREET UNDERPASS (NJ)

(1 span, riveted steel plate girders)

The structure is in overall good condition.

BROAD STREET UNDERPASS (NJ)

(1 span, steel multi-girder)

This bridge is currently under construction. Approach roadway work and cleaning and painting of the superstructure is currently underway.

RAMP N OVER UNION STREET (NJ)

(3 span, prestressed concrete girders)

The structure is in overall good condition.

WASHINGTON STREET OVERPASS (PA)

(1 span, steel multi-girder)

This bridge is currently under construction. Deck and approach roadway work on the northbound lanes is currently underway.

SOUTH PENNSYLVANIA AVENUE OVERPASS (PA)

(1 span steel multi-girder)

The structure is in overall good condition.

TRENTON-MORRISVILLE TOLL BRIDGE FACILITY AND GROUNDS

There is on going construction at the toll plaza and approaches. A new concrete tunnel was constructed under the toll plaza to provide access between the tollbooths and the administration building.

The entrance to the administration building closest to the toll plaza is closed because of ongoing construction on Route 1.

The HVAC system is not working adequately. The facility personnel have indicated that the HVAC duct cleaning has been completed.

The existing roof of the administration building consists of rubber membrane system. Repair patches were observed on the roof. Occasional roof leakage has been reported.

Contracts for an electronic surveillance system along with upgrading of the fire warning and alarm systems have been awarded.

The maintenance facility administration building roof replacement is in the planning stage.

CONCLUSIONS

TRENTON-MORRISVILLE TOLL BRIDGE

The structure is in overall good condition.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase.

ROUTE 29 OVERPASS (NJ)

The structure is in overall good condition.

RAMP N OVERPASS (NJ)

The structure is in overall satisfactory condition due to the cracks and spalls at the substructure. There are no repairs recommended at this time due to ongoing construction.

RAMP IY OVERPASS (NJ)

The structure is in overall good condition.

RAMP Y OVERPASS (LONG RAMP) (NJ)

The structure is in overall good condition.

UNION STREET OVERPASS (NJ)

The structure is in overall good condition.

CENTER STREET UNDERPASS (NJ)

The structure is in overall good condition.

BROAD STREET UNDERPASS (NJ)

The structure is in overall fair condition due to paint loss and minor pitting of the webs at the superstructure. There are no repairs recommended at this time due to the ongoing construction.

RAMP N OVER UNION STREET (NJ)

The structure is in overall good condition.

WASHINGTON STREET OVERPASS (PA)

The structure is in overall good condition.

SOUTH PENNSYLVANIA AVENUE OVERPASS (PA)

The structure is in overall good condition.

TRENTON-MORRISVILLE TOLL BRIDGE FACILITY AND GROUNDS

A study should be performed to determine the best method of upgrading the HVAC system.

The administration building elevator should be replaced to eliminate frequent breakdowns and repairs. Presently the building elevator replacement is being studied. ***Contract No. T-500A Trenton - Morrisville Administration Building Elevator Modernization was awarded in 2008 and construction will begin in early 2009.***

For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

Trenton-Morrisville Toll Bridge

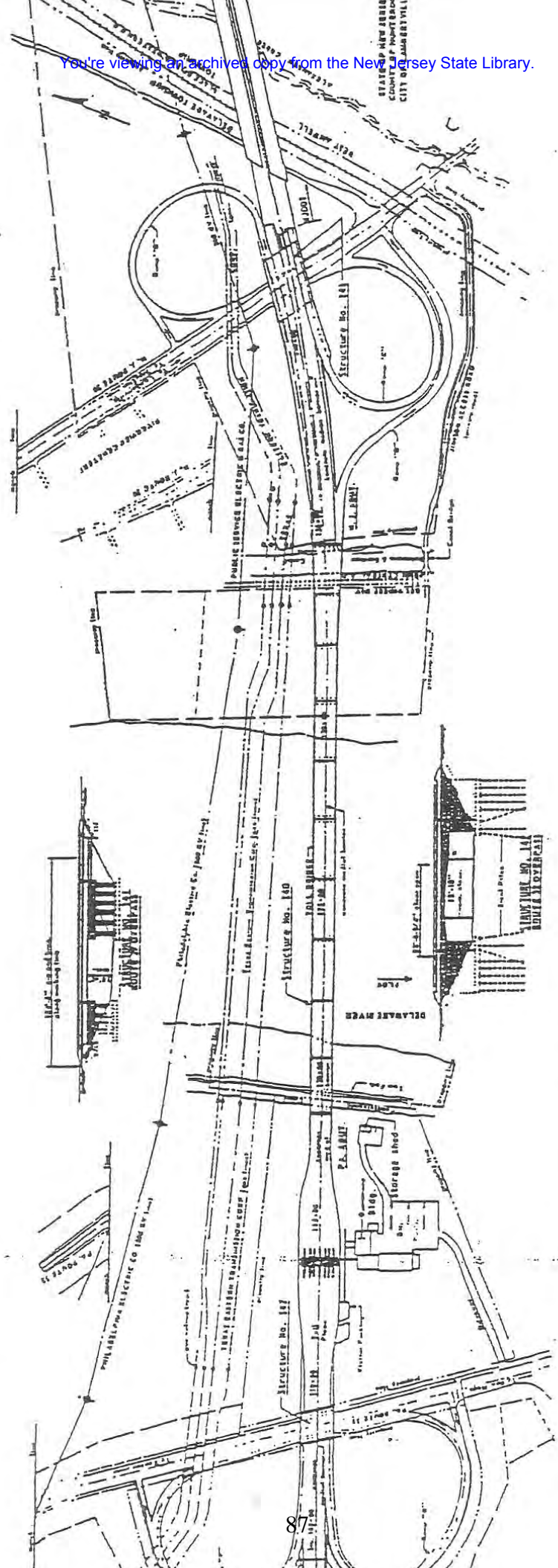
ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
380	T-M TB Rehab + One Aux. NB Lane	\$102,384,000	\$28,465,000	\$271,000
BRIDGES SUB TOTAL		\$102,384,000	\$28,465,000	\$271,000
<u>Facilities and Grounds</u>				
TMTB	Miscellaneous Projects (less than \$100k each)	\$666,000	\$50,000	\$52,000
468	TM Buildings Roof Replacement	\$731,000	\$731,000	\$0
500	TM Elevator Upgrade	\$457,000	\$381,000	\$0
519	TM Renovations (Roof, HVAC, Space)	\$3,295,000	\$213,000	\$1,698,000
FACILITIES AND GROUNDS SUB TOTAL		\$5,149,000	\$1,375,000	\$1,750,000
TOTAL COST		\$107,533,000	\$29,840,000	\$2,021,000

NEW HOPE-LAMBERTVILLE
TOLL BRIDGE FACILITY
(Structure No. 140)

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF BUCKS
TOWNSHIP OF SOLEBURY

STATE OF NEW JERSEY
COUNTY OF HUNTERDON
TOWNSHIP OF DELAWARE



PENNSYLVANIA APPROACH TO THE
NEW HOPE-LAMBERTVILLE TOLL BRIDGE

NEW JERSEY APPROACH TO THE
NEW HOPE-LAMBERTVILLE TOLL BRIDGE

NEW HOPE - LAMBERTVILLE TOLL BRIDGE

GENERAL

NEW HOPE-LAMBERTVILLE TOLL BRIDGE

(10 span, continuous, steel two girder/floorbeam/stringer)

The New Hope-Lambertville Bridge (Structure No. 140) was opened to traffic on July 22, 1971 and carries US Route 202 over the Delaware River between Lambertville, New Jersey and New Hope, Pennsylvania. The bridge is a ten span, continuous, steel two girder fracture critical structure. The deck is reinforced concrete and carries two lanes of traffic in each direction separated by a median barrier. The substructure units are composed of reinforced concrete with stone facing. The total length of the structure is 1,682 feet measured from center to center of bearings.

NEW HOPE-LAMBERTVILLE APPROACH BRIDGES

The Commission's jurisdiction also includes the loop-ramp interchanges with overpasses provided at Route 29 in New Jersey and Route 32 in Pennsylvania. The posted speed limit on the approach roadways is 55 mph.

NEW HOPE-LAMBERTVILLE FACILITY AND GROUNDS

The toll plaza and toll booths at the Pennsylvania approach have one-way toll collection, replacing the two-way collection prior to the reconstruction. All lanes are equipped with E-ZPass. The toll plaza is erected on concrete islands and is protected with an overhead canopy that matches the Operations building roof.

The 2007 inspection included the main river bridge, two approach bridges, and the facility and grounds.

SIGNIFICANT FINDINGS

NEW HOPE-LAMBERTVILLE TOLL BRIDGE

(10 span, continuous, steel two girder/floorbeam/stringer)

This structure has been classified as structurally deficient per the FHWA system due to deficiencies found in the cantilever brackets. The condition is being addressed under Task Order Assignment No. T-498A to improve the overall condition and remove the structurally deficient classification of the structure.

Design and post design services for the improvements to the cantilever brackets at the New Hope Lambertville Toll Bridge are being performed under Task Order Assignment No. C-449B-8.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

The deck, approach roadways, and substructure are in good condition.

Interim inspections of the superstructure should be performed on a 3 month interval to monitor the cracks at the cantilever brackets throughout the structure. The tie plates at the cantilever bracket are bent upwards due to pack rust between the tie plate and the top flange of the girders. Task Order Assignment No. C-449B-4 is addressing this condition with an in-depth inspection of the cantilever brackets with a permanent repair to follow. Several stringers exhibited arrested areas of material loss to the web and bottom flange. Small holes were noted at a few stringer webs. The Stringer 11 connection to the floorbeam 3 north cantilever bracket top flange in Span 9 exhibits two (2) of four (4) anchor bolts sheared off. The south fascia stringer in Span 4 exhibits a longitudinal crack at the base of the web which has been arrested by a ½” diameter drilled hole. The web at this location exhibits ¼” localized buckling.

An underwater inspection was performed in 2006 by under Contract No. C-467D. The substructure was found to be in good condition with only minor exposure of the pier footings.

ROUTE 29 OVERPASS

(3 span, simply supported, steel multi-stringer)

The structure is in overall satisfactory condition.

The deck is in good condition. There are several areas of loose portions of the wabo-flex deck joint at the northbound and southbound roadways.

The approach roadway is in satisfactory condition. The approach slabs exhibit several fine to medium cracks throughout.

The superstructure is in good condition.

The substructure is in satisfactory condition. Several large areas of hollow concrete are noted at the east abutment breastwall and the pier caps and columns. Pier 2 exhibits a large spall with exposed reinforcement and an adjacent hollow concrete area at the north end of the cap.

ROUTE 32 OVERPASS

(1 span, reinforced concrete rigid frame)

The structure is in overall satisfactory condition.

The roadway is in good condition.

The approach roadway is in satisfactory condition. The approach roadway slabs exhibit few medium to wide cracks throughout.

The superstructure is in satisfactory condition. The intrados of the rigid frame exhibits few fine to medium cracks with efflorescence at the north and south ends of the midspan. Incipient spalls are noted on the concrete rigid frame over the median and the northbound left lane.

The substructure is in good condition.

NEW HOPE-LAMBERTVILLE TOLL BRIDGE FACILITY AND GROUNDS

The New Hope-Lambertville tollbooths and tunnels are in new condition. The roadways at the tollbooths are in good condition. The administration building, attached garage facility, and barn sheds have new roofs. New lampposts have been installed in the parking lots.

An extension to the existing administration building should be under construction in late fall of 2007 and completed by spring of 2008. Also, 29 additional parking spaces will be constructed adjacent to the existing parking lot.

CONCLUSIONS

NEW HOPE-LAMBERTVILLE TOLL BRIDGE

The structure is in overall poor condition due to the superstructure. Due to the cracks noted in the steel superstructure, interim inspections are recommended to be performed on a three (3) month basis. These inspections should include all cantilever brackets on the bridge. Priority repairs to arrest the noted cracks should be undertaken within the next 6-12 months. This recommendation has been addressed through Task Order Assignment No. C-449B-4. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

ROUTE 29 OVERPASS

The structure is in overall satisfactory condition. The deck joints are deteriorated throughout the structure and the portions of the deck joints that are either loose or missing should be repaired. There are several areas of spalls with exposed reinforcement and hollow concrete areas throughout the substructure that should be patched with concrete. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

ROUTE 32 OVERPASS

The structure is in overall satisfactory condition. The concrete rigid frame exhibited areas of incipient spalls over the median and the northbound left lane. The concrete at these areas should be removed, the exposed reinforcement cleaned, and the area epoxy coated. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

NEW HOPE-LAMBERTVILLE TOLL BRIDGE FACILITY AND GROUNDS

A HVAC study was included with the administration building extension to be built in the near future. Contract No. T-397B will include upgrades to the HVAC system. Contract No. T-397B will also include a back-up generator to supply all power needs of the facility. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

Contract No. T-397B: New Hope - Lambertville Toll Bridge Building Renovations & Addition was substantially completed in October 2008 and rededication of the building was held in December 2008.

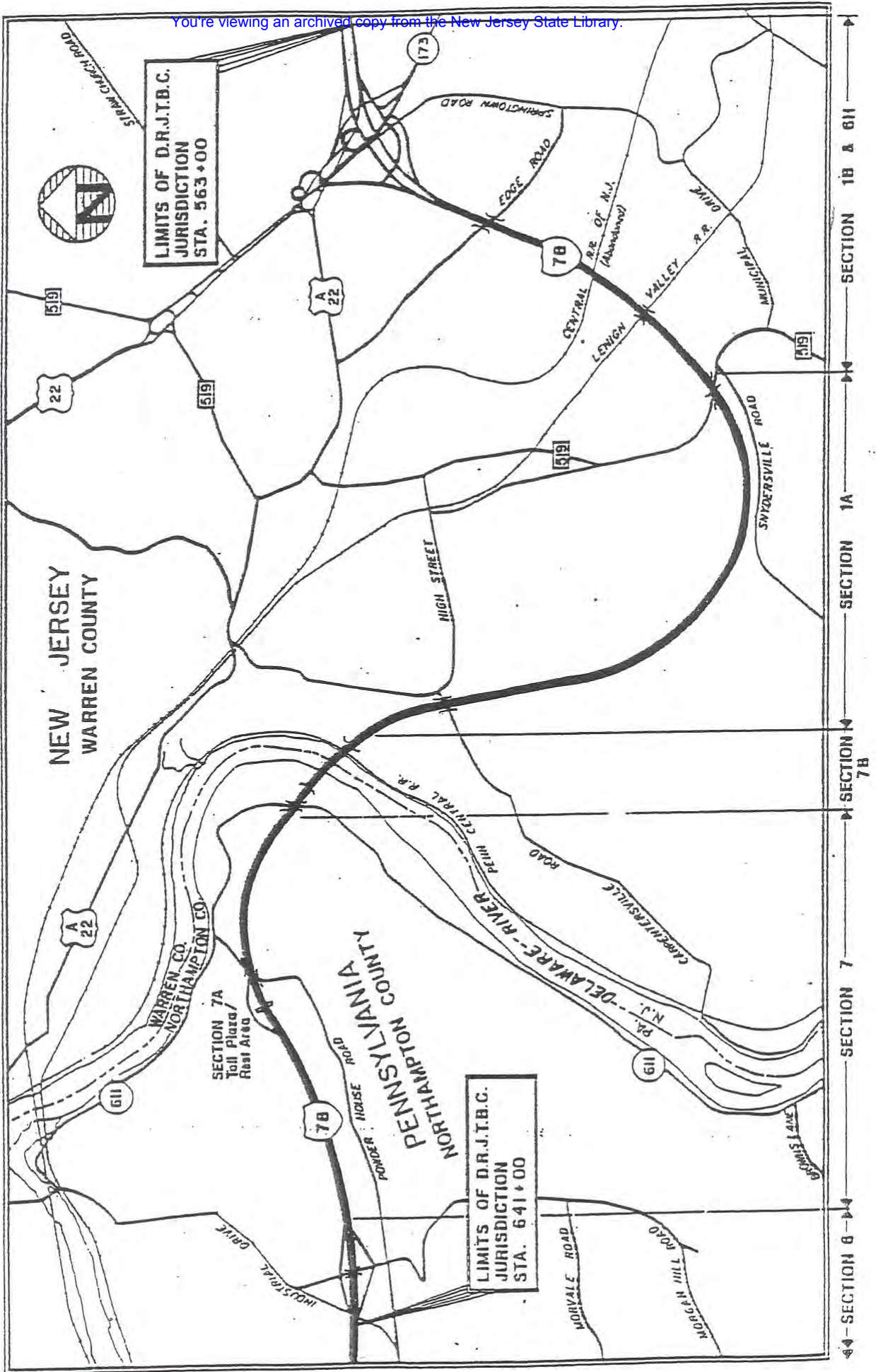
New Hope Lambertville Toll Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
498	NH-L TB - Floorbeam Bracket Improvements	\$5,671,000	\$5,266,000	\$0
BRIDGES SUB TOTAL		\$5,671,000	\$5,266,000	\$0
<u>Facilities and Grounds</u>				
NHLTB	Miscellaneous Projects (less than \$100k each)	\$419,000	\$60,000	\$32,000
397	NH-L Additions & Renovations	\$5,951,000	\$190,000	\$0
521	NH-L TB Equipment and Salt Storage Building Replacement	\$934,000	\$48,000	\$467,000
FACILITIES AND GROUNDS SUB TOTAL		\$7,304,000	\$298,000	\$499,000
TOTAL COST		\$12,975,000	\$5,564,000	\$499,000

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INTERSTATE 78
TOLL BRIDGE FACILITY
(Structure Nos. 270 & 275)



INTERSTATE 78 TOLL BRIDGE

GENERAL

INTERSTATE 78 TOLL BRIDGE

(Twin 7 span, continuous, steel multi-girder)

The Interstate 78 toll bridge carries traffic over the Delaware River between Northampton County, Pennsylvania and Warren County, New Jersey. The facility was opened to traffic on November 21, 1989. The Interstate 78 main river bridge (Structure nos. 270 & 275) is a twin, 1,222 foot long, four girder, 7 span continuous bridge. The dual roadways are each 46 feet from curb to curb and carry three lanes of traffic. The substructure consists of reinforced concrete hammerhead piers and reinforced concrete stub abutments. The posted speed limit on the bridge is 55 mph.

INTERSTATE 78 APPROACH BRIDGES

The New Jersey approach consists of six approach structures. The Pennsylvania approach consists of five approach structures. In total there are eleven (11) approach structures owned and maintained by the Commission that are part of the Interstate 78 Toll Bridge Facility.

INTERSTATE 78 ROADWAY

The Commission's jurisdiction extends approximately 2.2 miles to the west at the Pennsylvania approach and includes five (5) bridges and a Welcome Center. The New Jersey approach extends approximately 4.7 miles to the east (not including Conrail over I-78 or the Route 173 structures) from the main river bridge and includes six (6) bridges.

INTERSTATE 78 TOLL BRIDGE FACILITY AND GROUNDS

The one-way toll plaza, located at the Pennsylvania approach of the westbound lanes, has seven toll lanes. All toll booth are erected on concrete islands and are protected by an overhead canopy. All lanes are equipped with E-ZPass.

The 2007 inspection included the eastbound and westbound main river bridges, eleven (11) approach structures, and the facility and grounds.

SIGNIFICANT FINDINGS

INTERSTATE 78 TOLL BRIDGE (WESTBOUND)

(7 span, continuous, steel multi-girder)

The structure is in overall good condition.

The deck, superstructure, substructure are in good condition.

The approach roadway is in satisfactory condition. Few medium to wide transverse cracks were noted at the approach roadways. The hot pour sealer at the abutment header is slightly deteriorated and depressed.

INTERSTATE 78 TOLL BRIDGE (EASTBOUND)

(7 span, continuous, steel multi-girder)

The structure is in overall good condition.

The deck is in satisfactory condition. The top of deck exhibits numerous fine to medium transverse cracks throughout. The SIP forms at the underside of the deck have isolated areas of spot rust and the concrete overhangs exhibit few fine cracks with efflorescence.

The approach roadway is in satisfactory condition. Few medium to wide transverse cracks were noted at the approach roadways. The east approach roadway exhibits a spall between the right and center lanes.

The superstructure and substructure are in good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructures for the eastbound and westbound roadways were found to be in good condition with only minor deficiencies noted.

MORGAN HILL ROAD OVERPASS

(2 span, simply supported, prestressed concrete spread box beams)

The structure is in overall good condition.

The deck is in satisfactory condition. The top of deck exhibits fine to medium cracks, some partially sealed, throughout. The compression seal deck joints are partially covered with hot poured sealer and exhibit deterioration where visible.

The approach roadway is in satisfactory condition. Medium to wide cracks were noted throughout both approach roadways. The approach shoulder pavement exhibits heavy scaling and potholes at the east and west shoulders of both approach roadways.

The superstructure and substructure are in good condition.

CEDARVILLE ROAD OVERPASS

(4 span, simply supported, prestressed concrete I-beams)

The structure is in overall good condition. The deck, approach roadways, superstructure, and substructure are all in good condition.

I-78 WESTBOUND OVER ROUTE 611

(3 span, simply supported, prestressed concrete spread box beams)

The structure is in overall good condition.

The deck, superstructure, and substructure are in good condition.

The approach roadways are in satisfactory condition. The west approach roadway exhibits medium to wide cracks. The east approach roadway has few spalls partially patched with asphalt.

I-78 EASTBOUND OVER ROUTE 611

(3 span, simply supported, prestressed concrete spread box beams)

The structure is in overall good condition.

The deck, superstructure, and substructure are in good condition.

The approach roadways are in satisfactory condition. The west approach roadway exhibits medium to wide cracks. The east approach roadway has few spalls partially patched with asphalt and few wide cracks.

CARPENTERSVILLE ROAD OVERPASS

(2 span, continuous, steel multi-stringer)

The structure is in overall good condition. The deck, approach roadways, superstructure, and substructure are all in good condition.

EDGE ROAD OVERPASS

(2 span, continuous, steel multi-stringer)

The structure is in overall satisfactory condition.

The deck, approach roadways, and substructure are in good condition.

The superstructure is in satisfactory condition. The bottom flanges exhibit light to moderate rust and the remaining portion of the superstructure and bearings exhibit light surface rust.

I-78 WESTBOUND OVER ROUTE 519

(2 span, continuous, steel multi-stringer)

The structure is in overall good condition. The deck, approach roadways, superstructure, and substructure are all in good condition.

I-78 EASTBOUND OVER ROUTE 519

(2 span, continuous, steel multi-stringer)

The structure is in overall good condition.

The deck is in satisfactory condition. The compression seal deck joints are partially covered with hot poured sealer and exhibit areas of minor to moderate settlement.

The approach roadways are in satisfactory condition. The west approach roadway exhibits few partially sealed wide cracks.

The superstructure and substructure are in good condition.

I-78 WESTBOUND OVER RAMP C

(1 span, simply supported, steel multi-stringer)

The structure is in overall good condition.

The deck, superstructure, and substructure are in good condition.

The approach roadways are in satisfactory condition. The west approach roadways exhibits spalls and wide cracks. There are spalls at the approach slabs between the lanes due to missing and broken lane reflectors.

I-78 EASTBOUND OVER RAMP C

(1 span, simply supported, steel multi-stringer)

The structure is in overall good condition.

The deck, superstructure, and substructure are in good condition.

The approach roadways are in satisfactory condition. The approach roadways exhibit wide cracks throughout.

SERVICE ROAD OVERPASS

(1 span, simply supported, prestressed concrete adjacent box beams)

The structure is in overall good condition. The deck, approach roadways, superstructure, and substructure are all in good condition.

INTERSTATE 78 ROADWAY

The I-78 roadway in New Jersey is comprised of concrete slabs. These slabs have many severe transverse cracks throughout the slabs. The concrete approach roadways have many settled and uneven slab sections with spalled edges along joints. Many joints between slabs have spalled and have been filled with asphalt. An Interstate 78 Roadway Rehabilitation Contract is underway (Contract No. T-424A) and is anticipated to be completed in 2009.

INTERSTATE 78 TOLL BRIDGE FACILITY AND GROUNDS

Permanent impact attenuators (protective crash cushions) should be considered for installation at the islands for increased protection to the traveling public and Commission employees.

Some of the I-78 facility vehicles and equipment are not protected from the weather and are stored along parking lots because of a lack of storage capacity within the building.

CONCLUSIONS

INTERSTATE 78 TOLL BRIDGE (WESTBOUND)

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

INTERSTATE 78 TOLL BRIDGE (EASTBOUND)

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

MORGAN HILL ROAD OVERPASS

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

CEDARVILLE ROAD OVERPASS

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

I-78 WESTBOUND OVER ROUTE 611

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

I-78 EASTBOUND OVER ROUTE 611

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

CARPENTERSVILLE ROAD OVERPASS

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

EDGE ROAD OVERPASS

The structure is in overall satisfactory condition. The guide rail at the east parapet of the south approach exhibits a sheared anchor bolt and should be replaced. The superstructure steel and bearings should be painted. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

I-78 WESTBOUND OVER ROUTE 519

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

I-78 EASTBOUND OVER ROUTE 519

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

I-78 WESTBOUND OVER RAMP C

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

I-78 EASTBOUND OVER RAMP C

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

SERVICE ROAD OVERPASS

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

INTERSTATE 78 ROADWAY

The I-78 roadway has excessive slab cracking and settlement for the majority of the Commission owned portion of I-78 (especially the NJ portion). Presently, Contract No. T-424 is underway for the I-78 Roadway Rehabilitation in New Jersey.

INTERSTATE 78 TOLL BRIDGE FACILITY AND GROUNDS

A study should be conducted to determine the need for additional vehicle and equipment storage at the I-78 facility.

A study of the HVAC system should be conducted to determine whether the system located in the maintenance facility needs to be upgraded.

Consideration should be given to the installation of permanent impact attenuators at the toll plaza.

For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

Interstate 78 Toll Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
424	I-78 Roadway Rehabilitation	\$57,624,000	\$27,729,000	\$0
427B	I-78 Open Road Tolling (ORT) Lanes	\$15,928,000	\$4,362,000	\$8,220,000
BRIDGES SUB TOTAL		\$73,552,000	\$32,091,000	\$8,220,000
<u>Facilities and Grounds</u>				
I-78TB	Miscellaneous Projects (less than \$100k each)	\$649,000	\$50,000	\$52,000
507	I-78 HVAC Upgrade	\$698,000	\$0	\$78,000
508	I-78 Vehicle Storage Building	\$3,108,000	\$0	\$189,000
506	I-78 Welcome Center Parking Lot Pavement Improvements	\$1,004,000	\$0	\$17,000
FACILITIES AND GROUNDS SUB TOTAL		\$5,459,000	\$50,000	\$336,000
TOTAL COST		\$79,011,000	\$32,141,000	\$8,556,000

EASTON-PHILLIPSBURG
TOLL BRIDGE FACILITY
(Structure No. 300)

GENERAL

EASTON-PHILLIPSBURG TOLL BRIDGE

(1 span, Petit Thru-Truss)

The Easton-Phillipsburg Toll Bridge (Structure No. 300) carries US Route 22 over the Delaware River. The bridge was opened to traffic on January 14, 1938. Westbound only toll collection commenced on June 4, 1989. The main river bridge consists of a 540 foot Petit thru-truss span over the Delaware River. The overall length, including the approaches on either end of the structure, is approximately 1,010 feet. The roadway width is 40 feet between the trusses and carries 4 lanes of traffic. There are 8 foot sidewalks cantilevered outside of both trusses. The substructure consists of reinforced concrete abutments. The posted speed limit through the toll bridge facility is 25 mph.

Sidewalk reconstruction was performed under Contract No. T-420 and was completed in 2004.

The underside of the Easton-Phillipsburg Toll Bridge, which includes the roadway stringers, floorbeams, and the bottom chords of the trusses, received an in-depth inspection in April 2007. This special in-depth inspection was required due to the limited access to those members for the regular inspections. The underside components were found to be in overall satisfactory condition. All major areas of section loss at the floorbeams and lateral bracing was found below the curblines due to poor drainage.

EASTON-PHILLIPSBURG TOLL BRIDGE APPROACH STRUCTURES

The Commission's jurisdiction includes a total of five (5) approach structures. On the Pennsylvania approach there are four approach structures.

Approximately 2,000 feet of the Pennsylvania approach was reconstructed in 1982. This reconstruction included new superstructures for the overpasses at Bank Street, Third Street, and Route 611. The truss support for the center bearing of the Broad Street Viaduct was reconstructed in 2001.

EASTON-PHILLIPSBURG TOLL BRIDGE FACILITY AND GROUNDS

The one-way toll plaza, located at the New Jersey approach, has five toll lanes. All tollbooths are erected on concrete islands and are protected by an overhead canopy. All lanes are equipped for E-ZPass.

The 2007 inspection included the main river bridge, five (5) approach bridges, and the facility and grounds.

SIGNIFICANT FINDINGS

EASTON-PHILLIPSBURG TOLL BRIDGE

(1 span, Petit Thru-Truss)

The structure is in overall satisfactory condition.

The deck is in satisfactory condition. Numerous medium to wide transverse cracks are noted throughout the bridge, mainly over the floorbeam locations. The pedestrian railing exhibited cracks at the base of the several posts.

There are no approach roadways for this structure due to the adjacent approach structures.

The superstructure is in satisfactory condition. Several members exhibit isolated areas of light to moderate surface rust and peeling paint. Pack rust was noted at several locations between eyebars and at gusset plate connections. Few access cover plates at the vertical truss members are welded and few welds are cracked. The underside inspection performed by Schoor DePalma, Inc. in April 2007 noted minor section loss to the floorbeams and lateral bracing.

The substructure is in good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure was noted to be in good condition. No major deficiencies were noted at either abutment in the underwater inspection report.

BROAD STREET VIADUCT

(5 span, simply supported, riveted steel three girder-floorbeam-stringer system)

The structure is in overall fair condition.

The deck is in satisfactory condition. Fine to medium transverse cracks are noted throughout the top of deck. Several areas of the underside steel trough and sidewalk SIP forms exhibit heavy laminar rust. Cracks are noted at the base plates of the north and south bridge pedestrian railing posts.

The approach roadway (east only) is in satisfactory condition. Medium to wide cracks are noted in the asphalt. The eastbound and westbound lane exhibited small spalls and loose concrete.

The superstructure is in fair condition. Several structural steel members exhibit areas of moderate to severe corrosion below the deck joints, along the curb openings, and those exposed directly to the elements. Severe rust was noted at the end stringers and floorbeam under the deck joint at Pier 4 with up to 50% material loss to the stringer connection bolts. Stringers 2 and 4 (from the north) deflect up to 1/4" at the connection to the floorbeam at Pier 4 due to the losses at the connection bolts. Repaired cracks were noted at Piers 1 to 3 at the floorbeam-kneebrace connections. The weld repair at the vertical connection to the Span 3 south girder at Pier 3 has cracked and is 21" long. The crack extends approximately 1/2" beyond the weld repair area.

The substructure is in good condition.

ROUTE 611 OVERPASS

(1 span, simply supported, prestressed concrete adjacent box beam)

The structure is in overall satisfactory condition.

The deck is in fair condition. The top of deck exhibits large areas of deteriorated asphalt patches and concrete areas. The compression seal deck joints at the east and west abutments are depressed, torn, and missing throughout. The parapets have few small spalls and incipient spalls throughout.

The approach roadway (west only) is in good condition.

The superstructure is in satisfactory condition. The prestressed box beams exhibit few small spalls and moderate water stains throughout.

The substructure is in satisfactory condition. The abutments have few medium to wide cracks throughout.

THIRD STREET OVERPASS

(1 span, simply supported, steel multi-stringer)

The structure is in overall good condition. The deck, approach roadways, superstructure, and substructure are all in good condition.

BANK STREET OVERPASS

(3 span, continuous, steel multi-stringer)

The structure is in overall good condition. The deck, approach roadways, superstructure, and substructure are all in good condition.

The inlet at the northwest corner of Bank Street under Span 2 has settled with erosion of the roadway slab subbase material adjacent to the inlet. The concrete sidewalls of the inlet have also spalled with several areas of missing and broken concrete.

PEDESTRIAN TUNNEL

(Single cell, reinforced concrete box culvert)

The structure is in overall good condition. The roadway and culvert are in good condition.

EASTON-PHILLIPSBURG TOLL BRIDGE FACILITY AND GROUNDS

The west side of the toll plaza has several concrete slabs of roadway with a few open and wide transverse cracks. The roadway surface is uneven with wear along tire lines and minor settlement of concrete slabs. During heavy rain, there are areas with ponding water and the tunnel under the toll booth exhibits minor leakage and occasionally the carpets on tunnel floor over the drains become wet. Overall the toll plaza is in fair condition.

Localized failure of steep embankments located at east and south sides of the maintenance yard, adjacent to the Broad Street ramp, were previously noted. Eroded embankment was observed at the base of the slope. These areas appear to be stable at the time of this inspection.

The current diesel fuel storage tank used by this facility has a 250 gallons capacity and it is inadequate for current needs. The fuel is dispensed utilizing a hand pump. The current underground diesel storage tank should be replaced with an above ground tank.

The circulating hot water heating system in the administration building is not functioning adequately and it needs to be flushed cleaned. Maintenance forces at the facility indicated that they will flush this heating system.

The tiles in the 1st floor hallway, 2nd floor hallway, and elevator of the administration building should be replaced due to the uneven walking surface. The existing tile and mastic material may contain asbestos. The Commission should have a qualified consultant or contractor test for asbestos containing material. If asbestos exists the Commission should let a contract to properly dispose of the material and replace the tile.

The roof on the administration building and garage was replaced in 2007 under Contract No.T-465A.

The City of Easton recently informed the Commission that a storm drainage line running in close proximity to the northern foundation of Sign Structure A is partially blocked by concrete. The storm drainage line in question is an 18" concrete line. The blockage's location is in line with Sign Structure A's northern foundation. ***This work was completed under C-T424A in 2008.***

CONCLUSIONS

EASTON-PHILLIPSBURG TOLL BRIDGE

The structure is in overall satisfactory condition. The general condition of the paint system of the above-deck truss is fair. Consideration should be given for a major rehabilitation project for the toll bridge and the approach structures. The rehabilitation project should include cleaning and painting of the superstructure, miscellaneous steel repairs, and drainage improvements. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

BROAD STREET VIADUCT

The structure is in overall fair condition. The cracked vertical connection angles between the south girder and east end floorbeam at Pier 3 and between Stringer 3 and the floorbeam at Pier 4 should be replaced during a future rehabilitation project, while arresting the cracks should be included in a future miscellaneous repair contract. All the floorbeam ends and gusset plates should be cleaned and spot painted. The cracks at the east abutment backwall and breastwall

should be sealed. The cracked and hollow areas at the east abutment backwall and northeast wingwall should be removed and patched with concrete. The repaired cracked welds at the connection angles throughout the structure and the Span 5 stringer connections at Pier 4 should be monitored. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

ROUTE 611 OVERPASS

The structure is in overall satisfactory condition. The top of deck exhibits large areas of deteriorated asphalt patches and concrete areas which should be removed and repaired with concrete. The compression seal deck joints at the east and west abutments should be replaced. Rebuild the settled and cracked south sidewalk at the west approach. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

THIRD STREET OVERPASS

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

BANK STREET OVERPASS

The structure is in overall good condition. The inlet at the northwest corner of Bank Street should be repaired due to the erosion around the inlet. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

PEDESTRIAN TUNNEL

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

EASTON-PHILLIPSBURG TOLL BRIDGE FACILITY AND GROUNDS

The deteriorated and cracked concrete slabs on the west side of the toll plaza should be replaced. Several of concrete slabs on the east side of toll plaza in the westbound lanes should be replaced. The spalled curbs and deteriorated relief joint should be repaired.

A study should be performed for the replacement of the current diesel fuel storage tank.

A contract will be let to rectify the storm water blockage at Sign Structure A's northern foundation in the near future.

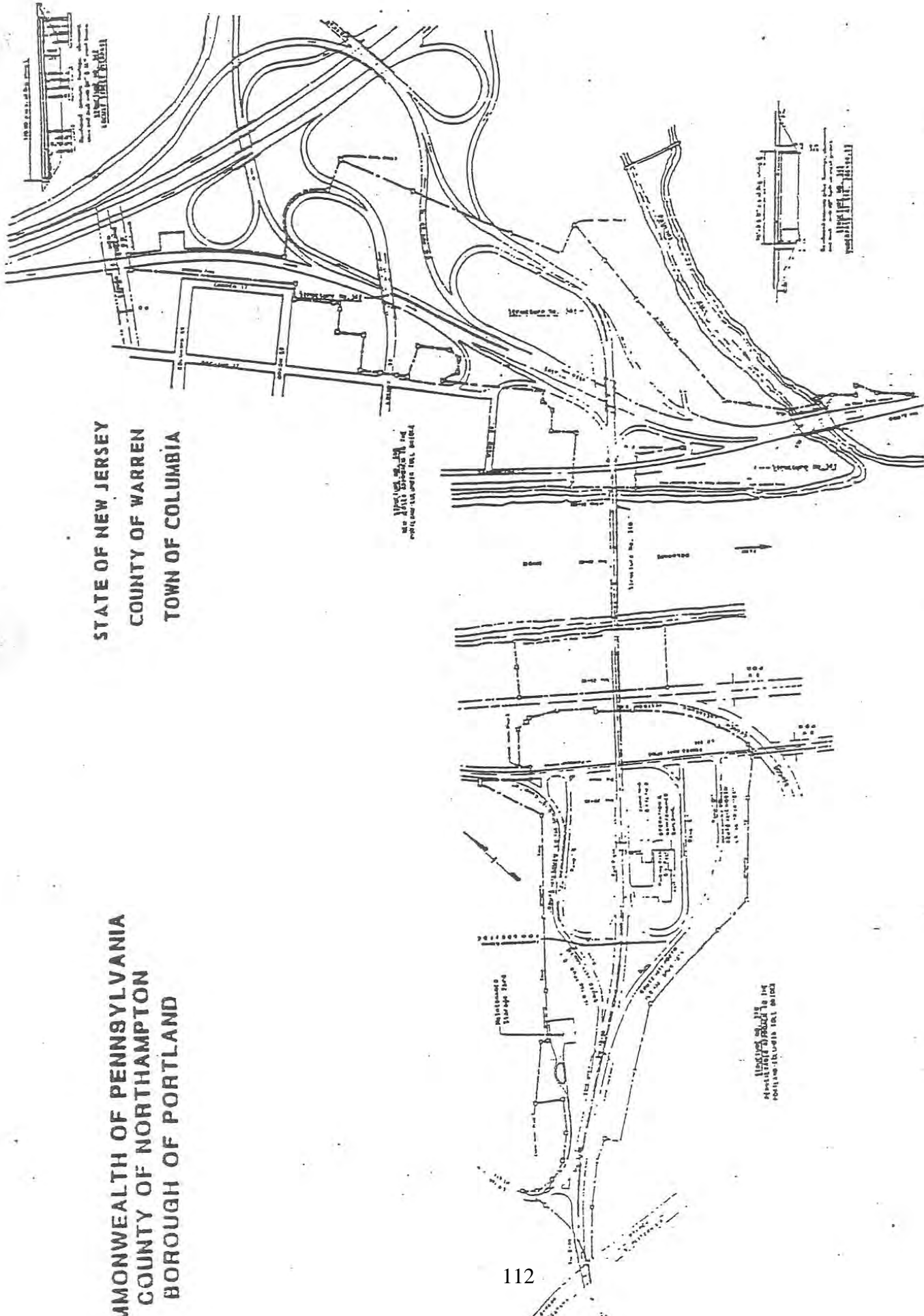
For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

Easton-Phillipsburg Toll Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
436	E-P TB Sign Struct Replacements, Repair & Signage Upgrades	\$2,779,000	\$166,000	\$0
437	E-P TB Facility Rehabilitation	\$15,842,000	\$168,000	\$787,000
BRIDGES SUB TOTAL		<i>\$18,621,000</i>	<i>\$334,000</i>	<i>\$787,000</i>
<u>Facilities and Grounds</u>				
EPTB	Miscellaneous Projects (less than \$100k each)	\$573,000	\$40,000	\$42,000
475	E-P AST Diesel Fuel Storage Tank Replacement	\$92,000	\$92,000	\$0
509	E-P HVAC Upgrade	\$581,000	\$0	\$63,000
522	E-P Elevator Modernization	\$529,000	\$107,000	\$423,000
FACILITIES AND GROUNDS SUB TOTAL		<i>\$1,775,000</i>	<i>\$239,000</i>	<i>\$528,000</i>
TOTAL COST		<i>\$20,396,000</i>	<i>\$573,000</i>	<i>\$1,315,000</i>

PORTLAND-COLUMBIA
TOLL BRIDGE FACILITY
(Structure No. 340)



STATE OF NEW JERSEY
 COUNTY OF WARREN
 TOWN OF COLUMBIA

COMMONWEALTH OF PENNSYLVANIA
 COUNTY OF NORTHAMPTON
 BOROUGH OF PORTLAND

PORTLAND - COLUMBIA TOLL BRIDGE

GENERAL

PORTLAND-COLUMBIA TOLL BRIDGE

(10 span, riveted steel multi-girder)

The Portland-Columbia Toll Bridge Facility (Structure No. 340) opened to traffic on December 1, 1953 and converted to toll collection in the westbound direction only on May 25, 1989. The bridge connects Pennsylvania Route 611 at Portland, Pennsylvania with US Route 46 at Columbia, New Jersey. US Route 46 merges with Interstate 80 located just north of the bridge on the New Jersey approach.

The main river bridge consists of a ten span, riveted steel plate girder system with an approximate total length of 1,309 feet. The roadway is 32 feet wide from curb to curb and carries one lane of traffic in each direction with a posted speed limit of 35 mph. The substructure units consist of reinforced concrete piers and concrete bin abutments. All the substructures are founded on spread footings with the exception of Pier 8, which is founded on piles. The piers also have partial granite stone facing.

A rehabilitation contract performed in 1992 included replacement of the existing concrete deck with a cast-in-place deck and concrete parapets. The combination sidewalk and maintenance walkway were removed and a new lighting system on the downstream side of the main bridge was installed. Approach roadway improvements (NJ and PA) and new drainage systems were also implemented. In 1998, the main river bridge, the pedestrian bridge to the north of the toll bridge, and both approach structures were cleaned and painted by contract.

PORTLAND-COLUMBIA APPROACH BRIDGES

The Commission's jurisdiction also includes two additional bridges at the New Jersey approach. Deck and barrier replacements were performed in 1992 in conjunction with the main river bridge rehabilitation contract.

PORTLAND-COLUMBIA TOLL BRIDGE FACILITY AND GROUNDS

The one-way toll plaza, located at the Pennsylvania approach, has three toll lanes. All the tollbooths are erected on concrete islands and are protected by an overhead canopy. All three lanes are equipped for E-ZPass.

The 2007 inspection included the main river bridge, two approach bridges, and the facility and grounds.

SIGNIFICANT FINDINGS

PORTLAND-COLUMBIA TOLL BRIDGE

(10 span, riveted steel multi-girder)

The structure is in overall good condition.

The deck is in good condition. Impact damage was noted to the cantilever sign structure connected to the north girder at Span 3. The steel support behind the sign panel is disconnected, however the sign panels are secure. The steel support at the base did not show any signs of distress or cracks. This condition is being repaired under Task Order Assignment No. C449A-2.

The approach roadway is in satisfactory condition. Large areas of fine map cracking are noted at both approaches with few medium to wide cracks. The guide rail at the north side of the east approach exhibits impact damage.

The superstructure and substructure are in good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The underwater components of the substructure were noted to be in good condition with only minor defects noted.

ROUTE 46 OVERPASS

(1 span, riveted steel multi-girder)

The structure is in overall good condition.

The deck, superstructure, and substructure are in good condition.

The approach roadway is in satisfactory condition. There is a previously patched cracked and deteriorated concrete area at the centerline of the roadway of the west approach. The east approach exhibits numerous medium to wide cracks throughout the pavement.

LOCUST STREET OVERPASS

(4 span, steel multi-stringer)

The structure is in overall satisfactory condition.

The deck, approach roadways, and superstructure are in good condition.

The substructure is in satisfactory condition. A spall was noted at the east abutment bridge seat exposing the anchor bolt of the Stringer 6 bearing with a 10 in² area of undermining of the masonry plate (approximately 10%). A 2 in² area of undermining (less than 5%) was also noted at the Stringer 1 bearing at the west abutment due to a small spall. All three piers exhibit hollow concrete areas at the pier columns and at the pier cap of Pier 1.

PORTLAND-COLUMBIA TOLL BRIDGE FACILITY AND GROUNDS

The maintenance parking lot is in poor condition with wide cracking of the asphalt pavement and unevenness throughout. The additional storage yard and driveway are in poor condition with numerous areas of deteriorated pavement. Also, the roadway drainage is poor because of spalling and cracking of the pavement.

The roof on the maintenance garage and the administration building was recently replaced under Contract No. T-439A.

The HVAC controls are approximately 20 years old and the controls are not working properly.

The paint system on the overhead sign structure over the eastbound roadway, west approach, is in poor condition with areas of rust. The reflectivity of the sign panels throughout the facility is degraded and consideration should be given to replace the panels.

The entire District 3 salt storage is maintained at this location. The existing storage capacity is not sufficient.

CONCLUSIONS

PORTLAND-COLUMBIA TOLL BRIDGE

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

ROUTE 46 OVERPASS

The structure is in overall good condition. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

LOCUST STREET OVERPASS

The structure is in overall satisfactory condition. Repair the spalls causing the minor undermining of the bearings of Stringer 6 at the east abutment and Stringer 1 at the west abutment. Remove the pack rust below the rocker bearings at Stringer 2 to 5 at the west abutment and Stringer 4 at the east abutment. Reset the shifted sliding plate bearings at all the piers. Replace the missing anchor bolts at Stringer 1 of Pier 3. The cracked and hollow concrete throughout the piers should be removed and patched with concrete. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

PORTLAND-COLUMBIA TOLL BRIDGE FACILITY AND GROUNDS

The maintenance (rear) parking lot and the salt storage yard access and turn around should be repaved. New sidewalks, curbs and drainage should be constructed. ***These improvements will be included in Contract No. 441A Locust Street Improvements.***

The sign structures should be repainted or replaced.

A study should be performed on the HVAC controls to determine what components need to be replaced, or if the entire system should be upgraded.

A study should be performed to determine the district's deicing requirements. The study should determine salt storage capacity, storage location and type of storage.

For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

Portland-Columbia Toll Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

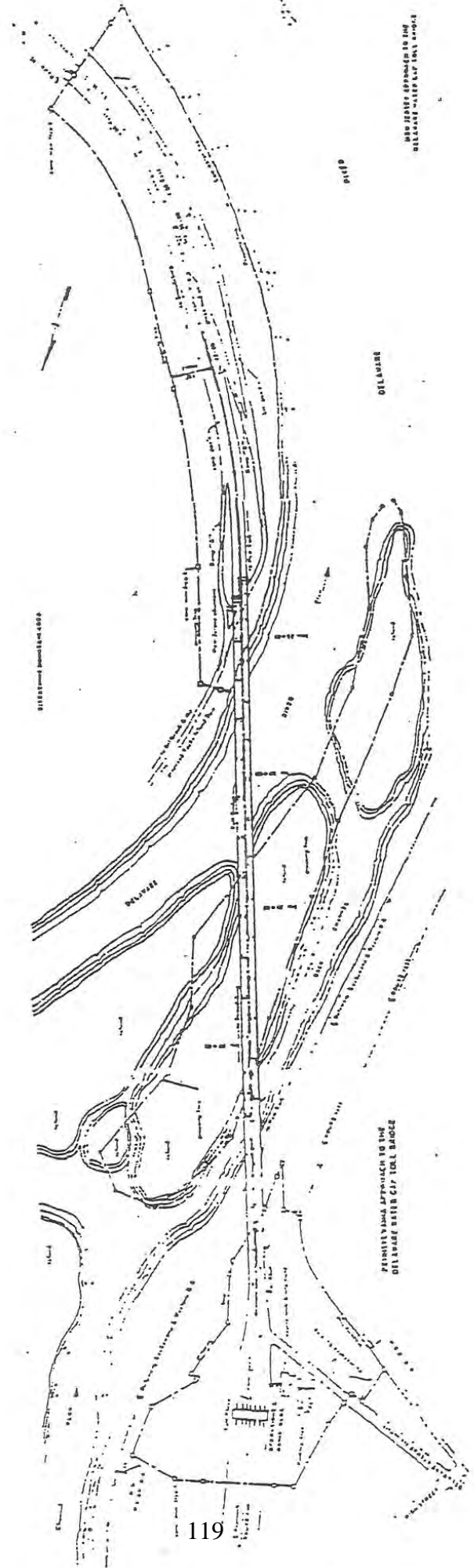
Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
441	Locust Street Bridge Imprvmts (incl. Impact Atten., Sewer & Pk Lot Repaving)	\$1,099,000	\$830,000	\$236,000
BRIDGES SUB TOTAL		\$1,099,000	\$830,000	\$236,000
<u>Facilities and Grounds</u>				
PCTB	Miscellaneous Projects (less than \$100k each)	\$263,000	\$20,000	\$21,000
460	Portland Wastewater System Connection (incorporated in 441A)	\$13,000	\$0	\$0
503	Portland - Columbia TB Sewer Force Main Conn. & Pk Lot Paving (inc. in 441A)	\$0	\$0	\$0
510	P-C Rear Parking Lot, Storage Yard and Driveway Paving (incorporated in 441A)	\$0	\$0	\$0
512	P-C HVAC Upgrade	\$580,000	\$0	\$78,000
FACILITIES AND GROUNDS SUB TOTAL		\$856,000	\$20,000	\$99,000
TOTAL COST		\$1,955,000	\$850,000	\$335,000

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***DELAWARE WATER GAP
TOLL BRIDGE FACILITY
(Structure Nos. 380 & 390)***

STATE OF NEW JERSEY
COUNTY OF WARREN
TOWNSHIP OF PAHAQUARRY

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF MONROE
BOROUGH OF DELAWARE WATER GAP



DELAWARE WATER GAP TOLL BRIDGE

GENERAL

DELAWARE WATER GAP TOLL BRIDGE

(Eastbound: 17 span, riveted steel multi-girder)

(Westbound: 16 span, riveted steel multi-girder)

The Delaware Water Gap Toll Bridge (Structure Nos. 380 and 390) carries Interstate 80 across the Delaware River near Stroudsburg, Pennsylvania, providing a gateway from the eastern metropolitan area to the Pocono recreational area. Through Pennsylvania, the four lane limited access highway crosses the width of Pennsylvania to the Ohio border and directly connects to the Ohio Turnpike. On the New Jersey side, Interstate 80 connects the Delaware Water Gap Toll Bridge to the George Washington Bridge.

The toll bridge, built by the Commission and opened on December 16, 1953, is a twin, multi-span (17 spans EB and 16 spans WB), steel riveted plate girder bridge approximately 2465 feet in total length. The dual roadways are each 28 feet wide from curb to curb, carrying two lanes of traffic each, and are separated by an aluminum barrier. A 5 foot wide sidewalk is located on the south side of the eastbound roadway, separated from the travel lanes with a concrete barrier. The substructure units consist of reinforced concrete bin abutments and piers. The piers also have partial granite stone facing. The speed limit posted at both approach roadways is 55 mph.

Major rehabilitation work was completed in 1989. The rehabilitation work included reconstruction of the toll plaza for one-way toll collection in the westbound direction (8 total lanes), deck replacement, construction of a New Jersey approach pedestrian walkway, toll plaza access tunnel, and miscellaneous pavement replacement. Other work performed under this contract included the installation of the aluminum median barrier, lighting and signage.

DELAWARE WATER GAP TOLL BRIDGE FACILITY AND GROUNDS

The one-way toll plaza, located at the Pennsylvania approach has eight (8) toll lanes. All tollbooths are erected on concrete islands and are protected by an overhead canopy.

A ½ mile section of Interstate 80 east of the bridge was resurfaced in 2007 under Contract No T-492A.

The 2007 inspection included the eastbound and westbound main river bridges and the facility and grounds.

SIGNIFICANT FINDINGS

DELAWARE WATER GAP TOLL BRIDGE (EASTBOUND)

(17 span, riveted steel multi-girder)

The structure is in overall satisfactory condition.

The deck is in satisfactory condition. The cast-in-place microsilica concrete (deck slab) roadway and sidewalk deck, installed in 1989, exhibits numerous fine to medium transverse cracks and longitudinal cracks over the stringer locations. These cracks were formed during the initial pouring procedures. Cores taken in 1989 and again in 1996 indicated that cracks to have grown to a maximum width of 1/16" at some locations, and also showed no signs of corrosion to the reinforcement. This inspection revealed minor or no rust to the stay-in-place forms at the underside of the deck and no significant changes to the cracks on the surface of the deck. The cracks noted at the top of deck do not pose a structural concern at this time. The deck joints were rebuilt during the deck replacement in 1989 and are consist of steel plates welded to the original finger joints, combined with steel angle armoring and strip seals. The "Seva" patch material, used as the joint header material, is deteriorated at numerous locations throughout. The material is settled, cracked, and spalled, exposing the steel plates and steel angle armoring below. Few deck joints in the eastbound roadway are slightly vertically offset between spans resulting in minor plow catch damage. All the deck joints also exhibit moderate debris accumulation in the joint opening.

The approach roadway in Pennsylvania is in satisfactory condition. Fine to medium map cracks were noted at the approaches. Random cracks and small spalls were also noted at the approaches.

The superstructure is in good condition. The north girder exhibits isolated areas of minor material loss to the bottom flange throughout all spans. Several rocker bearings exhibit moderate to heavy rust at the bearings and keeper angles. Minor material loss and missing retaining bolts were noted at a few of the keeper angles. Few bearings are missing shoulder bolts. No lateral movement of the bearings was noted at the time of inspection. The paint at the fascia beams is in fair condition, while the paint at the interior beams is in good condition.

The substructure is in satisfactory condition. The substructure exhibits areas of spall repair and epoxy coating that was performed by Maintenance forces. Numerous areas of spalled and hollow concrete were noted throughout the substructure. Some of these areas have been removed by maintenance forces and the exposed reinforcement was epoxy coated. The footing at Pier 8 is exposed.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure for the eastbound roadway was found to be in satisfactory condition due minor deterioration including spalls with exposed reinforcement on the concrete pier caps and stems and the exposure of the footings with no undermining noted.

DELAWARE WATER GAP TOLL BRIDGE (WESTBOUND)

(16 span, riveted steel multi-girder)

The structure is in overall satisfactory condition.

The deck is in satisfactory condition. The defects noted at the westbound roadway deck are similar to the eastbound roadway deck. The deck joints in the westbound roadway exhibit ½” to ¾” vertical offset resulting in plow catch damage at the east and west abutments and Pier 3. The aluminum median barrier exhibits scrape marks and a large gouge (6’ long by 1’ high) was noted in Span 1.

The approach roadway is in satisfactory condition. Fine to medium map cracks were noted at the approaches. A large spall was noted at the east approach slab.

The superstructure is in satisfactory condition. The defects noted at the westbound superstructure are similar to the eastbound superstructure.

The substructure is in good condition.

An underwater inspection was performed in 2006 under Contract No. C-467D. The substructure for the westbound roadway was found to be in good condition with only minor defects noted.

The results of the recently completed Northerly Crossing Corridor Congestion Mitigation Study indicate that the I-80 DWG Bridge currently operates at a level of service F during the weekday PM peak period. This report recommends that the DRJTBC proceed with an Open Road Tolling project at the I-80 DWG Bridge to help increase the throughput capacity at the I-80 bridge. Included in the ORT is the addition of a third lane in the westbound direction on the northern section of the bridge approaching the toll plaza. Widening is proposed for a length of approximately 800 feet approaching the toll plaza. This concept will require the widening of the bridge in the westbound direction. These improvements are proposed under Contract No. T-440.

DELAWARE WATER GAP TOLL BRIDGE FACILITY AND GROUNDS

The District 3 superintendent has requested that the existing maintenance garage facility be expanded. The maintenance garage currently does not have bathroom, locker room or lunchroom facilities, which are present at the other Commission toll facilities. Several of the Commission vehicles are parked outside in the open areas a distance away from the facility equipment. A training/meeting room for the district is requested. Presently meetings take place in the garage area and are disrupted by outside activity.

Maintenance has requested to replace HVAC system because it is not functioning properly. Maintenance has also requested to replace streetlight electrical panels at three locations. The metal cabinets are corroded and are difficult to open and close.

Maintenance has indicated that the salt storage capacity is insufficient for the entire district.

CONCLUSIONS

DELAWARE WATER GAP TOLL BRIDGE (EASTBOUND)

The structure is in overall satisfactory condition. The bearings should be cleaned and painted throughout the structure. Replace the bolts at locations where keeper angle and shoulder bolts are missing. The Commission should consider replacement of these bearings with elastomeric bearings. The hollow concrete areas and spalls throughout the substructure should be repaired with concrete. The north and south fascia girders and the end 6 feet of all girders should be painted. Install riprap around the exposed footing at Pier 8. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

Contract No. C-472 Delaware Water Gap Toll Bridge Bearing Remediation and Deck Study is underway and addresses the above items

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

DELAWARE WATER GAP TOLL BRIDGE (WESTBOUND)

The structure is in overall satisfactory condition. The bearings should be cleaned and painted throughout the structure. Replace the bolts at locations where keeper angle and shoulder bolts are missing. The Commission should consider replacement of these bearings with elastomeric bearings. The north and south fascia girders and the end 6 feet of all girders should be painted. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

Contract No. C-472 Delaware Water Gap Toll Bridge Bearing Remediation and Deck Study is underway and addresses the above items

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

DELAWARE WATER GAP TOLL BRIDGE FACILITY AND GROUNDS

A study for the expansion and modifications of maintenance garage is recommended.

A study should be performed on the HVAC controls to determine what components need upgrading, or if entire system should be upgraded.

A study should be performed on the electrical panels for the streetlights to determine which units need to be replaced and to specify the replacement item.

A study should be performed to determine the district's overall deicing requirements. The study should include but not limited to determining salt storage capacity, storage location, type of storage and any additional deicing capabilities.

For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

Delaware Water Gap Toll Bridge

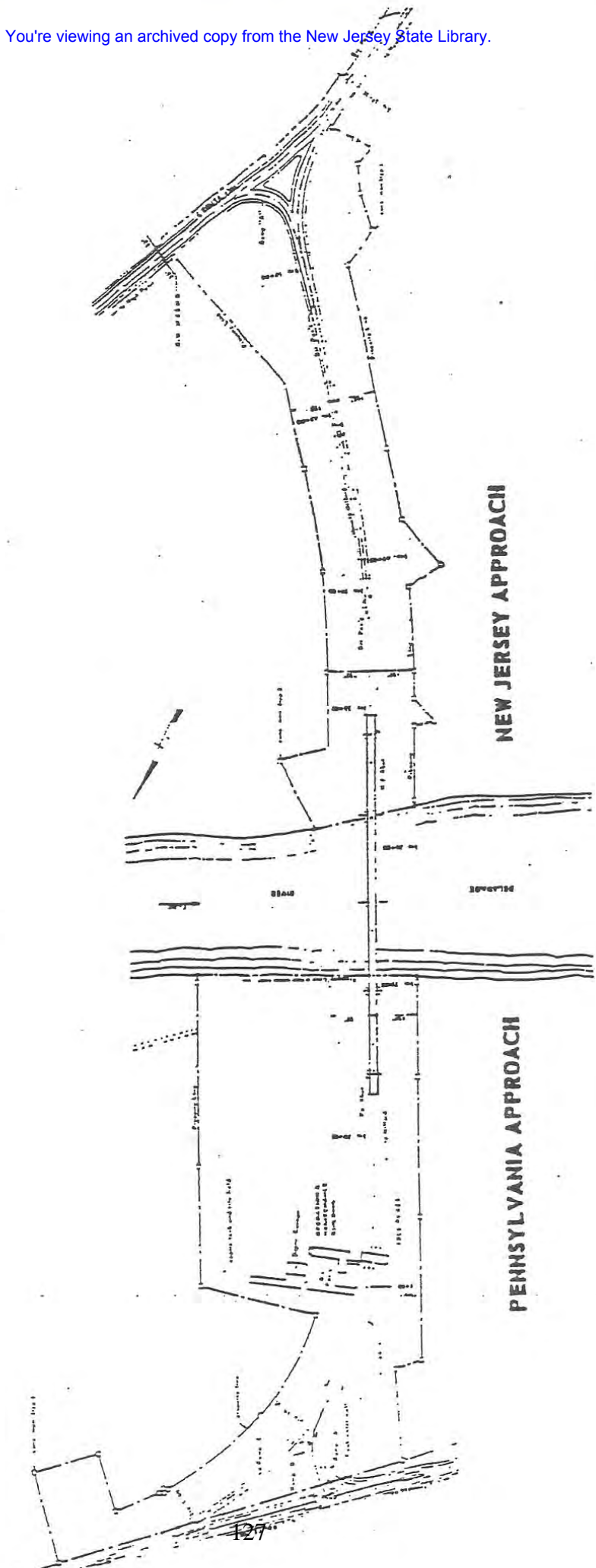
ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
395B	I-80 / DWG Task Force Consultant	\$560,000	\$0	\$0
440A	Phase 1 DWG Toll Bridge ORT Study	\$500,000	\$400,000	\$0
440B	Phase 1 - DWG Toll Bridge ORT Implementation	\$39,176,000	\$3,288,000	\$10,679,000
440C	DWG Toll Bridge Improvements	\$140,994,000	\$0	\$1,633,000
472	DWG TB Bearing Remediation and Deck Study	\$12,523,000	\$4,914,000	\$7,371,000
BRIDGES SUB TOTAL		\$193,753,000	\$8,602,000	\$19,683,000
<u>Facilities and Grounds</u>				
DWGTB	Miscellaneous Projects (less than \$100k each)	\$649,000	\$50,000	\$52,000
474	DWG Space Utilization Study	\$101,000	\$75,000	\$26,000
513	DWG HVAC Upgrade	\$562,000	\$60,000	\$502,000
FACILITIES AND GROUNDS SUB TOTAL		\$1,312,000	\$185,000	\$580,000
TOTAL COST		\$195,065,000	\$8,787,000	\$20,263,000

MILFORD-MONTAGUE
TOLL BRIDGE FACILITY
(Structure No. 400)

STATE OF NEW JERSEY
COUNTY OF SUSSEX
TOWN OF MONTAGUE

COMMONWEALTH OF PENNSYLVANIA
COUNTY OF PIKE
TOWN OF MILFORD



MILFORD - MONTAGUE TOLL BRIDGE

GENERAL

MILFORD-MONTAGUE TOLL BRIDGE

(4 span, continuous, steel deck truss)

The Milford-Montague Toll Bridge (Structure No. 400) is the northern-most toll bridge across the Delaware River under the Commission's jurisdiction. Located seven miles south of the New Jersey/New York state line, the bridge connects US Route 206 at Montague, New Jersey to US Routes 6 and 209 at Milford, Pennsylvania.

The toll bridge, built by the Commission and opened to traffic on December 30, 1953, is a four span continuous steel deck truss structure with an approximate total length of 1,150 feet. The curb to curb width of the roadway is 27'-6" and carries one lane of traffic in each direction with a posted speed limit on the New Jersey approach of 40 mph. Cantilevered from the north truss is a 4'-0" wide sidewalk. The substructure units consist of reinforced concrete abutments and piers with granite stone facing on the piers.

In 1982 the original deck was replaced with precast concrete deck panels and stringers were relocated (fifth stringer added) for the addition of the cantilevered sidewalk. Also included in the 1982 rehabilitation project were modifications to the substructures and bridge lighting, and the addition of the aluminum safety barriers. In 1998, the New Jersey approach was milled and repaved by contract.

Contract No. T-430A, a rehabilitation contract for the Milford-Montague Toll Bridge, is currently underway with an anticipated construction start date of Spring 2008. The proposed improvements to the structure are:

- Concrete deck replacement
- Superstructure steel repairs
- Cleaning and painting of the superstructure
- Substructure repairs
- Slope protection and erosion damage repairs
- Approach roadway repaving
- Drainage improvements
- Safety feature improvements (signage, guide rails, etc.)
- Toll plaza rehabilitation

MILFORD-MONTAGUE TOLL BRIDGE FACILITIES AND GROUNDS

At the Pennsylvania approach, there are three westbound toll collection lanes that are protected by a canopy and founded on concrete islands.

Maintenance forces completed the conversion of the Pennsylvania toll plaza in 1999, converting it to one-way tolls. This project included removing two toll booths and their respective lanes and canopy, reconstructing slabs, installing median barriers, and impact attenuators on the ends of the median barrier.

The 2007 inspection included the main river bridge and the facility and grounds.

SIGNIFICANT FINDINGS

MILFORD-MONTAGUE TOLL BRIDGE

(4 span, continuous, steel deck truss)

The structure is in overall satisfactory condition.

The deck is in fair condition. Several of the precast concrete deck panels exhibit large spalls with exposed epoxy coated reinforcement and few incipient spalls at the underside of the deck. Transverse cracks with efflorescence were noted beneath the transverse deck panel joints. The deck slab expansion joints, located at the piers and abutments, exhibited signs of water leakage. The east abutment finger joint is misaligned with the east approach side ½" higher causing a potential plow catch. At several locations, the bridge scupper pipes are located directly above the truss bottom chord members. No deck joint drainage trough is present below the west abutment finger joint.

The approach roadway is in good condition.

The superstructure is in satisfactory condition. Heavy rust with localized material loss was noted below at the top and bottom flange of the center stringer throughout and locally on the adjacent stringers. Several floorbeams also exhibited material loss to the web at the connection with Stringer 3. The top and bottom chord members exhibit peeling paint with light to moderate surface rust throughout and isolated locations of minor pitting. Several gusset plate connections and end diaphragms exhibit moderate to heavy rust, few with material losses, due to drainage pipes located above the members. No deck joint drainage trough is present below the west abutment finger joint. The water drains directly onto the bridge seat and down the abutment walls causing the bearings and steel below the joint to be moderately rusted.

The substructure is in satisfactory condition. Areas of fine map cracking were noted throughout both abutments and medium transverse cracks were noted at the concrete exposed portion of the pier caps. The granite stone facing at the piers exhibited random areas of missing mortar.

An underwater inspection was performed in 2006 under Contract No. C-467D. The underwater components of the substructure were noted to be in good condition with only minor defects noted. No undermining was noted during the inspection, although the Pier 2 footing was found to be partially exposed.

MILFORD-MONTAGUE TOLL BRIDGE FACILITIES AND GROUNDS

The Pennsylvania approach slab, just east of the toll plaza, is severely deteriorated with numerous wide cracks and medium to large spalls throughout. The pavement relief joint is cracked and spalled.

The concrete slabs west of the toll plaza, were rehabilitated and found to be in good condition.

Contract No. T-430A includes the rehabilitation of the toll plaza and approaches.

The water storage and distribution system for the facility is not providing sufficient pressure on occasions. Fire hydrants are located at a distance from facilities. This creates reduced fire protection for the facility and is below the capacity of other toll facilities. Contract No. T-432A, completed in 2007, provided a direct connection for municipal water through the Milford Water Authority.

The HVAC system is showing signs of the age and it is not functioning satisfactorily.

The maintenance facility asphalt pavement parking lot is in fair to poor condition with uneven pavement and wide cracking throughout. The sidewalks have random cracking and the curbs are spalled.

The paint system is failing on the steel cantilever sign structure with multiple areas of light rust. Maintenance reports that the sign panels reflectivity is significantly reduced.

The present salt storage capacity is insufficient for the entire district in the event of a major snowstorm.

CONCLUSIONS

MILFORD-MONTAGUE TOLL BRIDGE

The structure is in overall satisfactory condition. No significant work is recommended due to Contract No. T-430A, a rehabilitation contract for the Milford-Montague Toll Bridge, which is currently underway with an anticipated substantial completion date by Summer 2009. For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

The Commission has undertaken a Substructure & Scour Remediation project in Districts 1, 2 & 3 under Contract No. C-476A. This project is currently in the design phase and includes this bridge.

MILFORD-MONTAGUE TOLL BRIDGE FACILITIES AND GROUNDS

The toll plaza, approach roadway, and sign structures will be rehabilitated under Contract No. T-430A.

The parking lot should be rehabilitated.

Construction of a new waterline connecting the facility to the Milford Water Authority has been completed under Contract No. T-432A. This resolved the problem of water storage and the inadequate water pressure at the facility.

A study should be performed on the HVAC controls to determine what components need upgrading, or if the entire system should be upgraded.

A study should be performed to determine the district's overall deicing requirements. The study should include but not limited to determining salt storage capacity, storage location, type of storage and any additional deicing capabilities.

For a list of maintenance repair items, see the *Eleventh Annual Maintenance Report*.

Milford-Montague Toll Bridge

ESTIMATED COST OF RECOMMENDED IMPROVEMENTS FUNDED BY THE GENERAL RESERVE FUND

Contract No.	Bridge and Roadway Recommended Improvements	Program Cost	General Reserve Fund	
			2009	2010
<u>Bridges, Roadways, Sidewalks, and Approaches</u>				
430	M-M Toll Bridge Rehabilitation	\$19,078,000	\$7,227,000	\$0
BRIDGES SUB TOTAL		\$19,078,000	\$7,227,000	\$0
<u>Facilities and Grounds</u>				
MMTB	Miscellaneous Projects (less than \$100k each)	\$453,000	\$35,000	\$37,000
432	M-M Upgrade Water Supply	\$755,000	\$92,000	\$0
514	M-M HVAC Upgrade	\$369,000	\$52,000	\$317,000
FACILITIES AND GROUNDS SUB TOTAL		\$1,577,000	\$179,000	\$354,000
TOTAL COST		\$20,655,000	\$7,406,000	\$354,000

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VEHICLES AND EQUIPMENT

The following section identifies vehicles and equipment that have reached their useful life and are in need of being replaced. The section also recommends the addition of vehicles and equipment that will aid the Commission with daily operations.

VEHICLES AND EQUIPMENT

TRENTON-MORRISVILLE TOLL BRIDGE

Vehicular and Maintenance Equipment

Recommended New Units	Items To Be Replaced, Sold, or *Transferred	Estimated Purchase	Estimated Sale	Estimated Net
Small Tools/Misc. Equipment	New Items	\$5,000	\$0	\$5,000
2009 Hybrid SUV	*2003 Ford Crown Victoria Ser. No. 2FAHP71WX3X1556959 Lic. No. MG20553A Mileage 35,569 TM 10004	\$30,000	\$0	\$30,000
2009 Hybrid SUV	2002 Chevrolet Blazer, 4WD Ser. No. 1GNDD13W02K233650 Lic. No. SG21286 Mileage 45,122 TM 11010	\$30,000	\$4,000	\$26,000
ETC Transponders	New Items	\$52,500	\$0	\$52,500
2009 Roadway Sweeper	1996 Elgin Whirlwind Roadway Sweeper Ser. No. 1FDXH70C9TVA29963 Lic. No. SG12036 Mileage 33,839 TM 20047	\$210,000	\$30,000	\$180,000
	2002 Gerber Scientific Odyssey Signmaker Ser. No. C010209004 Lic. No. N/A Mileage N/A TM 40004	\$30,000	\$2,000	\$28,000

*Transfer to NH-L as Commission Pool Vehicle

Estimated Total	\$357,500	\$36,000	\$321,500
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NEW HOPE-LAMBERTVILLE TOLL BRIDGE

Vehicular and Maintenance Equipment

Recommended New Units	Items To Be Replaced, Sold, or Transferred	Estimated Purchase	Estimated Sale	Estimated Net
Small Tools/Misc. Equipment	New Items	\$5,000	\$0	\$5,000
ETC Transponders	New Items	\$52,500	\$0	\$52,500
Upgrade Print Shop Equip.	New Items	\$25,000	\$0	\$25,000
2009 Ford F-250 4x4 Super Duty	New Item	\$35,000	\$0	\$35,000
2009 Portable VMS Sign with Radar	New Item	\$18,000	\$0	\$18,000
	Estimated Total	\$135,500	\$0	\$135,500

SOUTHERN DIVISION TOLL-SUPPORTED BRIDGES

Vehicular and Maintenance Equipment

Recommended New Units	Items To Be Replaced, Sold, or Transferred	Estimated Purchase	Estimated Sale	Estimated Net
Small Tools/Misc. Equipment	New Items	\$5,000	\$0	\$5,000
	Estimated Total	\$5,000	\$0	\$5,000

VEHICLES AND EQUIPMENT

INTERSTATE 78 TOLL BRIDGE

Vehicular and Maintenance Equipment

Recommended New Units	Items To Be Replaced, Sold, or Transferred	Estimated Purchase	Estimated Sale	Estimated Net
Small Tools/Misc. Equipment	New Items	\$5,000	\$0	\$5,000
ETC Transponders	New Items	\$52,500	\$0	\$52,500
Welcome Center Restroom Fixtures	Old Restroom Fixtures	\$60,000	\$0	\$60,000
2009 Aerial Lift Bucket Truck	1990 Aerial Lift Bucket Truck Ser. No. 1GDM7D1Y4LV509875 Lic. No. SGB23L Hours 1,067 Mileage 13,538 I78 20127	\$175,000	\$5,000	\$170,000
2009 Hybrid SUV	*2002 Chevrolet Blazer, 4WD Ser. No. 1GNDT13X23K171663 Lic. No. SG21859 Mileage 46,947 I78 11001	\$30,000	\$0	\$30,000
Cab for John Deere Gator Tractor	New Item	\$5,500	\$0	\$5,500
		*Transfer to NH-L as Commission Pool Vehicle		
Estimated Total		\$328,000	\$5,000	\$323,000

EASTON-PHILLIPSBURG TOLL BRIDGE

Vehicular and Maintenance Equipment

Recommended New Units	Items To Be Replaced, Sold, or Transferred	Estimated Purchase	Estimated Sale	Estimated Net
Small Tools/Misc. Equipment	New Items	\$5,000	\$0	\$5,000
ETC Transponders	New Items	\$52,500	\$0	\$52,500
Portable Crash Attenuator	New Item	\$25,000	\$0	\$25,000
Car/Truck Lift for Maintenance Garage	New Item	\$30,000	\$0	\$30,000
Salt Bin Roll-up Curtains (for two (2) salt bins)	New Item	\$16,000	\$0	\$16,000
Glare Screen System for Roadway	Glare Screens need Replacement	\$25,000	\$0	\$25,000
2009 Crew Cab 4x4 Pickup Truck	2002 F-250 SuperCab Pickup Ser. No. 1FTNX21F22EA45057 Lic. No. SG20769 Mileage 115,313 EP 12001	\$40,000	\$5,000	\$35,000
2009 Hybrid SUV	2001 Ford Crown Victoria Ser. No. 2FAFP73W61X206124 Lic. No. MG93923 Mileage 81,911 EP 10002	\$30,000	\$4,000	\$26,000
2009 Medium Dumptruck	*1997 Ford F800 Dump Truck Ser. No. 1FDXF80EXVVA16641 Lic. No. SG12123 Mileage 24,355 EP 15033	\$130,000	\$0	\$130,000
	* Transfer to I-78			
	Estimated Total	\$353,500	\$9,000	\$344,500

NORTHERN DIVISION TOLL-SUPPORTED BRIDGES

Vehicular and Maintenance Equipment

Recommended New Units	Items To Be Replaced, Sold, or Transferred	Estimated Purchase	Estimated Sale	Estimated Net
Small Tools/Misc. Equipment	New Items	\$5,000	\$0	\$5,000
	Estimated Total	\$5,000	\$0	\$5,000

DELAWARE WATER GAP TOLL BRIDGE

Vehicular and Maintenance Equipment

Recommended New Units	Items To Be Replaced, Sold, or Transferred	Estimated Purchase	Estimated Sale	Estimated Net
Small Tools/Misc. Equipment	New Items	\$5,000	\$0	\$5,000
ETC Transponders	New Items	\$52,500	\$0	\$52,500
	Estimated Total	\$57,500	\$0	\$57,500

MILFORD-MONTAGUE TOLL BRIDGE

Vehicular and Maintenance Equipment

Recommended New Units	Items To Be Replaced, Sold, or Transferred	Estimated Purchase	Estimated Sale	Estimated Net
Small Tools/Misc. Equipment	New Items	\$5,000	\$0	\$5,000
ETC Transponders	New Items	\$52,500	\$0	\$52,500
	Estimated Total	\$57,500	\$0	\$57,500

SUMMARY BY DISTRICT

LOCATION	Estimated Pur. Price	Estimated Sale Price	Estimated Net Price
Trenton-Morrisville	\$357,500	\$36,000	\$321,500
New Hope-Lambertville	\$135,500	\$0	\$135,500
Southern Div. Toll-Supported	\$5,000	\$0	\$5,000
<i>District 1 Total</i>	\$498,000	\$36,000	\$462,000
Interstate 78	\$328,000	\$5,000	\$323,000
Easton-Phillipsburg	\$353,500	\$9,000	\$344,500
Northern Div. Toll-Supported	\$5,000	\$0	\$5,000
<i>District 2 Total</i>	\$686,500	\$14,000	\$672,500
Portland-Columbia	\$102,500	\$0	\$102,500
Delaware Water Gap	\$57,500	\$0	\$57,500
Milford-Montague	\$57,500	\$0	\$57,500
<i>District 3 Total</i>	\$217,500	\$0	\$217,500
TOTAL	\$1,402,000	\$50,000	\$1,352,000

2009 VEHICLES & EQUIPMENT \$1,402,000

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2009-2010 CAPITAL PLAN ESTIMATED EXPENDITURES



SUMMARY OF EXPENDITURES

CAPITAL PROGRAM ESTIMATED EXPENDITURES

	<i>2009</i>	<i>2010</i>
Toll Bridge Facilities	\$85,161,000	\$33,343,000
Toll-Supported Bridge Facilities	\$21,323,000	\$114,091,000
Commission Initiatives & System-Wide Projects	\$34,706,000	\$16,497,000

	<i>Subtotal</i>	<i>\$141,190,000</i>	<i>\$163,931,000</i>
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VEHICLE / EQUIPMENT GROSS PURCHASES

	<i>2009</i>	<i>2010</i>
Vehicular and Maintenance Equipment	\$1,402,000	\$1,500,000

	<i>Subtotal</i>	<i>\$1,402,000</i>	<i>\$1,500,000</i>
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	<i>2009</i>	<i>2010</i>
<i>TOTAL 2009 - 2010 CAPITAL PLAN</i>	<i>\$142,592,000</i>	<i>\$165,431,000</i>

2009-2010 CAPITAL PLAN ESTIMATED EXPENDITURES



<u>TOLL BRIDGES</u>	<u>2009</u>	<u>2010</u>
<u>Trenton-Morrisville</u>	\$29,840,000	\$2,021,000
<u>New Hope-Lambertville Toll-Supported Bridge</u>	\$5,564,000	\$499,000
<u>Interstate 78</u>	\$32,141,000	\$8,556,000
<u>Easton-Phillipsburg</u>	\$573,000	\$1,315,000
<u>Portland-Columbia</u>	\$850,000	\$335,000
<u>Delaware Water Gap</u>	\$8,787,000	\$20,263,000
<u>Milford-Montague</u>	\$7,406,000	\$354,000
<i>Subtotal</i>	<i>\$85,161,000</i>	<i>\$33,343,000</i>
<u>TOLL-SUPPORTED BRIDGES</u>	<u>2009</u>	<u>2010</u>
<u>Lower Trenton</u>	\$10,000	\$11,000
<u>Calhoun Street</u>	\$588,000	\$12,607,000
<u>Scudder Falls</u>	\$17,149,000	\$79,890,000
<u>Washington Crossing</u>	\$1,310,000	\$1,978,000
<u>New Hope-Lambertville</u>	\$20,000	\$11,000
<u>Centre Bridge-Stockton</u>	\$5,000	\$6,000
<u>Lumberville-Raven Rock Pedestrian Bridge</u>	\$456,000	\$1,838,000
<u>Uhlerstown-Frenchtown</u>	\$30,000	\$32,000
<u>Upper Black Eddy-Milford</u>	\$767,000	\$10,198,000
<u>Riegelsville</u>	\$752,000	\$6,224,000
<u>Northampton Street</u>	\$50,000	\$52,000
<u>Riverton-Belvidere</u>	\$116,000	\$1,233,000
<u>Portland-Columbia</u>	\$70,000	\$11,000
<i>Subtotal</i>	<i>\$21,323,000</i>	<i>\$114,091,000</i>
<u>COMMISSION INITIATIVES & SYSTEM-WIDE PROJECTS</u>	<u>2009</u>	<u>2010</u>
	<i>\$34,706,000</i>	<i>\$16,497,000</i>
<i>TOTAL CAPITAL PLAN EST. EXPENDITURES</i>	<i>\$141,190,000</i>	<i>\$163,931,000</i>

2009-2010 CAPITAL PLAN ESTIMATED EXPENDITURES



BRIDGES, ROADWAYS, SIDEWALKS, & APPROACHES SUMMARY

<u>DISTRICT I</u>	2009	2010
<u>Trenton-Morrisville Toll Bridge</u>	\$28,465,000	\$271,000
<u>Lower Trenton Toll-Supported Bridge</u>	\$0	\$0
<u>Calhoun Street Toll-Supported Bridge</u>	\$578,000	\$12,596,000
<u>Scudder Falls Toll-Supported Bridge</u>	\$17,139,000	\$79,879,000
<u>Washington Crossing Toll-Supported Bridge</u>	\$1,300,000	\$1,967,000
<u>New Hope-Lambertville Toll-Supported Bridge</u>	\$0	\$0
<u>New Hope Lambertville Toll Bridge</u>	\$5,266,000	\$0
<u>Centre Bridge-Stockton Toll-Supported Bridge</u>	\$0	\$0
<u>Lumberville-Raven Rock Pedestrian Bridge</u>	\$446,000	\$1,827,000
<i>District I Total</i>	\$53,194,000	\$96,540,000
<u>DISTRICT II</u>	2009	2010
<u>Uhlerstown-Frenchtown Toll-Supported Bridge</u>	\$0	\$0
<u>Upper Black Eddy-Milford Toll-Supported Bridge</u>	\$752,000	\$10,182,000
<u>Riegelsville Toll-Supported Bridge</u>	\$742,000	\$6,213,000
<u>Interstate 78 Toll Bridge</u>	\$32,091,000	\$8,220,000
<u>Northampton Street Toll-Supported Bridge</u>	\$0	\$0
<u>Easton-Phillipsburg Toll Bridge</u>	\$334,000	\$787,000
<u>Riverton-Belvidere Toll-Supported Bridge</u>	\$111,000	\$1,227,000
<i>District II Total</i>	\$34,030,000	\$26,629,000

2009-2010 CAPITAL PLAN ESTIMATED EXPENDITURES



BRIDGES, ROADWAYS, SIDEWALKS, & APPROACHES SUMMARY

<u>DISTRICT III</u>	<u>2009</u>	<u>2010</u>
<u>Portland-Columbia Toll Bridge</u>	\$830,000	\$236,000
<u>Portland-Columbia Pedestrian Bridge</u>	\$60,000	\$0
<u>Delaware Water Gap Toll Bridge</u>	\$8,602,000	\$19,683,000
<u>Milford-Montague Toll Bridge</u>	\$7,227,000	\$0
District III Total	\$16,719,000	\$19,919,000

	<u>2009</u>	<u>2010</u>
BRIDGES, ROADWAYS, SIDEWALKS & APPROACHES TOTAL	\$103,943,000	\$143,088,000

FACILITIES AND GROUNDS SUMMARY

<u>DISTRICT I</u>	<u>2009</u>	<u>2010</u>
<u>Trenton-Morrisville Toll Bridge</u>	\$1,375,000	\$1,750,000
<u>Lower Trenton Toll-Supported Bridge</u>	\$10,000	\$11,000
<u>Calhoun Street Toll-Supported Bridge</u>	\$10,000	\$11,000
<u>Scudder Falls Toll-Supported Bridge</u>	\$10,000	\$11,000
<u>Washington Crossing Toll-Supported Bridge</u>	\$10,000	\$11,000
<u>New Hope-Lambertville Toll-Supported Bridge</u>	\$20,000	\$11,000
<u>New Hope Lambertville Toll Bridge</u>	\$298,000	\$499,000
<u>Centre Bridge-Stockton Toll-Supported Bridge</u>	\$5,000	\$6,000
<u>Lumberville-Raven Rock Pedestrian Bridge</u>	\$10,000	\$11,000
District I Total	\$1,748,000	\$2,321,000

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2009-2010 CAPITAL PLAN ESTIMATED EXPENDITURES



FACILITIES AND GROUNDS SUMMARY

<u>DISTRICT II</u>	<u>2009</u>	<u>2010</u>
<u>Uhlerstown-Frenchtown Toll-Supported Bridge</u>	\$30,000	\$32,000
<u>Upper Black Eddy-Milford Toll-Supported Bridge</u>	\$15,000	\$16,000
<u>Riegelsville Toll-Supported Bridge</u>	\$10,000	\$11,000
<u>Interstate 78 Toll Bridge</u>	\$50,000	\$336,000
<u>Northampton Street Toll-Supported Bridge</u>	\$50,000	\$52,000
<u>Easton-Phillipsburg Toll Bridge</u>	\$239,000	\$528,000
<u>Riverton-Belvidere Toll-Supported Bridge</u>	\$5,000	\$6,000
<i>District II Total</i>	\$399,000	\$981,000

FACILITIES AND GROUNDS SUMMARY

<u>DISTRICT III</u>	<u>2009</u>	<u>2010</u>
<u>Portland-Columbia Toll Bridge</u>	\$20,000	\$99,000
<u>Portland-Columbia Pedestrian Bridge</u>	\$10,000	\$11,000
<u>Delaware Water Gap Toll Bridge</u>	\$185,000	\$580,000
<u>Milford-Montague Toll Bridge</u>	\$179,000	\$354,000
<i>District III Total</i>	\$394,000	\$1,044,000

	<u>2009</u>	<u>2010</u>
<i>FACILITIES AND GROUNDS TOTAL</i>	\$2,541,000	\$4,346,000

2009-2010 CAPITAL PLAN ESTIMATED EXPENDITURES



EQUIPMENT PURCHASES

2009 VEHICLE & EQUIPMENT PURCHASES

<u>Toll Facility</u>	<u>Estimated Purchase Price of New Units</u>	<u>Estimated Sell Price of Used Units</u>	<u>Estimated Net Cost</u>
Trenton-Morrisville	\$357,500	\$36,000	\$321,500
New Hope-Lambertville	\$135,500	\$0	\$135,500
Interstate Route 78	\$328,000	\$5,000	\$323,000
Easton-Phillipsburg	\$353,500	\$9,000	\$344,500
Portland-Columbia	\$102,500	\$0	\$102,500
Delaware Water Gap	\$57,500	\$0	\$57,500
Milford-Montague	\$57,500	\$0	\$57,500
Southern - Toll-Supported Bridges	\$5,000	\$0	\$5,000
Northern - Toll-Supported Bridges	\$5,000	\$0	\$5,000
	<u>\$1,402,000</u>	<u>\$50,000</u>	<u>\$1,352,000</u>

TOTAL 2009 GROSS VEHICLE & EQUIPMENT PURCHASES \$1,402,000

ESTIMATED 2010 GROSS VEHICLE & EQUIPMENT PURCHASES* \$1,500,000

**The \$1.5M 2010 Vehicle & Equipment Expense is a Planned Budget Amount. 2009 Vehicle & Equipment Expense are based on the "actual" estimates shown in the "Vehicles and Equipment" section of the 2008 Annual Inspection Report.*

SCHEDULE OF INSURANCE

I. CURRENT SCHEDULE OF INSURANCE (2009)

The Delaware River Joint Toll Bridge Commission currently has in effect the following principle types and amounts of insurance coverage:

A. General Liability

\$	2,000,000	General Aggregate Limit
\$	2,000,000	Products/Completed Operations Aggregate Limit
\$	1,000,000	Personal/Advertising Injury Limit
\$	1,000,000	Each Occurrence Limit
\$	300,000	Fire Damage Limit, Any One Fire
\$	15,000	Medical Expense Limit, Any One Person

The above General Liability limits apply for all bridges (Toll and Toll-Supported Bridges).

The above General Liability limits apply per each location.

Coverage includes Independent Contractors, Medical Payments, Contractual Liability, Fire Damage, Legal Liability, Employees as Additional Insured, Host Liquor Liability, Incidental Medical Malpractice, Broad Form Property Damage Liability, Non-owned Watercraft Liability (under 26ft), Limited Worldwide Products Liability and Extended Bodily Injury Liability.

B. Commercial Automobile Liability

\$	1,000,000	Bodily Injury/Property Damage Combined Single Limit, Each Accident
\$	35,000	Uninsured/Underinsured Motorist Coverage (PA & NJ) (\$1,000,000 applies to PPV's, \$35,000 applies to all other vehicles)
\$	50,000	Garagekeepers Liability
(Lesser of ACV or Cost of Repair)		Hired Car Physical Damage Coverage

Deductible on Comprehensive and Collision

\$	500	Cost New Less than \$29,999
\$	1,000	Cost New \$30,000-\$49,999
\$	2,000	Cost New Greater Than 50,000

C. Umbrella Liability

\$	25,000,000	Each Occurrence, Annual Aggregate
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There is an excess umbrella policy with a \$25,000,000 limit. The total coverage of \$50,000,000 is inclusive of all Bridges, Vehicles, and Operations.

SCHEDULE OF INSURANCE

D. Building & Contents Insurance

\$	1,000,000	Extra Expense
\$	10,000,000	Loss Limit Location #1
\$	5,000,000	Loss Limit Locations 2-7
\$	500,000	Unnamed Locations
\$	5,000	Deductible

(Additional sub-limits and deductibles apply)

Coverage extensions include: Debris Removal, Pollutant Cleanup and Removal, Newly Acquired Buildings and Personal Property, Personal Property of Others/Employees, Valuable papers-Cost of Research, Property Off Premises within 1,000 feet, Outdoor Property - Trees, Shrubs and Plants, Property in Transit (Special Form Only) and Fences and Signs.

E. Equipment Floater Limits (Included in Building Policy)

\$	2,280,800	Specific Limits Apply Per Schedule
\$	240,756	Miscellaneous Unscheduled Tools
\$	1,000	Deductible

F. Bridge Property Coverage

Loss Limits:

\$	50,000,000	Loss Limit – Primary
\$	50,000,000	Loss Limit – Excess of \$50,000,000 per Occurrence
\$	375,000,000	Loss Limit – Excess of \$100,000,000 per Occurrence

Values:

Toll Bridge Summary

Trenton-Morrisville Facility

\$	24,100,000	Bridge
\$	18,100,000	Viaducts
\$	9,378,237	Use/Occupancy

New Hope-Lambertville Facility

\$	30,500,000	Bridge
\$	6,800,000	Viaducts
\$	2,374,689	Use/Occupancy

SCHEDULE OF INSURANCE

Interstate Route 78 Facility

\$	34,300,000	Bridge
\$	26,100,000	Viaducts
\$	38,881,866	Use/Occupancy

Easton-Phillipsburg Facility

\$	18,200,000	Bridge
\$	4,000,000	Viaducts
\$	9,404,786	Use/Occupancy

Portland-Columbia Facility

\$	16,400,000	Bridge
\$	4,200,000	Viaducts
\$	1,737,063	Use/Occupancy

Delaware Water Gap Facility

\$	45,000,000	Bridge
\$	26,881,266	Use/Occupancy

Milford-Montague Facility

\$	12,100,000	Bridge
\$	1,206,526	Use/Occupancy

All Seven (7) Bridges

\$	180,600,000	Bridges
\$	59,200,000	Viaducts
\$	89,864,433	Use and Occupancy
\$	329,664,433	TOTAL (Toll Bridges)

Toll-Supported Bridge Summary

<u>Lower Trenton</u>	\$	14,100,000
<u>Calhoun Street</u>	\$	17,700,000
<u>Scudder Falls</u>	\$	54,100,000
<u>Washington Crossing</u>	\$	12,200,000
<u>New Hope-Lambertville</u>	\$	14,600,000
<u>Centre Bridge-Stockton</u>	\$	11,500,000
<u>Lumberville-Raven Rock</u>	\$	2,900,000
<u>Uhlerstown-Frenchtown</u>	\$	13,100,000
<u>Upper Black Eddy-Milford</u>	\$	10,000,000
<u>Riegelsville</u>	\$	8,600,000
<u>Northampton Street</u>	\$	12,200,000
<u>Riverton-Belvidere</u>	\$	9,500,000

SCHEDULE OF INSURANCE

Portland-Columbia \$ 3,200,000

All Thirteen (13) Bridges \$ 183,700,000

GRAND TOTAL: TWENTY (20) BRIDGES: \$513,364,433

Use and Occupancy Deductible – 5 days, All other 1% of Loss (\$50,000 Minimum)
Flood Coverage - \$250,000,000 Annual Aggregate - Multiple Policies
Earthquake Coverage – \$150,000,000 Annual Aggregate - Multiple Policies
Boiler & Machinery Coverage Insured under separate policy

G. Public Officials / Employee Liability

\$ 10,000,000 Each Loss
\$ 10,000,000 Aggregate

Retention

\$ 50,000 Corporate Reimbursement
\$ 50,000 Entity Coverage
\$ 35,000 Employee Coverage

Excess policy provides additional \$10,000,000 Per Claim/Annual Aggregate

H. Workers Compensation Coverage - The Graham Company is not the broker for this coverage

Statutory Benefits for Medical, Disability, Funeral Expenses and Rehabilitative Expenses
\$500,000 Bodily Injury by Accident – Each Accident
\$500,000 Bodily Injury by Disease – Policy Limit
\$500,000 Bodily Injury by Disease – Each Employee

I. Commercial Crime Coverage

\$ 10,000 Forgery or Alteration, \$1,000 deductible
\$ 250,000 Money In-Out for Theft, Disappearance and Destruction, \$10,000 deductible
\$ 5,000,000 Employee Dishonesty, \$50,000 Deductible

Coverage includes all locations.

SCHEDULE OF INSURANCE

II. INSURANCE REQUIREMENTS FOR 2009

In accordance with Section 708 of the Bridge System Revenue Bond Resolution, the following types of insurance are required to be maintained by the Commission to the extent as reasonably obtainable:

MULTI-RISK INSURANCE

The Commission currently maintains insurance for full replacement of all twenty (20) Toll and Toll-Supported Bridges and their approach structures (viaducts). In 1999 the Commission supplemented the full insurance coverage for the thirteen (13) Toll-Supported Bridges. The full replacement costs are reviewed annually and updated accordingly to follow current inflation and construction costs.

TranSystems has re-assessed each of the twenty (20) Toll and Toll-Supported Bridges and their associated approach structures (viaducts) with respect to the structures replacement costs. Most of the bridges, when and if replaced, will be required to be re-constructed in accordance with current standards, codes and practices, in lieu of a replacement in kind. A simple cost per square foot (the overall bridge length multiplied by its overall width) was used in the development of replacement costs for all of the Toll and Toll-Supported Bridges and their approach structures (viaducts). Square foot unit costs may vary between bridges due to specific characteristics such as the need for deep foundations, feature crossed and aesthetics.

The 2008 Estimated Replacement Costs for the twenty Toll and Toll-Supported Bridges and their approach structures are listed below:

<u>TOLL FACILITY</u>		<u>BRIDGE</u>		<u>APPROACH STRUCTURES</u>
Trenton-Morrisville	\$	43,600,000	\$	20,400,000
New Hope-Lambertville	\$	43,000,000	\$	9,400,000
Interstate Route 78	\$	50,400,000	\$	33,600,000
Easton-Phillipsburg	\$	10,200,000	\$	10,700,000
Portland-Columbia	\$	18,400,000	\$	3,800,000
Delaware Water Gap	\$	68,000,000	\$	0
Milford-Montague	\$	15,600,000	\$	0
<i>SUBTOTALS</i>	\$	<i>249,200,000</i>	\$	<i>77,900,000</i>

<u>TOLL-SUPPORTED FACILITY</u>		<u>BRIDGE</u>		<u>APPROACH STRUCTURES</u>
Lower Trenton	\$	17,900,000	\$	0
Calhoun Street	\$	10,700,000	\$	0
Scudder Falls	\$	44,400,000	\$	5,600,000
Washington Crossing	\$	5,600,000	\$	0
New Hope-Lambertville	\$	8,900,000	\$	0
Centre Bridge-Stockton	\$	7,200,000	\$	700,000
Lumberville-Raven Rock *	\$	2,500,000	\$	0
Uhlerstown-Frenchtown	\$	7,100,000	\$	0
Upper Black Eddy-Milford	\$	6,300,000	\$	0
Riegelsville	\$	4,000,000	\$	0
Northampton Street	\$	7,500,000	\$	0
Riverton-Belvidere	\$	4,900,000	\$	0

SCHEDULE OF INSURANCE

Portland-Columbia *	\$	3,400,000	\$	0
<i>SUBTOTALS</i>	\$	<i>130,400,000</i>	\$	<i>6,300,000</i>

* Pedestrian Bridge

Total (All Bridges) Replacement Cost for 2008 = \$ 463,800,000

USE AND OCCUPANCY INSURANCE

The Commission currently maintains Use and Occupancy Insurance for all of its seven (7) Toll Facilities. The Commission has provided the anticipated 2009 revenues presented below.

<u>TOLL FACILITY</u>	<u>2009 ANTICIPATED REVENUE</u>
Trenton-Morrisville	\$ 8,882,610
New Hope-Lambertville	\$ 2,058,528
Interstate Route 78	\$ 36,300,475
Easton-Phillipsburg	\$ 9,105,203
Portland-Columbia	\$ 1,679,293
Delaware Water Gap	\$ 24,437,661
Milford-Montague	\$ 1,150,872
(Total Toll Revenue)	\$ 83,614,642
Interest on Investments	\$ 3,927,000
Other Income	\$ 389,000
(TOTAL PROJECTED REVENUE - 2009)	\$ 87,930,642

WAR-RISK INSURANCE

The Commission does not maintain this type of insurance for any of its bridges, as it is not reasonably obtainable due to its excessive cost. However the Commission does maintain coverage for terrorism.

PUBLIC LIABILITY – PROPERTY DAMAGE – BODILY INJURY

Public Liability and Property Damage are maintained by the Commission under its General Liability and Auto Liability insurance coverage, which provides a maximum coverage of \$1,000,000. In addition the Commission carries \$50,000,000 maximum coverage in Excess Liability Insurance on all Bridges, Vehicles and Operations and \$500,000 in Business Travel Accident Insurance.

SCHEDULE OF INSURANCE

**BLANKET REAL AND PERSONAL PROPERTY INSURANCE-
ADMINISTRATIVE & MAINTENANCE BUILDINGS, CONTENTS, TOLL BOOTHS, ETC.**

The Commission currently maintains Building and Contents Insurance in the amount of \$27,189,000. Estimated replacement costs for all Toll Facility Administration Buildings, Maintenance Buildings and Garages and Toll Plazas were recalculated, based upon the overall square-foot area of each facility originally calculated and increased by a factor of 1.5% and rounded. The estimated replacement costs for 2009 are as follows:

<u>LOCATION</u>	<u>2009 ESTIMATED REPLACEMENT VALUE</u>
Trenton-Morrisville	\$ 8,444,000
New Hope-Lambertville	\$ 3,485,000
Interstate 78	\$ 4,044,000
Easton-Phillipsburg	\$ 4,080,000
Portland-Columbia	\$ 1,641,000
Delaware Water Gap	\$ 3,696,000
Milford-Montague	\$ 2,293,000
Belvidere (Storage Bldg.)	\$ 256,000
New Hope Toll Supported (Garage)	\$ 180,000
15 Toll Supported Bridge Officer Shelters	\$ 217,000
Lumberville-Raven Rock (Bridge Tender house)	\$ 266,000
TOTAL	\$ 28,602,000

OTHER INSURANCE

Following good business practice and conforming to the laws of the State of New Jersey and the Commonwealth of Pennsylvania, the Commission carries additional insurance to that which is required by the Bridge System Revenue Bond Resolution. Among this additional coverage is a \$10 million Public Officials Liability insurance.

SCHEDULE OF INSURANCE

III. CONCLUSIONS AND RECOMMENDATIONS FOR 2009

In general the Commission's overall insurance coverage is adequately provided; however, the amounts of the following coverage's should be adjusted:

- The Multi-Risk Insurance coverage should be adjusted for each Toll and Toll-Supported Bridge Facility to reflect the estimated 2009 bridge (and approach structure) replacement costs, as outlined above.
- The Use and Occupancy Insurance should be adjusted to reflect the estimated 2009 anticipated revenues in conformance with the Bridge System Revenue Bond Resolutions.
- The Blanket Building and Contents Insurance should be adjusted to reflect the 2009 estimated property replacement values published above.

PAIN T CONDITION RATINGS

- EXCELLENT** - No problems noted.
- GOOD** - Some minor problems, but paint is sound and functioning as intended to protect the metal surfaces.
- SATISFACTORY** - Surface or freckled rust has formed or is forming. The paint system may be chalking, peeling or showing signs of paint distress, but there is no exposure of metal.
- FAIR** - Surface or freckled rust is prevalent. There may be exposed metal and/or beginning signs of active corrosion, but there is little to no section loss of steel members.
- POOR** - The overall paint system has failed which has consequently caused corrosion and significant section loss to steel members. Exposed metal and/or corrosion is typical throughout the bridge. A new paint system is required.

NOTE: Paint system ratings for a bridge will be an overall condition. Although localized areas may exhibit a better or worse condition, the rating encompasses the majority of the bridge paint system for the entire bridge.

BRIDGE CONDITION RATINGS

<u>EXCELLENT</u> -	New bridge.
<u>VERY GOOD</u> -	No problems noted.
<u>GOOD</u> -	Some minor problems.
<u>SATISFACTORY</u> -	Some minor deterioration of structural elements.
<u>FAIR</u> -	Minor section loss, deterioration, spalling and/or scour of primary structural elements.
<u>POOR</u> -	Advanced section loss, deterioration, spalling and/or scour of primary structural elements.
<u>SERIOUS</u> -	Seriously deteriorated primary structural elements.
<u>CRITICAL</u> -	Facility should be closed until repairs are performed.
<u>IMMINENT FAILURE</u> -	Facility is closed. Study of repairs is feasible.
<u>FAILED</u> -	Facility is closed and beyond repair.

NOTE: These condition ratings are used to describe the existing, in-place bridge as compared to its as-built condition or its posted weight restriction. These ratings provide an overall characterization of the general condition of the entire bridge. These ratings do not describe a localized or nominally occurring instance of deterioration or disrepair or reflect structural or geometric adequacy.

COST ESTIMATING

The costs associated with the repairs and rehabilitation for various elements at the bridge facilities are estimated based upon the following criteria as applicable or available:

- 1) **BID PRICES**: Quantities are developed during routine inspections for the appropriate repair (square foot, cubic yard, etc.). A unit cost is developed using standard bid items most resembling the repair. Inflation, if required, is used to increase unit costs for repair next year.
- 2) **COMMISSION PERSONNEL/HISTORY**: Maintenance staff are interviewed about the materials and length of time required for certain repairs. Maintenance staff are also asked about previous work relating to the proposed work and the costs relating to them. Depending on the year and extent of the previous work, the proposed costs are adjusted accordingly.
- 3) **EXPERIENCE**: Some of the proposed repairs/rehabilitation cannot be accurately quantified and no previous related work is available. Costs are then developed based upon experience of similar tasks. A length of time to complete the job is assumed and costs are approximated.

NOTE: Cost Estimates for major rehabilitation work include a 20% increase in cost to account for engineering services to prepare the contract documents and supervise construction.

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APPENDIX A:
BRIDGE LISTING

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Bridge Name	Structure Type	No. Of Spans	Structure Length (FT - IN)
Trenton-Morrisville Toll Bridge	Steel Multi-Girder	12	1324 - 6
Washington Street Overpass (Pa)	Steel Multi-Girder	1	52 - 9 c-c brg.
South Pennsylvania Avenue Overpass (Pa)	Steel Multi-Girder	1	63 - 7 c-c brg.
Ramp "IY" Overpass (NJ) (Bridge St.)	Steel Multi-Girder	3	132 - 9 c-c brg.
Union Street Overpass (NJ)	Steel Multi-Girder	1	74 - 6 c-c brg.
Ramp "C" over Route 29 (NJ)	Steel Multi-Girder	3	183 - 3
Ramp "N" Over Union Street (NJ)	P/S Concrete Girder	3	168 - 0 c-c brg.
Center Street Underpass (NJ)	Riveted Steel Plate Girder	1	91 - 3 c-c brg.
Broad Street Underpass (NJ)	Steel Multi-Girder	1	76 - 11 c-c brg.
Ramp 'N' Overpass (NJ)	Steel Multi-Girder	1	77 - 1 c-c brg.
Route 29 Overpass @ TMTB (NJ)	P/S Concrete Spread Box Beams	3	118 - 0
Ramp 'Y' Overpass (Long Ramp) (NJ)	Steel Multi-Girder	4	282 - 0 c-c brg.
Lower Trenton Toll-Supported Bridge	Subdivided Warren Truss	5	1021 - 7
Calhoun Street Toll-Supported Bridge	Iron Phoenix Truss	7	1273 - 3
Scudder Falls Toll-Supported Bridge	Riveted Steel 2 Girder/Floorbeam/Stringer	10	1740
Taylorville Road Overpass (Pa)	Steel Multi-Stringer	3	134 - 0 c-c brg.
Pennsylvania Canal Overpass (Pa)	Steel Multi-Stringer	1	61 - 4
Washington Crossing Toll-Supported Bridge	Double Warren Truss	6	876 - 7
New Hope-Lambertville Toll-Supported Bridge	Pratt Truss	6	1045 - 6.5
New Hope Lambertville Toll Bridge	Steel 2 Girder/Floorbeam/Stringer	10	1682
Route 32 Overpass (Pa)	Concrete Rigid Frame	1	83 - 7
Route 29 Overpass @ NHLTB (NJ)	Steel Multi-Stringer	3	185 - 0 c-c brg.
Centre Bridge-Stockton Toll-Supported Bridge	Riveted Steel Warren Truss	6	824 - 10
Pennsylvania Canal Bridge	P/S Concrete Adjacent Box Beams	1	63 - 0
Lumberville-Raven Rock Pedestrian Bridge	Suspension	4	688 - 3
Uhlertown-Frenchtown Toll-Supported Bridge	Riveted Steel Warren Truss	6	950 - 10
Upper Black Eddy-Milford Toll-Supported Bridge	Warren Truss	3	699 - 9.25
Riegelsville Toll-Supported Bridge	Suspension	3	576 - 9.875
Interstate 78 Toll Bridge WB	Steel Multi-Girder	7	1222
Interstate 78 Toll Bridge EB	Steel Multi-Girder	7	1222
Morgan Hill Road Bridge Overpass (Pa)	P/S Concrete Spread Box Beams	2	210 - 0 c-c brg.
Cedarville Road Overpass (Pa)	P/S Concrete I-Beams	4	Unknown
I-78 over Route 611 (Pa) WB	P/S Concrete Spread Box Beams	3	197 - 6 c-c brg.
I-78 over Route 611 (Pa) EB	P/S Concrete Spread Box Beams	3	199 - 9 c-c brg.
Carpentersville Road Overpass (NJ)	Steel Multi-Stringer	2	203 - 0 c-c brg.
Edge Road Overpass (NJ)	Steel Multi-Stringer	2	272 - 0 c-c brg.
I-78 WB over Route 519 (NJ)	Steel Multi-Stringer	2	237 - 10 c-c brg.
I-78 EB over Route 519 (NJ)	Steel Multi-Stringer	2	236 - 5 c-c brg.
I-78 WB over Ramp C (NJ)	Steel Multi-Stringer	1	112 - 6 c-c brg.
I-78 EB over Ramp C (NJ)	Steel Multi-Stringer	1	116 - 11 c-c brg.
Service Road Overpass (Pa)	P/S Concrete Adjacent Box Beams	1	43 - 0 c-c brg.
Northampton Street Toll-Supported Bridge	Cantilever Truss	3	550 - 0 Pin to Pin
Easton-Phillipsburg Toll Bridge	Petit Thru-Truss	1	539 - 8 Pin to Pin
Broad Street Viaduct (NJ)	Riveted Steel 3 Girder/Floorbeam/Stringer	5	431 - 4
Third Street Overpass (Pa)	Steel Multi-Stringer	1	83 - 0 c-c brg.
Pedestrian Tunnel (Pa)	Reinforced Concrete Box Culvert	1	Unknown
Bank Street Overpass (Pa)	Steel Multi-Stringer	3	120 - 0 c-c brg.
Route 611 Overpass (Pa)	P/S Concrete Adjacent Box Beams	1	34 - 0 fc-fc abut.
Riverton-Belvidere Toll-Supported Bridge	Riveted Steel Double Warren Truss	4	652 - 5
Portland-Columbia Toll Bridge	Riveted Steel Multi-Girder	10	1309
Route 46 Overpass (NJ)	Riveted Steel Multi-Girder	1	96 - 1
Locust Street Overpass (NJ)	Steel Multi-Stringer	4	170 - 0 c-c brg.
Portland-Columbia Pedestrian Bridge	Steel Thru-Deck Girder	4	770
Delaware Water Gap Toll Bridge EB	Riveted Steel Multi-Girder	17	2398 - 6 c.c Brg. Abut.
Delaware Water Gap Toll Bridge WB	Riveted Steel Multi-Girder	16	2462 - 10 c.c. Brg. Abut.
Milford-Montague Toll Bridge	Steel Deck Truss	4	1150

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2008 TRAFFIC ENGINEERING REPORT

Year 2009 Toll Bridge Traffic Volume And Revenue Projections



Delaware River
Joint Toll Bridge
Commission

Submitted to:

Delaware River Joint Toll Bridge Commission

New Hope Headquarters and Administration Building

2492 River Road, New Hope, PA 18938-9519

February 23, 2009

COPY

Gregory K. Farnum, P.E.

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EXECUTIVE SUMMARY

Pennoni Associates Inc. (Pennoni) has been retained by the Delaware River Joint Toll Bridge Commission (Commission) to determine if the projected year 2009 revenues will be enough to satisfy the conditions of all current Bridge System Revenue Bonds, which require under Section 703 (b), paragraph 2 that the Commission will not issue any Additional Bonds constituting Long-Term Indebtedness unless (along with other things) the following is delivered to the Trustee:

A report of a Consultant to the effect that (i) the Net Revenues of the Commission during the preceding Fiscal Year were at least 130% of the Maximum Annual Debt Service on all Applicable Long-Term Indebtedness then Outstanding and on any Applicable Long-Term Indebtedness proposed to be issued (which report may assume any revisions of the Tolls which have been approved by the Commission subsequent to the beginning of such Fiscal Year were in effect for the entire Fiscal Year), and (ii) the Projected Debt Service Coverage Ratio is not less than 1.30.

Revenues for 2009 were projected by applying the current toll structure to the 2009 projected volumes for each vehicle type on the seven (7) toll bridges under the jurisdiction of the Commission.

The sum of year 2009 projected toll bridge revenues (\$88,297,992) under the current toll structure with modified E-ZPass discounts is high enough to satisfy Section 703 (b), paragraph 2 of current Bridge System Revenue Bonds. Table 16 lists the projected revenues and expenditures for the year 2009. Since there is a projected Debt Service Coverage Ratio of 1.40, the requirements of current Bridge System Revenue Bonds are projected to be met.

In an effort to achieve the required revenues, the Commission has removed the 20% casual E-ZPass discount for passenger cars and the 5% E-ZPass discount for trucks during peak periods (6 AM – 9 PM), and the off peak truck E-ZPass discount has been reduced from 15% to 10%. This toll adjustment occurred on January 15, 2009. Additionally, to bring the Commission in conformance with other toll agency practices, the Commission will be charging the 67,800 E-ZPass accounts a monthly \$1 account service fee beginning June 1, 2009.



INTRODUCTION

Pennoni Associates Inc. (Pennoni) has been retained by the Delaware River Joint Toll Bridge Commission (Commission) to project traffic volumes by vehicle type on the seven (7) toll bridges for the year 2009. The seven (7) toll bridges and 13 toll-supported bridges under the jurisdiction of the Commission are listed below from south to north.

TOLL BRIDGES	TOLL-SUPPORTED BRIDGES
DISTRICT ONE	
Trenton–Morrisville (U.S. Route 1)	Lower Trenton
New Hope–Lambertville (U.S. Route 202)	Calhoun Street
	Scudder Falls (Interstate 95)
	Washington Crossing
	New Hope–Lambertville (Route 179)
	Centre Bridge–Stockton
	Lumberville–Raven Rock (Pedestrian Only)
DISTRICT TWO	
Interstate 78	Uhlerstown–Frenchtown
Easton–Phillipsburg (U.S. Route 22)	Upper Black Eddy–Milford
	Riegelsville
	Northampton Street
	Riverton–Belvidere
DISTRICT THREE	
Portland–Columbia	Portland Columbia (Pedestrian Only)
Delaware Water Gap (Interstate 80)	
Milford–Montague	

The purpose of the study is to determine if year 2009 projected toll revenues (under the current toll structure) will satisfy the requirements of current Bridge System Revenue Bonds, which require under Section 703 (b), paragraph 2 that the Commission will not issue any Additional Bonds constituting Long-Term Indebtedness unless (along with other things) the following is delivered to the Trustee:

A report of a Consultant to the effect that (i) the Net Revenues of the Commission during the preceding Fiscal Year were at least 130% of the Maximum Annual Debt Service on all Applicable Long-Term Indebtedness then Outstanding and on any Applicable Long-Term Indebtedness proposed to be issued (which report may assume any revisions of the Tolls which have been approved by the Commission subsequent to the beginning of such Fiscal Year were in effect for the entire Fiscal Year), and (ii) the Projected Debt Service Coverage Ratio is not less than 1.30.

Since there is a projected Debt Service Coverage Ratio of 1.40, the requirements of current Bridge System Revenue Bonds are projected to be met.



METHODOLOGY

To project traffic volumes on the toll bridges for the year 2009, we considered new development projects which could add traffic to the toll bridges, roadway construction projects which could divert motorists from their regular routes, and general background growth, based on historic traffic volume data crossing the bridges.

YEAR 2009 DEVELOPMENT PROJECTS

County planning/engineering offices for the eight (8) counties along the Delaware River within the study area (Bucks, Northampton, Monroe, and Pike counties in Pennsylvania and Mercer, Hunterdon, Warren, and Sussex counties in New Jersey) as well as staff from the Pennsylvania Department of Transportation (PENNDOT) and the New Jersey Department of Transportation (NJDOT) were contacted to learn of large developments which could have a major affect on toll bridge volumes during the year 2009. While several development projects are underway, only a few major projects are expected to open/expand/contract during the 2009 calendar year. For informational purposes, we have discussed major projects which may reach full buildout in the distant future but will likely not contribute any traffic during the year 2009.

District 1

In Mercer County, there are several small to medium sized developments at various stages of the approval process, but many are not near the Delaware River or Route 1. The Quakerbridge Mall expansion is still a few years off. No specific increase in traffic at the Trenton-Morrisville Toll Bridge was assumed from Mercer County developments.

In Bucks County, several medium sized office, retail and industrial developments ranging from 100,000 – 300,000 square feet have been proposed. We have not assumed any additional traffic crossing the Trenton-Morrisville Toll Bridge from these developments in 2009.

In Hunterdon County, there are no new major developments proposed in the area of the toll bridges.

District 2

The Sands Bethworks Casino is one of the five new stand alone casinos in Pennsylvania, and one of two outside of Philadelphia and Pittsburgh. The 3,000 slot machine casino will be located at the former Bethlehem Steel plant in the City of Bethlehem and is expected to open in late May of 2009. In addition, the site will include a 300 room hotel, 200,000 square feet of retail space and a variety of dining and entertainment options. The property will also be home to the National Museum of Industrial History, an arts and cultural center, a 50,000 square foot multi-purpose event center, and the broadcast home of the local PBS affiliate. At this point, only the casino is expected to open in May 2009. There is no schedule for the remaining portion of the development.

Majestic Realty has proposed approximately 8 million square feet of industrial / warehouse space in Bethlehem, also at the former Bethlehem Steel plant. Phase 1 of the development will consist of approximately 1 million square feet of industrial / warehouse space, and this phase is currently scheduled to be on line in 2010. Nothing is anticipated to be completed and occupied during 2009.



An upscale retail lifestyle center (The Summit) was considered in Bethlehem on Route 33 and Freemansburg Road, but this development will not happen, and St. Luke's Hospital has obtained the property with long term plans for medical offices. The project will not be completed in 2009.

Greenfield Industrial Park is a proposed 1,300,000 square foot warehouse located in Bethlehem which is currently under construction and is scheduled to open sometime in 2009. We have not assumed any changes in toll bridge traffic from this development.

Martin Tower in Bethlehem is proposed to consist of a 22-story condominium building with 800 units and retail (Phase 2) and 585 townhouses on the acreage surrounding the tower (Phase 1). Phase 1 was approved in 2007 but no construction has been scheduled and is not expected to be completed by the end of 2009. Phase 2 has been put on hold indefinitely.

A 500,000 square foot mixed use office / retail project at Route 33 and Route 248 in Nazareth received approvals in 2008 but has not yet started construction. We have assumed that the project will not be occupied in 2009, and no additional traffic crossing the bridges was considered.

No major developments are proposed in southern Warren County or northern Hunterdon County for 2009.

District 3

In Pike County, the Highland Village residential development remains in the final stages of the approval process for the projects beginning phases. However nothing is expected to be occupied during 2009.

No major developments are proposed in Monroe County, northern Warren County or Sussex County for 2009.

RECENT ROADWAY CONSTRUCTION PROJECTS

County planning/engineering offices and the departments of transportation were also asked about significant roadway construction projects near the bridges. In addition, the Delaware Valley Regional Planning Commission (DVRPC) Transportation Improvement Program (TIP) was also reviewed. Our findings are as follows:

Trenton-Morrisville (Route 1) Toll Bridge Rehabilitation Project

- ☑ The Trenton-Morrisville Toll Bridge is being rehabilitated, widened and improved. The construction schedule is broken up into three (3) phases from 2007-2009. Phase 3 is underway, and is scheduled to be completed by September 2009. Traffic impacts occurring during Phase 3 are lane shifts, decreasing of lane widths, lane closures and lane restrictions. During the peak hours the bridge will have a minimum of two (2) lanes in each direction open. However during all other times the contractor has the option to close lanes in either or both directions.





Milford-Montague Toll Bridge Rehabilitation Project

- ☑ The Milford-Montague Toll Bridge is being rehabilitated to prevent major repairs for a 15 year period. The work included in the bridge improvements project is rehabilitating the bridge structure and approach roadways, and replacement of the toll plaza. The construction is scheduled to be completed before the summer of 2009. The bridge rehabilitation project will not impact the toll bridge volumes.

Interstate 78, New Jersey and Pennsylvania

- ☑ The I-78 Toll Bridge roadway within the DRJTBC jurisdiction in New Jersey will be rehabilitated. The roadway improvements will include rehabilitating the concrete roadway pavement, rehabilitation of the bridge decks and various highway feature upgrades along the corridor. The roadway improvements are scheduled to take place from October 2007 to October 2009. During the peak hours the roadway will have three (3) lanes in each direction open. However during all other times the contractor has the option to close lanes in either or both directions. Any diversions due to congestion will likely go to the Easton-Phillipsburg Toll Bridge, so no net change in revenues is anticipated.
- ☑ The I-78 Toll Bridge roadway within the DRJTBC Pennsylvania jurisdiction will be implementing open road tolling (ORT) within the vicinity of the toll plaza. The ORT will allow drivers to pass under a barrier-free electronic array without stopping or slowing down. Any diversions due to congestion will likely go to the Easton-Phillipsburg Toll Bridge, so no net change in revenues is anticipated.

Bucks County, Pennsylvania

- ☑ The Pennsylvania Turnpike will have an interchange with I-95 in Bristol. The project is in preliminary phases of construction, with stage 1 construction anticipated to begin in the first half of 2009. This phase is not expected to impact any traffic patterns in 2009.
- ☑ In Montgomery, Doylestown and Warrington Townships, US Route 202 from Horsham Road to SR 611 is being widened. The construction schedule will be broken up into four (4) phases from 2007-2010. However the distance of the project to the New Hope - Lambertville Toll Bridge is not anticipated to impact the toll bridge volumes.

Mercer and Hunterdon Counties, New Jersey

- ☑ There are no significant projects in the vicinity of the Commission bridges anticipated to affect traffic volumes.

Warren and Sussex Counties, New Jersey

- ☑ Resurfacing of Interstate 80 will take place from Hope, in Warren County, to Byram in Sussex County. The project is scheduled for completion in October, 2009. The resurfacing is not anticipated to alter traffic volumes in 2009.

In reviewing the Lehigh Valley Transportation Improvement Program (TIP) as well as the DVRPC TIP for Pennsylvania and New Jersey, there are no major construction projects planned in other areas that are projected to have significant effects on volumes or patterns near the bridges.



HISTORICAL TRAFFIC VOLUMES

The Commission provided historical traffic volume information for the 11 vehicular toll-supported bridges and the seven (7) toll bridges. For the purpose of this study, volumes and toll revenue data from the years 2003 to 2008 were used.

Monthly traffic volume data for the toll-supported bridges is summarized on a yearly basis from 2003 to 2008, as listed in Tables 1 through 6. Where volume data was not available, traffic volumes were estimated and are shown in italics. No vehicle classification was provided, but most toll-supported bridges (with the exception on the Scudder Falls Bridge and the Upper Black Eddy-Milford Bridge) have weight restrictions prohibiting large trucks.

The Scudder Falls Toll Supported Bridge carries approximately 20-21 million vehicles per year, which converts to an average annual daily traffic volume (AADT) of approximately 57,000 vehicles. Since traffic is higher on weekdays, the average weekday traffic volume (AWDT) is approximately 60,000 vehicles. Volumes on the Scudder Falls Bridge have remained constant for several years. The Northampton Street and Calhoun Street Toll Supported Bridges carry approximately 6.7-8.5 million vehicles per year, and the Lower Trenton and New Hope-Lambertville Toll Supported Bridges carry approximately 4.5 – 6.8 million vehicles per year. While the Calhoun Street Toll Supported Bridge has always carried more traffic than the Lower Trenton Toll Supported Bridge, this past year the Lower Trenton Toll Supported Bridge carried approximately 50,000 more vehicles than the Calhoun Street Toll Supported Bridge, in part from construction on Route 1. The remaining toll-supported bridges carry from 1.1 to 2.7 million vehicles per year.

At the toll supported bridges, there were minor fluctuations in volumes year to year on most bridges, with the five (5) year trend generally less than three (3) percent per year. During 2004, the New Hope -Lambertville Toll Supported Bridge was under construction, decreasing volumes. The Lower Trenton Toll Supported Bridge realized the greatest yearly changes immediately after the toll increase, and again saw a large change from 2003 to 2004 due to the toll decrease. More vehicles gradually returned to the Trenton-Morrisville (Route 1) Toll Bridge from the Lower Trenton Toll Supported Bridge during 2005 and 2006, but shifted back to the Lower Trenton Toll Supported Bridge during 2007, in part from the construction at the Trenton-Morrisville Toll Bridge.

Reviewing information from the seven (7) toll bridges under the jurisdiction of the Commission during 2008, we found the Trenton-Morrisville (US Route 1), I-78, Easton-Phillipsburg (US Route 22), and Delaware Water Gap (Interstate 80) Toll Bridges carry between 6.4 million and 9.9 million toll paying (westbound) vehicles per year. The remaining three (3) toll bridges carry between 1.3 million and 1.8 million toll paying (westbound) vehicles per year. These figures have remained consistent over the past few years, with the exception of the New Hope -Lambertville Toll Bridge, which saw a spike in passenger cars during 2004 from the construction on the New Hope -Lambertville Toll Supported Bridge.

The five (5) axle tractor-trailer continues to be the most common truck type, representing approximately 9.2 percent of vehicles crossing the seven (7) toll bridges during 2008, and estimated to comprise approximately nine percent of vehicles during 2009 but generating approximately 59 percent of the 2009 toll revenue. Conversely, passenger cars represented approximately 86 percent of the vehicles on the seven (7) toll bridges during 2008, and are projected to generate approximately 26 percent of the toll revenue during 2009. The auto /



tractor trailer volume and revenue percentages have remained consistent for the past several years.

YEAR 2009 TRAFFIC VOLUME AND TOLL REVENUE PROJECTIONS

Based on the findings listed above, a growth or reduction factor was applied to 2008 data for each vehicle type on each toll bridge to project year 2009 volumes. Generally, recent one (1) year to three (3) year growth trends are considered. However, historic economic conditions experienced during 2008, particularly since late September 2008 indicate that one to three year growth trends may not be practical.

Gasoline prices were steadily rising throughout 2008, peaking at approximately \$4.00 per gallon in July. By early December 2008, gasoline was observed at \$1.44 per gallon in New Jersey. In addition, the Dow Jones Industrial Average has lost 32% of its value from January 2, 2008 to December 8, 2008, and 20% of its value from September 29, 2008 to December 8, 2008. The economic slowdown is expected to reduce the movement of commuters and truck crossings during 2009.

Tables 7 through 13 illustrate actual traffic volumes for the seven (7) toll bridges for the years 2003 through 2008, as well as the projected year 2009 volumes. As indicated in the tables, passenger car volumes are projected to decrease from 1% to 3% at the seven (7) toll bridges from 2008 to 2009. The five axle tractor trailers, or Class 5 trucks (the most common truck type) is projected to decrease from 2% to 4% at the seven (7) toll bridges from 2008 to 2009, with Class 5 trucks at I-78 and the Delaware Water Gap (representing 87% of Commission Class 5 toll crossings) projected to decrease 4%. The current toll structure was applied to the projected 2009 volumes to determine the projected year 2009 revenue for each toll bridge. **While the reduction in volumes may be higher than four percent (4%), we cannot justify it from a traffic engineering perspective.**

The E-ZPass electronic toll collection system provided a discount over cash paying customers, but the casual discount has been removed. However, frequent or commuter E-ZPass passenger car users that have 20 or more crossings in a 35-calendar day period will continue to pay \$0.45, a 40% discount over the cash fare. The sum of commuter E-ZPass transactions was provided for the seven (7) toll bridges. Based on E-ZPass penetration rates at each toll bridge and the number of total commuter E-ZPass transactions, we were able to estimate the number of cash paying passenger cars/casual E-ZPass passenger cars, and commuting E-ZPass passenger cars at each of the seven (7) toll bridges.

For trucks, the discounted E-ZPass fares for peak traffic (6 AM – 9 PM) have been removed, while off peak traffic (9 PM – 6 AM) will receive a 10% discount, reduced from 15%. Review of E-ZPass truck data from January through October 2008 provided the percentage of peak traffic and off peak traffic for each truck class on every toll bridge.

We determined the percentage of off peak E-ZPass users for each vehicle class at each bridge to reach a weighted average toll. For example, the 2-axle trucks at the Trenton-Morrisville Bridge will have 93.7% cash/peak E-ZPass users at \$5.00, and 6.3% off peak E-ZPass users at \$4.25, for a weighted average toll of \$4.95. Special permit vehicles will maintain the same toll structure of \$0.40 per ton plus \$2.00 permit fee. For example, a truck weighing 80,000 pounds (40 tons) will pay \$18.00.



Table 14 compares the 2008 volumes and revenues for each bridge and maintenance district with the projected 2009 volumes. As indicated, overall toll traffic volumes are projected to decrease by approximately 780,000 vehicles (-2.10%) and revenues are projected to increase by approximately \$1.88 million (+2.18%).

Table 15 is provided in response to (i) of Section 703 (b) paragraph 2 and provides 2008 Total Revenue, 2008 Operating Expenses, Net Revenue, Maximum Annual Debt Service, and 130% of the Maximum Annual Debt Service. All values were provided by the Commission. The requirement that the Net Revenue for the preceding fiscal year be at least 130% of the Maximum Annual Debt Service was met, as indicated in the Table.

Additionally, to bring the Commission in conformance with other toll agency practices, the Commission will be charging the 67,800 E-ZPass accounts a monthly \$1 account service fee, generating an additional \$813,600 annually. Assuming a June 1, 2009 implementation date for the E-ZPass account service fee, the Commission will recognize additional revenue of \$474,600 during 2009.

Table 16 lists the 2009 projected toll revenues, and subtracts the projected operating expenses. The Net Revenue is then divided by Maximum Annual Debt Service to calculate a Projected Debt Service Coverage Ratio that is required to be not less than 1.30. The Commission provided all the figures in Table 16, with the exception of the projected 2009 toll revenue. With a Projected Debt Service Coverage Ratio of 1.40, the requirements of all current Bridge System Revenue Bonds are projected to be met.



Table 1 - 2003 Toll Supported Bridge Volumes

Month	Lower Trenton	Calhoun Street	Scudler Falls	Washington Crossing	New Hope/Lambertville	Centre Bridge/Stockton	Uhlertown/Frenchtown	Upper Black Eddy/Milford	Riegalsville	Northampton Street	Riverton/Belvidere	Total
January	564,310	<i>550,000</i>	1,599,968	196,664	429,548	157,104	<i>117,000</i>	<i>100,000</i>	<i>79,000</i>	974,041	143,833	4,911,468
February	443,845	521,260	1,356,222	164,661	377,167	122,798	95,028	101,033	78,494	555,358	128,562	3,944,428
March	548,534	640,157	1,693,978	207,462	482,877	159,330	123,253	128,069	96,178	685,567	161,045	4,926,450
April	538,237	636,833	1,731,919	214,795	488,760	178,486	127,826	132,334	100,623	689,978	167,730	5,007,521
May	<i>564,018</i>	<i>650,000</i>	1,803,229	221,906	514,736	189,238	138,952	144,159	105,264	722,607	179,714	5,233,823
June	551,801	<i>611,738</i>	1,774,949	220,403	509,340	188,205	138,492	140,910	100,064	700,702	176,310	5,112,914
July	579,269	639,029	1,825,107	234,055	535,268	<i>203,903</i>	152,565	<i>148,691</i>	105,971	721,007	185,300	5,330,165
August	<i>569,290</i>	626,182	1,797,945	223,958	527,067	195,991	147,191	145,387	103,158	<i>720,548</i>	<i>176,188</i>	5,232,905
September	547,070	596,817	1,725,191	219,640	482,969	178,125	133,080	136,146	94,722	<i>691,376</i>	168,274	4,973,410
October	573,398	621,353	1,849,644	236,089	522,009	188,364	136,929	141,315	100,340	724,195	177,738	5,271,374
November	515,313	568,435	1,689,946	205,939	478,800	173,185	124,837	128,857	94,727	673,099	157,195	4,810,333
December	527,089	572,532	1,633,976	199,306	458,381	153,321	110,861	120,553	94,378	669,187	149,241	4,688,825
Total	6,522,174	7,234,336	20,482,074	2,544,878	5,806,922	2,088,050	1,546,014	1,567,454	1,152,919	8,527,665	1,971,130	59,443,616

estimated figures due to adjustments shown in *italics*
 adjusted figures due to counter malfunction rounded



Table 2 - 2004 Toll Supported Bridge Volumes

Month	Lower Trenton	Calhoun Street	Scudler Falls	Washington Crossing	New Hope/Lambertville	Centre Bridge/Stockton	Ukiahstown/Frenchtown	Upper Black Eddy/Milford	Riegalsville	Northampton Street	Riverton/Belvidere	Total
January	495,049	545,773	1,558,639	215,643	131,272	158,320	104,599	112,951	89,299	647,096	139,963	4,198,604
February	491,631	541,831	1,584,203	220,054	157,576	162,945	108,568	114,563	86,645	646,150	141,661	4,255,827
March	543,079	599,213	1,773,426	244,549	137,807	181,410	122,478	128,366	97,390	718,412	160,225	4,706,355
April	532,424	594,358	1,802,794	250,560	154,061	193,391	130,759	141,162	97,980	724,557	164,864	4,786,910
May	548,806	619,033	1,841,026	270,327	143,402	214,181	143,818	149,292	100,696	750,157	174,202	4,954,940
June	537,692	609,804	1,831,365	250,175	301,416	196,765	139,959	146,241	97,578	708,925	171,847	4,991,767
July	533,218	584,653	1,758,351	231,474	426,710	188,715	146,966	149,039	98,234	716,311	174,983	5,008,654
August	536,367	555,972	1,811,783	221,746	436,722	185,161	144,138	144,543	97,182	720,922	170,090	5,024,626
September	<i>547,070</i>	534,777	1,789,294	199,741	390,205	166,567	125,187	133,017	89,810	634,981	160,925	4,771,574
October	<i>573,398</i>	587,641	1,459,900	216,130	443,149	177,050	133,368	142,999	98,574	714,031	170,631	4,716,871
November	515,313	556,841	1,740,078	190,649	409,346	159,500	116,295	127,600	92,094	675,077	153,783	4,736,576
December	480,000	577,632	1,736,170	190,638	420,771	155,204	112,075	127,686	97,425	692,831	155,831	4,746,263
Total	6,334,047	6,907,528	20,687,029	2,701,686	3,552,437	2,139,209	1,528,210	1,617,459	1,142,907	8,349,450	1,939,005	56,898,967

estimated figures due to adjustments shown in *italics*
 adjusted figures due to counter malfunction rounded



Table 3 - 2005 Toll Supported Bridge Volumes

Month	Lower Trenton	Calhoun Street	Scudler Falls	Washington Crossing	New Hope/Lambertville	Centre Bridge/Stockton	Uhlertown/Frenchtown	Upper Black Eddy/Milford	Riegalsville	Northampton Street	Riverton/Belvidere	Total
January	453,991	520,094	1,566,950	175,523	369,422	130,496	98,147	115,312	85,589	615,480	136,364	4,267,368
February	436,839	506,071	1,521,736	167,777	359,028	126,870	95,552	109,768	82,146	599,385	135,497	4,140,669
March	543,079	580,142	1,766,709	192,763	405,788	148,274	113,344	126,969	94,544	692,465	155,279	4,819,356
April	532,424	527,249	1,131,518	21,035	385,277	145,725	103,752	116,299	99,691	591,668	147,988	3,802,626
May	<i>548,806</i>	604,119	1,877,850	183,915	441,442	182,304	128,463	137,669	114,644	716,255	167,351	5,102,818
June	537,692	601,724	1,858,574	198,817	436,210	182,171	127,998	132,171	116,004	710,299	165,285	5,066,945
July	533,218	599,309	1,786,565	202,953	427,856	188,107	138,408	135,112	114,466	700,001	170,799	4,996,794
August	<i>510,000</i>	598,063	1,858,505	201,975	437,261	180,094	134,231	131,779	110,654	741,908	162,021	5,066,491
September	482,514	558,116	1,662,649	202,075	417,298	160,857	125,248	125,340	103,239	690,890	160,440	4,688,666
October	504,022	560,559	1,745,874	200,667	439,579	<i>172,000</i>	125,108	124,343	104,940	710,506	166,786	4,854,384
November	472,857	541,370	1,654,746	186,307	417,122	145,307	116,073	116,732	99,694	678,235	159,536	4,587,979
December	480,984	558,001	1,673,429	177,476	414,259	128,022	106,302	112,082	101,868	697,971	155,725	4,606,119
Total	6,036,426	6,754,817	20,105,105	2,111,283	4,950,542	1,890,227	1,412,626	1,483,576	1,227,479	8,145,063	1,883,071	56,000,215

estimated figures due to adjustments shown in *italics*
 adjusted figures due to counter malfunction rounded



Table 4 - 2006 Toll Supported Bridge Volumes

Month	Lower Trenton	Calhoun Street	Scudder Falls	Washington Crossing	New Hope/Lambertville	Centre Bridge-Stockton	Unierstown-Frenchtown	Upper Black Eddy-Milford	Riegalsville	Northampton Street	Riverton/Belvidere	Total
January	481,349	542,134	1,647,638	180,403	392,376	113,462	106,700	109,085	97,553	692,038	156,259	4,518,997
February	460,026	506,035	1,512,963	162,729	329,479	114,662	96,112	101,386	90,141	657,336	144,571	4,175,440
March	523,914	581,075	1,776,740	191,241	429,947	145,430	116,468	118,054	105,674	743,968	167,761	4,900,272
April	504,442	559,811	1,734,750	195,203	444,336	154,511	120,327	121,299	107,097	739,970	164,243	4,845,989
May	527,000	581,547	1,826,526	212,848	464,451	169,518	130,353	135,000	111,162	753,909	173,749	5,086,063
June	512,623	600,000	1,788,813	205,000	450,000	165,000	128,000	111,000	106,000	691,000	173,000	4,930,436
July	506,000	558,000	1,700,000	200,000	445,000	159,000	115,000	141,000	104,618	670,000	163,480	4,762,098
August	522,121	570,908	1,826,859	212,444	458,066	159,240	115,004	145,038	105,974	703,761	162,924	4,982,339
September	507,037	539,572	1,687,969	208,244	432,513	149,144	119,096	116,836	101,082	676,601	156,138	4,694,232
October	522,611	562,501	1,511,747	224,156	445,294	156,057	123,489	120,092	104,976	713,693	122,807	4,607,423
November	491,981	529,549	1,703,521	193,677	409,206	148,027	110,682	109,788	100,046	679,434	130,358	4,606,269
December	507,939	546,301	1,710,279	194,945	437,619	149,662	113,254	113,532	105,741	721,389	155,793	4,756,454
Total	6,067,043	6,677,433	20,427,805	2,380,890	5,138,287	1,783,713	1,394,485	1,442,110	1,240,064	8,443,099	1,871,083	56,866,012

estimated figures due to adjustments shown in *italics*

adjusted figures due to counter malfunction rounded

June July figures represent adjusted volumes without closures due to flooding



Table 5 - 2007 Toll Supported Bridge Volumes

Month	Lower Trenton	Calhoun Street	Scudder Falls	Washington Crossing	New Hope/Lambertville	Centre Bridge-Stockton	Uhlerstown-Frenchtown	Upper Black Eddy-Milford	Riegelsville	Northampton Street	Riverton-Belvidere	Total
January	524,204	542,687	1,685,485	192,444	419,839	41,725	109,786	105,158	96,816	693,543	118,486	4,530,173
February	469,357	473,726	1,500,683	171,680	367,118	37,041	93,879	91,227	85,694	605,955	102,020	3,998,380
March	563,583	565,893	1,757,094	200,232	441,053	42,871	112,786	110,080	100,047	719,066	122,695	4,735,400
April	552,445	553,288	1,753,484	199,323	440,986	52,777	116,939	111,931	104,787	725,886	124,217	4,736,063
May	610,088	605,582	1,907,911	228,224	485,112	-	135,253	125,821	114,212	757,722	152,232	5,122,157
June	588,576	597,164	1,873,937	219,692	459,198	144,609	129,958	121,992	110,936	716,876	140,936	5,103,874
July	585,804	594,745	1,840,925	214,810	469,964	156,410	134,273	128,239	111,487	703,747	140,140	5,080,544
August	607,531	606,545	1,899,467	215,831	473,885	153,788	131,437	128,664	110,141	718,414	150,648	5,196,351
September	560,732	550,187	1,757,370	209,360	449,773	151,546	124,988	122,259	104,853	679,051	145,880	4,855,999
October	604,763	581,938	1,895,727	231,077	464,487	148,710	125,436	121,808	109,742	714,884	153,385	5,151,957
November	568,910	535,795	1,769,634	208,935	426,255	132,809	108,631	111,020	102,463	686,364	137,410	4,788,226
December	553,963	550,136	1,685,119	208,741	420,918	125,165	102,058	105,890	100,357	669,354	127,623	4,649,324
Total	6,789,956	6,757,686	21,326,836	2,500,349	5,318,588	1,187,451	1,425,424	1,384,089	1,251,535	8,390,862	1,615,672	57,948,448

estimated figures due to adjustments shown in *italics*
 adjusted figures due to counter malfunction rounded

Centre Bridge - Stockton Bridge weekday closures for TS-429A (Jan-May, Nov) and the count station loop detectors were replaced in May

Riverton - Belvidere Bridge weekend and weekday closures for TS-371A (Jan-July, Oct-Nov)

Increase in traffic volume on Lower Trenton Bridge is a result of construction on the Trenton - Morrisville Toll Bridge (T-380B)



Table 6 - 2008 Toll Supported Bridge Volumes

Month	Lower Trenton	Calhoun Street	Scudder Falls	Washington Crossing	New Hope-Lambertville	Centre Bridge-Stockton	Unlertown-Frenchtown	Upper Black Eddy-Milford	Riegalsville	Northampton Street	Riverton-Belvidere	Total
January	549,171	546,405	1,713,675	212,292	401,420	118,394	107,030	104,657	100,513	684,793	136,459	4,674,809
February	517,339	518,435	1,599,753	196,860	374,271	104,984	95,233	90,579	93,440	633,000	120,662	4,344,555
March	581,858	576,991	1,757,169	213,822	418,575	127,589	111,371	106,873	107,891	669,907	143,026	4,815,072
April	584,031	577,733	1,819,802	232,002	434,746	135,471	116,558	108,559	112,799	636,451	149,179	4,907,331
May	589,750	585,529	1,853,292	236,178	459,526	145,187	126,709	117,382	<i>114,937</i>	666,428	154,320	5,049,238
June	579,166	570,037	1,809,912	222,609	447,505	146,138	124,312	114,459	<i>110,376</i>	683,685	148,997	4,957,196
July	585,286	581,134	1,827,359	206,636	457,613	143,721	131,454	114,257	102,929	684,172	154,410	4,988,971
August	581,026	574,488	1,813,596	217,165	463,633	150,120	129,548	114,789	104,825	664,408	152,225	4,965,823
September	551,811	553,676	1,815,570	216,066	424,235	137,923	116,167	102,073	98,494	635,448	147,383	4,798,846
October	557,418	<i>577,329</i>	<i>1,826,723</i>	233,058	443,608	<i>140,967</i>	126,718	98,207	103,852	672,295	156,041	4,936,216
<i>November</i>	<i>568,910</i>	535,795	1,769,634	208,935	426,255	<i>132,809</i>	108,631	111,020	102,463	686,364	<i>137,410</i>	4,788,226
<i>December</i>	<i>553,963</i>	550,136	1,685,119	208,741	420,918	125,165	102,058	105,890	100,357	669,354	127,623	4,649,324
Total	6,799,729	6,747,688	21,291,603	2,604,364	5,172,305	1,608,468	1,395,789	1,288,745	1,252,877	7,986,305	1,727,735	57,875,607

estimated figures due to adjustments shown in *italics*

adjusted figures due to counter malfunction rounded

Increase in traffic volume on Lower Trenton Bridge (January and February) is a result of construction on the Trenton - Morrisville Toll Bridge (T-380B)

November and December data from 2007, and not adjusted



Table 7: Trenton-Morrisville Toll Bridge Volume and Revenue Projections

class	2003 volume	2004 volume	2005 volume	2006 volume	2007 volume	2008 volume	2009 volume (projected)	factor from 2008 to 2009
1 - passenger car	5,771,654	6,281,830	6,588,111	6,805,085	6,396,032	6,115,067	5,992,766	0.980
2 - 2-axle truck	145,020	159,655	172,109	181,550	176,380	175,101	171,599	0.980
3 - 3-axle truck	60,411	71,473	74,247	78,038	83,143	85,423	83,715	0.980
4 - 4-axle truck	47,223	50,275	55,136	58,329	61,861	65,872	64,555	0.980
5 - 5-axle truck	165,579	169,038	185,618	194,518	178,566	172,610	165,706	0.960
6 - 6-axle truck	1,404	1,594	1,876	1,769	1,493	1,762	1,744	0.990
8 - special permit *	61	-	-	-	-	-	-	
7 - 7-axle truck	122	146	132	136	39	32	32	0.990
total toll	6,191,474	6,734,011	7,077,229	7,319,425	6,897,514	6,615,867	6,480,117	

* Special Permit vehicles were classified differently after 2003

class	toll	2009 volume (projected)	2009 revenue
1 - passenger car	\$ 0.73	5,992,766	\$ 4,358,200.90
2 - 2-axle truck	\$ 4.97	171,599	\$ 852,622.24
3 - 3-axle truck	\$ 9.65	83,715	\$ 808,006.63
4 - 4-axle truck	\$ 12.84	64,555	\$ 828,899.75
5 - 5-axle truck	\$ 15.98	165,706	\$ 2,647,272.12
6 - 6-axle truck	\$ 19.36	1,744	\$ 33,756.09
8 - special permit	varies	0	\$ 0.00
7 - 7-axle truck	\$ 22.58	<u>32</u>	\$ <u>722.61</u>
Totals		6,480,117	\$ 9,529,480.33



Table 8: New Hope-Lambertville Toll Bridge Volume and Revenue Projections

class	2003 volume	2004 volume	2005 volume	2006 volume	2007 volume	2008 volume	2009 volume (projected)	factor from 2008 to 2009
1 - passenger car	1,298,859	2,026,746	1,700,215	1,720,641	1,894,591	1,751,053	1,716,032	0.980
2 - 2-axle truck	35,788	52,056	50,979	56,265	57,425	55,179	54,075	0.980
3 - 3-axle truck	20,198	24,171	26,248	31,139	28,569	24,315	23,829	0.980
4 - 4-axle truck	6,470	7,797	7,052	6,938	7,614	6,712	6,578	0.980
5 - 5-axle truck	24,372	27,141	26,682	26,910	28,473	25,554	24,787	0.970
6 - 6-axle truck	745	804	718	757	964	791	783	0.990
8 - special permit *	1	-	4	1	-	1	1	1.000
7 - 7-axle truck	34	67	48	52	50	21	21	0.990
	1,386,467	2,138,782	1,811,946	1,842,703	2,017,686	1,863,626	1,826,106	

* Special Permit vehicles were classified differently after 2003

** 2004 auto volumes higher than normal due to New Hope Lambertville Toll Supported Bridge Closures

class	toll	2009 volume (projected)	2009 revenue
1 - passenger car	\$ 0.73	1,716,032	\$ 1,247,759.54
2 - 2-axle truck	\$ 4.97	54,075	\$ 268,919.09
3 - 3-axle truck	\$ 9.68	23,829	\$ 230,778.77
4 - 4-axle truck	\$ 12.71	6,578	\$ 83,581.14
5 - 5-axle truck	\$ 16.03	24,787	\$ 397,259.44
6 - 6-axle truck	\$ 19.26	783	\$ 15,083.89
8 - special permit	varies	1	\$ 26.00
7 - 7-axle truck	\$ 22.64	21	\$ 475.48
Totals		1,826,106	\$ 2,243,883.35



Table 9: Interstate 78 Toll Bridge Volume and Revenue Projections

class	2003 volume	2004 volume	2005 volume	2006 volume	2007 volume	2008 volume	2009 volume (projected)	factor from 2008 to 2009
1 - passenger car	6,518,607	6,974,743	7,226,070	7,605,954	7,821,489	7,572,684	7,345,503	0.970
2 - 2-axle truck	199,840	222,516	231,076	236,629	235,204	230,976	226,356	0.980
3 - 3-axle truck	102,434	93,683	99,176	104,217	106,916	103,201	101,137	0.980
4 - 4-axle truck	115,586	111,525	119,102	127,958	124,799	115,792	113,476	0.980
5 - 5-axle truck	1,891,300	1,946,024	1,922,988	1,943,206	1,877,951	1,843,399	1,769,663	0.960
6 - 6-axle truck	30,728	35,967	38,604	41,381	42,808	49,048	48,558	0.990
8 - special permit *	797	8	12	8	9	3	3	1.000
7 - 7-axle truck	1,113	1,379	1,420	1,485	1,123	1,358	1,344	0.990
total toll	8,860,405	9,385,845	9,638,448	10,060,838	10,210,299	9,916,461	9,606,040	

* Special Permit vehicles were classified differently after 2003

class	toll	2009 volume (projected)	2009 revenue
1 - passenger car	\$ 0.73	7,345,503	\$ 5,345,485.85
2 - 2-axle truck	\$ 4.96	226,356	\$ 1,122,218.38
3 - 3-axle truck	\$ 9.60	101,137	\$ 971,290.33
4 - 4-axle truck	\$ 12.61	113,476	\$ 1,431,020.78
5 - 5-axle truck	\$ 15.95	1,769,663	\$ 28,222,230.95
6 - 6-axle truck	\$ 18.92	48,558	\$ 918,769.77
8 - special permit	varies	3	\$ 101.20
7 - 7-axle truck	\$ 22.58	1,344	\$ 30,351.47
Totals		9,606,040	\$ 38,041,468.73



Table 10: Easton-Phillipsburg Toll Bridge Volume and Revenue Projections

class	2003 volume	2004 volume	2005 volume	2006 volume	2007 volume	2008 volume	2009 volume (projected)	factor from 2008 to 2009
1 - passenger car	5,004,027	5,551,047	5,690,754	5,702,051	5,742,513	5,914,575	5,855,429	0.990
2 - 2-axle truck	154,235	168,748	173,094	168,505	164,859	159,661	156,468	0.980
3 - 3-axle truck	62,981	60,320	64,105	64,531	59,599	61,510	60,895	0.990
4 - 4-axle truck	41,555	45,422	42,727	48,881	60,400	59,053	58,462	0.990
5 - 5-axle truck	259,050	263,362	263,496	250,482	210,038	213,711	207,300	0.970
6 - 6-axle truck	3,841	4,853	5,826	6,699	3,351	3,080	3,049	0.990
8 - special permit *	72	-	-	-	-	-	-	-
10 - local bus								
11 - 7-axle truck	208	211	252	177	113	71	70	0.990
total toll vehicles	5,525,969	6,093,963	6,240,254	6,241,326	6,240,873	6,411,661	6,341,673	

* Special Permit vehicles were classified differently after 2003

class	toll	2009 volume (projected)	2009 revenue
1 - passenger car	\$ 0.73	5,855,429	\$ 4,267,179.67
2 - 2-axle truck	\$ 4.98	156,468	\$ 778,478.93
3 - 3-axle truck	\$ 9.65	60,895	\$ 587,693.77
4 - 4-axle truck	\$ 12.87	58,462	\$ 752,188.33
5 - 5-axle truck	\$ 15.99	207,300	\$ 3,315,654.94
6 - 6-axle truck	\$ 19.23	3,049	\$ 58,633.03
8 - special permit	varies	0	\$ 0.00
7 - 7-axle truck	\$ 22.26	70	\$ 1,558.00
Totals		6,341,673	\$ 9,761,386.67



Table 11: Portland Columbia Toll Bridge Volume and Revenue Projections

class	2003 volume	2004 volume	2005 volume	2006 volume	2007 volume	2008 volume	2009 volume (projected)	factor from 2008 to 2009
1 - passenger car	1,083,030	1,162,560	1,217,782	1,221,400	1,365,195	1,272,395	1,234,223	0.970
2 - 2-axle truck	27,528	28,720	29,958	30,743	30,772	29,159	29,013	0.995
3 - 3-axle truck	9,413	11,677	10,874	10,287	12,364	12,904	12,839	0.995
4 - 4-axle truck	5,795	6,149	6,780	8,645	7,980	8,635	8,549	0.990
5 - 5-axle truck	28,508	31,778	34,076	34,464	33,480	32,371	31,724	0.980
6 - 6-axle truck	226	453	705	589	475	313	310	0.990
8 - special permit *	9	-	-	-	-	-	-	-
7 - 7-axle truck	6	14	12	13	9	12	12	1.000
total toll vehicles	1,154,515	1,241,351	1,300,187	1,306,141	1,450,275	1,355,789	1,316,670	

* Special Permit vehicles were classified differently after 2003

class	toll	2009 volume (projected)	2009 revenue
1 - passenger car	\$ 0.73	1,234,223	\$ 898,076.70
2 - 2-axle truck	\$ 4.98	29,013	\$ 144,450.78
3 - 3-axle truck	\$ 9.70	12,839	\$ 124,544.61
4 - 4-axle truck	\$ 12.94	8,549	\$ 110,604.44
5 - 5-axle truck	\$ 16.14	31,724	\$ 511,903.64
6 - 6-axle truck	\$ 19.46	310	\$ 6,031.57
8 - special permit	varies	0	\$ 0.00
7 - 7-axle truck	\$ 22.75	12	\$ 273.00
Totals		1,316,670	\$ 1,795,884.72



Table 12: Delaware Water Gap (Interstate 80) Toll Bridge Volume and Revenue Projections

class	2003 volume	2004 volume	2005 volume	2006 volume	2007 volume	2008 volume	2009 volume (projected)	factor from 2008 to 2009
1 - passenger car	8,066,666	8,488,847	8,493,107	8,590,643	8,501,039	8,293,436	8,169,034	0.985
2 - 2-axle truck	143,521	161,134	161,724	162,397	162,971	153,981	150,901	0.980
3 - 3-axle truck	87,427	93,075	95,818	95,676	96,380	92,707	90,853	0.980
4 - 4-axle truck	52,233	57,861	63,106	63,265	67,828	63,181	61,917	0.980
5 - 5-axle truck	1,108,058	1,128,514	1,120,941	1,124,054	1,175,507	1,098,482	1,054,543	0.960
6 - 6-axle truck	19,127	20,887	20,884	19,712	23,663	24,652	24,159	0.980
8 - special permit *	780	69	64	42	38	46	46	0.990
7 - 7-axle truck	992	1,346	1,246	1,376	1,104	1,022	1,022	1.000
total toll vehicles	9,478,804	9,951,733	9,956,890	10,057,165	10,028,530	9,727,507	9,552,475	

* Special Permit vehicles were classified differently after 2003

class	toll	2009 volume (projected)	2009 revenue
1 - passenger car	\$ 0.73	8,169,034	\$ 5,944,411.33
2 - 2-axle truck	\$ 4.96	150,901	\$ 747,948.79
3 - 3-axle truck	\$ 9.60	90,853	\$ 872,422.30
4 - 4-axle truck	\$ 12.69	61,917	\$ 785,654.57
5 - 5-axle truck	\$ 15.97	1,054,543	\$ 16,839,737.71
6 - 6-axle truck	\$ 18.99	24,159	\$ 458,753.61
8 - special permit	varies	46	\$ 2,360.34
7 - 7-axle truck	\$ 22.68	<u>1,022</u>	\$ <u>23,176.94</u>
Totals		9,552,475	\$ 25,674,465.59



Table 13: Milford-Montague Toll Bridge Volume and Revenue Projections

class	2003 volume	2004 volume	2005 volume	2006 volume	2007 volume	2008 volume	2009 volume (projected)	factor from 2008 to 2009
1 - passenger car	1,231,491	1,311,848	1,300,872	1,303,872	1,310,047	1,267,024	1,254,354	0.990
2 - 2-axle truck	21,418	22,786	23,234	24,278	26,171	26,875	26,606	0.990
3 - 3-axle truck	5,139	5,328	5,244	5,228	4,545	4,469	4,424	0.990
4 - 4-axle truck	2,145	1,929	1,887	1,946	2,011	1,616	1,584	0.980
5 - 5-axle truck	10,626	10,495	10,014	9,380	8,921	8,897	8,719	0.980
6 - 6-axle truck	119	107	99	78	85	130	129	0.990
8 - special permit *	7	3	-	-	-	-	-	-
7 - 7-axle truck	41	38	24	18	11	7	7	0.990
total toll	1,270,986	1,352,534	1,341,374	1,344,800	1,351,791	1,309,018	1,295,823	

* Special Permit vehicles were classified differently after 2003

class	toll	2009 volume (projected)	2009 revenue
1 - passenger car	\$ 0.73	1,254,354	\$ 912,847.41
2 - 2-axle truck	\$ 4.97	26,606	\$ 132,339.67
3 - 3-axle truck	\$ 9.71	4,424	\$ 42,957.62
4 - 4-axle truck	\$ 12.95	1,584	\$ 20,505.83
5 - 5-axle truck	\$ 16.07	8,719	\$ 140,113.19
6 - 6-axle truck	\$ 19.42	129	\$ 2,504.93
8 - special permit	varies	0	\$ 0.00
7 - 7-axle truck	\$ 22.10	7	\$ 154.70
Totals		1,295,823	\$ 1,251,423.36



Table 14: Volume and Revenue Comparison -- 2008 to 2009 with Reduced E-ZPass Discounts

Revenue Summary by Bridge/District		2008 Volumes *	2008 Revenues **	2009 Volume (Projected)	2009 Revenue (Projected)	Change in Actual vs. Projected Volume from 2008 to 2009		Change in Actual vs. Projected Revenue from 2008 to 2009	
						vehicles	percent	dollars	percent
District									
1	Trenton-Morrisville	6,615,867	\$ 9,132,379.48	6,480,117	\$ 9,529,480.33	-135,750	-2.05%	\$ 397,100.85	4.35%
1	New Hope-Lambertville	1,863,626	\$ 2,097,836.53	1,826,106	\$ 2,243,883.35	-37,520	-2.01%	\$ 146,046.82	6.96%
2	Interstate 78	9,916,461	\$ 37,722,660.04	9,606,040	\$ 38,041,468.73	-310,421	-3.13%	\$ 318,808.69	0.85%
2	Easton-Phillipsburg	6,411,661	\$ 9,284,658.05	6,341,673	\$ 9,761,386.67	-69,988	-1.09%	\$ 476,728.62	5.13%
3	Portland-Columbia	1,355,789	\$ 1,699,004.88	1,316,670	\$ 1,795,884.72	-39,119	-2.89%	\$ 96,879.84	5.70%
3	Delaware Water Gap	9,727,507	\$ 25,315,834.20	9,552,475	\$ 25,674,465.59	-175,032	-1.80%	\$ 358,631.39	1.42%
3	Milford-Montague	1,309,018	\$ 1,162,969.71	1,295,823	\$ 1,251,423.36	-13,195	-1.01%	\$ 88,453.65	7.61%
	Total	37,199,929	\$ 86,415,342.89	36,418,904	\$ 88,297,992.76	-781,025	-2.10%	\$ 1,882,649.87	2.18%

* 2008 volumes from December 2007 to November 2008

** 2008 revenues from December 2007 to November 2008



Table 15: Actual Revenue and Expenditures for 2008

District	Bridge	2008 Volume *	2008 Revenue *
1	Trenton-Morrisville	6,615,867	\$ 9,132,379.48
1	New Hope-Lambertville	1,863,626	\$ 2,097,836.53
2	Interstate 78	9,916,461	\$ 37,722,660.04
2	Easton-Phillipsburg	6,411,661	\$ 9,284,658.05
3	Portland-Columbia	1,355,789	\$ 1,699,004.88
3	Delaware Water Gap	9,727,507	\$ 25,315,834.20
3	Milford-Montague	<u>1,309,018</u>	<u>\$ 1,162,969.71</u>
Total		37,199,929	\$ 86,415,342.89
Total Toll Revenue		(From above)	\$ 86,415,342.89
Interest Income		(Provided by Commission)	\$ 6,440,000.00
**	Other Income	(Provided by Commission)	<u>\$ 2,718,000.00</u>
1. Total Revenue - 2008			\$ 95,573,342.89
2. Operating Expenses - 2008		(Provided by Commission)	<u>\$ 44,340,268.00</u>
3. Net Revenue		(Line 1 - Line 2)	\$ 51,233,074.89
***	4. Maximum Annual Debt Service	(Provided by Commission)	\$ 32,850,400.00
5. 130% of Maximum Annual Debt Service			\$ 42,705,520.00
6. Calculated Debt Service Coverage Ratio		(Line 3 / Line 4)	1.56
* Based on Actual Volumes and Revenues from December 2007 to November 2008			
** 2008 other income includes \$1,380,000 recovered from FEMA and insurance from flood damages, and \$1,124,000 from the sale of the Commission's radio frequency.			
*** Due to disruption in the option rate securities market interest cost was higher than anticipated.			
Therefore, the requirement that the Net Revenue for the preceding fiscal year be greater than 130% of the Maximum Annual Debt Service has been met, satisfying the requirements of all current Bridge System Revenue Bonds, Section 703 (b), Paragraph 2 (i)			



Table 16: Projected Revenue and Expenditures for 2009

Projected Revenue by Bridge/District		2009 Volume	2009 Revenue
1	Trenton-Morrisville	6,480,117	\$ 9,529,480.33
1	New Hope-Lambertville	1,826,106	\$ 2,243,883.35
2	Interstate 78	9,606,040	\$ 38,041,468.73
2	Easton-Phillipsburg	6,341,673	\$ 9,761,386.67
3	Portland-Columbia	1,316,670	\$ 1,795,884.72
3	Delaware Water Gap	9,552,475	\$ 25,674,465.59
3	Milford-Montague	<u>1,295,823</u>	<u>\$ 1,251,423.36</u>
	Total	36,418,904	\$ 88,297,992.76
	Total Toll Revenue	(From above)	\$ 88,297,992.76
*	Interest Income	(Provided by Commission)	\$ 3,927,000.00
**	E-ZPass account service fee	(Provided by Commission)	\$ 474,600.00
	Other Income	(Provided by Commission)	<u>\$ 389,000.00</u>
1.	Total Projected Revenue - 2009		\$ 93,088,592.76
2.	Operating Expenses - 2009	(Provided by Commission)	<u>\$ 46,656,321.00</u>
3.	Net Revenue	(Line 1 - Line 2)	\$ 46,432,271.76
4.	Maximum Annual Debt Service	(Provided by Commission)	<u>\$ 33,078,000.00</u>
5.	Projected Debt Service Coverage Ratio	(Line 3 / Line 4)	1.40
* Interest income assumes approximately \$194,000,000 average balance and average interest rate of 2%			
** Monthly account service fee of \$1 for 67,800 E-ZPass accounts will be charged effective June 1, 2009			